

Town of Waterford
TRANSFERS OUT TO CAPITAL IMPROVEMENTS

Statement Code: GF10638SUM

Account Number / Description	Adopted Budget 7/1/2011 - 6/30/2012	Revised Budget 7/1/2011 - 6/30/2012	Current Period 3/1/2012 - 3/31/2012	Reporting Period 7/1/2011 - 3/31/2012	Encumbrances 7/1/2011 - 3/31/2012	Amt Remaining 7/1/2011 - 3/31/2012	% Remaining 7/1/2011 - 3/31/2012
10638-55738-101-060-38-00-00 FLEET MANAGEMENT PLAN	1,095,000.00	1,095,000.00	0.00	1,095,000.00	0.00	0.00	0.00 %
10638-55739-101-060-38-00-00 IT NETWORK UPGRADE	0.00	0.00	(23,000.00)	0.00	0.00	0.00	---
10638-55777-101-060-38-00-00 COMPUTER TO PLATE-PRINT SHOP	23,000.00	23,000.00	23,000.00	23,000.00	0.00	0.00	0.00 %
10638-55778-101-060-38-00-00 SEWER SYSTEM UPGRADE	18,000.00	21,951.00	0.00	21,951.00	0.00	0.00	0.00 %
GRAND TOTAL	\$1,136,000.00	\$1,139,951.00	\$0.00	\$1,139,951.00	\$0.00	\$0.00	0.00 %

Town of Waterford
TRANSFERS TO CAPITAL AND NON-RECURRING FUND

Statement Code: GF10640SUM

Account Number / Description	Adopted Budget	Revised Budget	Current Period	Reporting Period	Encumbrances	Amt Remaining	% Remaining
	7/1/2011 - 6/30/2012	7/1/2011 - 6/30/2012	3/1/2012 - 3/31/2012	7/1/2011 - 3/31/2012	7/1/2011 - 3/31/2012	7/1/2011 - 3/31/2012	7/1/2011 - 3/31/2012
10640-57495-101-060-40-00-00 ROAD RECLAMATION	240,747.00	240,747.00	0.00	240,747.00	0.00	0.00	0.00 %
10640-57639-101-060-40-00-00 REVALUATION	75,000.00	75,000.00	0.00	75,000.00	0.00	0.00	0.00 %
10640-57706-101-060-40-00-00 SOCCER LIGHTS & SPERA FIELD	34,500.00	34,500.00	0.00	34,500.00	0.00	0.00	0.00 %
10640-57731-101-060-40-00-00 POLICE ROOF & GUTTER REPLACEM	135,000.00	135,000.00	0.00	135,000.00	0.00	0.00	0.00 %
10640-57733-101-060-40-00-00 OSWEGATCHIE FIRE BLDG IMPROVE	20,000.00	20,000.00	0.00	20,000.00	0.00	0.00	0.00 %
10640-57734-101-060-40-00-00 FIRE COMM. UNDERGROUND TANK	45,000.00	45,000.00	0.00	45,000.00	0.00	0.00	0.00 %
10640-57735-101-060-40-00-00 LEARY PARK RD/PARKING IMPROVE	20,000.00	20,000.00	0.00	20,000.00	0.00	0.00	0.00 %
GRAND TOTAL	\$570,247.00	\$570,247.00	\$0.00	\$570,247.00	\$0.00	\$0.00	0.00 %

ANNUAL BUDGET		DEPT/AGENCY:		10638 CURRENT YEAR CAPITAL IMPROVEMENTS					FISCAL YEAR 2012/2013		
		COLUMN 1	COLUMN 2	COLUMN 3	COLUMN 4	COLUMN 5	COLUMN 6	COLUMN 7	COLUMN 8	COLUMN 9	
LINE	ITEM	2010/11	168	2011/12	ACTUAL	2012/2013	2012/2013	2012/2013	2012/2013	2012/2013	
	DESCRIPTION	EXPENDED	APPROP.	ADDITIONAL	TO 1/1/12	REQUEST	FIRST SELECTMAN	BD OF SELECTMEN	BD OF FINANCE	APPROVED	
CAPITAL IMPROVEMENTS											
BOARD OF SELECTMEN:											
55738	FLEET MANAGEMENT PLAN	820,000	1,095,000		1,095,000	1,095,000	1,095,000	1,095,000	1,095,000	1,095,000	
	SUBTOTAL BD. OF SELECTMEN	820,000	1,095,000	0	1,095,000	1,095,000	1,095,000	1,095,000	1,095,000	1,095,000	
INFORMATION TECHNOLOGY											
55739	IT NETWORK UPGRADE-RMS POLICE CARS	19,752	0		0	0	0	0	0	0	
55777	COMPUTER TO PLATE-PRINT SHOP	0	23,000		23,000	8,500	8,500	8,500	8,500	8,500	
	POLICE TELESTAFF SCHEDULING PROGRAM	0	0		0	28,750	0	0	0	0	
	SUBTOTAL INFORMATION TECHNOLOGY:	19,752	23,000	0	23,000	37,250	8,500	8,500	8,500	8,500	
EMERGENCY MANAGEMENT											
	UPS SYSTEM FOR COMMUNICATIONS CENTER	0	0		0	32,000	0	0	0	0	
	SUBTOTAL FIRE COMMISSION	0	0	0	0	32,000	0	0	0	0	
RECREATION & PARKS:											
55774	WFD BEACH PARK PAVILION REPAIRS	11,000	0		0	0	0	0	0	0	
	WBP ACCESSIBLE BATHROOM	0	0		0	23,817	0	0	0	0	
	SUBTOTAL RECREATION & PARKS	11,000	0	0	0	23,817	0	0	0	0	
PUBLIC WORKS:											
55775	FUEL RECORDING SYSTEM	30,000	0		0	0	0	0	0	0	
55776	RECYCLING ROLL OUT CONTAINERS	82,672	0		0	0	0	0	0	0	
	SUBTOTAL PUBLIC WORKS	112,672	0	0	0	0	0	0	0	0	
LIBRARY:											
55760	ENG. STUDY-RETROFIT ROOFTOP HVAC	20,000	0		0	0	0	0	0	0	
55778	SEWER SYSTEM UPGRADE	0	18,000		18,000	0	0	0	0	0	
	SUBTOTAL LIBRARY	20,000	18,000	0	18,000	0	0	0	0	0	
BOARD OF EDUCATION:											
	SOUNDFIELD SYSTEM - CLMS	0	0	0	0	71,000	0	0	0	0	
	SUBTOTAL BOARD OF EDUCATION:	0	0	0	0	71,000	0	0	0	0	
YOUTH SERVICES:											
	LEAD ABATEMENT/EXT. PAINTING/WINDOW REP	0	0	0	0	86,500	0	0	0	0	
	SUBTOTAL YOUTH SERVICES:	0	0	0	0	86,500	0	0	0	0	
SENIOR SERVICES:											
	REPLACEMENT CHAIRS @ COMMUNITY CENTER	0	0	0	0	0	12,155	12,155	12,155	12,155	
	SUBTOTAL SENIOR SERVICES:	0	0	0	0	0	12,155	12,155	12,155	12,155	
	DEPARTMENT TOTAL	983,424	1,136,000	0	1,136,000	1,345,567	1,115,655	1,115,655	1,115,655	1,115,655	

ANNUAL BUDGET		DEPT/AGENCY:		10640 TRANSFERS TO CAPITAL AND NON-RECURRING EXPENDITURE FUND					FISCAL YEAR 2012/2013		
LINE	ITEM	COLUMN 1	COLUMN 2	COLUMN 3	COLUMN 4	COLUMN 5	COLUMN 6	COLUMN 7	COLUMN 8	COLUMN 9	
ITEM	DESCRIPTION	ACTUAL	R.T.M.	(TRANSFER)	EXPENDED/ENC	DEP/AGENCY	FIRST SELECTMAN	RECOMMENDED	RECOMMENDED	R.T.M.	
SERVICES		EXPENDED	APPROP.	ADDITIONAL	TO 1/1/12	REQUEST	RECOMMENDED	BD OF SELECTMEN	BD OF FINANCE	APPROVED	
BOARD OF SELECTMEN:											
57639	REVALUATION	75,000	75,000		75,000	75,000	75,000	75,000	75,000	75,000	
	COHANZIE SCHOOL REMEDIATION & DEMO	0	0		0	463,100	463,100	463,100	463,100	463,100	
	AUDIO VISUAL UPGRADE T.H. AUDITORIUM	0	0		0	0	27,374	27,374	27,374	27,374	
	SUBTOTAL BD. OF SELECTMEN	75,000	75,000	0	75,000	538,100	565,474	565,474	565,474	565,474	
POLICE											
57731	ROOF & GUTTER REPLACEMENT	0	135,000		135,000	0	0	0	0	0	
	SUBTOTAL POLICE:	0	135,000	0	135,000	0	0	0	0	0	
FIRE COMMISSION:											
57733	OSWEGATCHIE BUILDING IMPROVEMENTS	0	20,000		20,000	500,000	0	0	0	0	
57734	UNDERGROUND TANK REPLACEMENTS	0	45,000		45,000	45,000	45,000	45,000	45,000	45,000	
	JORDAN PARKING LOT IMPROVEMENTS	0	0		0	80,000	80,000	80,000	80,000	80,000	
	JORDAN BLDG. DOOR REPLACEMENT	0	0		0	20,000	0	0	0	0	
	SUBTOTAL FIRE COMM.	0	65,000	0	65,000	645,000	125,000	125,000	125,000	125,000	
RECREATION & PARKS:											
57718	WTFD BEACH PARK MAINT. BLDG. REPAIR	27,000	0		0	0	0	0	0	0	
57706	SOCER LIGHTS & SPERA FIELD	45,000	34,500		34,500	0	0	0	0	0	
57735	LEARY PARK ROAD/PARKING IMPROVE.	0	20,000		20,000	0	0	0	0	0	
	LEARY PARK IRRIGATION SYSTEM	0	0		0	26,000	0	0	0	0	
	LL SOUTH - NEW LITTLE LEAGUE FIELD	0	0		0	50,000	0	0	0	0	
	SEWER CONNECTION O'NEILL/WBP	0	0		0	170,000	0	0	0	0	
	SUBTOTAL REC & PARKS	72,000	54,500	0	54,500	246,000	0	0	0	0	
SENIOR SERVICES:											
	REPLACEMENT CHAIRS-COMM. CENTER	0	0		0	12,155	0	0	0	0	
	SUBTOTAL SENION SERVICES:	0	0	0	0	12,155	0	0	0	0	
PUBLIC WORKS:											
57651	DOUGLAS LANE RECONSTRUCTION #2	0	0		0	99,000	99,000	99,000	99,000	99,000	
57090	SIDEWALKS & TRAILS	0	0		0	323,000	0	0	0	0	
57495	ROAD RECLAMATION/OVERLAY-DIMMOCK	255,000	240,747		240,747	271,619	271,619	271,619	271,619	271,619	
	JORDAN COVE RD. BRIDGE REPLACEMENT	0	0		0	380,000	380,000	380,000	380,000	380,000	
	SUBTOTAL PUBLIC WORKS	255,000	240,747	0	240,747	1,073,619	750,619	750,619	750,619	750,619	
UTILITIES COMMISSION:											
57605	MAGO POINT PS UPGRADE	390,000	0		0	0	0	0	0	0	
57719	NLWWP CTDEP Draft Order	35,000	0		0	0	0	0	0	0	
	RICHARDS GROVE PUMP STATION UPGRADE	0	0		0	422,000	0	0	0	0	
	LOGGERS HILL SEWER LINE REHABILITATION	0	0		0	1,200,000	0	0	0	0	
	SUBTOTAL UTILITIES COMM	425,000	0	0	0	1,622,000	0	0	0	0	
	DEPARTMENT TOTAL	827,000	570,247	0	570,247	4,136,874	1,441,093	1,441,093	1,441,093	1,441,093	



January 30, 2012

Paul A. Suprin, Selectman
Paul Konstantakis Selectman

RE: FIVE YEAR CAPITAL IMPROVEMENT PLAN RECOMMENDATIONS

As part of my responsibilities to prepare a Capital Improvement Plan, I received \$5,579,541 in capital improvement project requests for the fiscal year beginning July 1, 2012 and \$38,027,216 in requests for the 5 year planning period of FY 13 through FY17. Funds for capital improvements are appropriated or designated during the budget process primarily into two accounts. First, into the Current Year Capital (CYC) Improvements budget 10638; and second, Transfers to Capital and Non-Recurring Expenditure Fund (CNR) 10640. \$1,706,247 was funded in FY 12 into these accounts. I have recommended that the funding level for these two budgets for the FY 13 Capital Improvement Plan should be increased to \$2,556,748. The net increase considering revenue from 80% reimbursement on the Jordan Cove Road Bridge replacement grant and 100% on the Cohanzie School demolition from LoCIP funding, results in a net request of \$1,789,648.

The information contained in the binder includes the CIP Guidelines, Capital Improvement Fund balances and all the capital improvement requests by department. In addition my recommended plan for the upcoming fiscal year as well as the 5 year planning period are attached with my comments along with the summary of the budgets for the CNR & CYC for the FY 13 budget. This document and binder constitute my recommendations for the Capital Improvement Plan(CIP) for Fiscal Years 2013-2017. The Town Charter requires these recommendations be ratified by the Board of Selectmen.

The following are my comments on how the majority of the funds are anticipated to be used:

- Continued funding of the Fleet Management Plan in the amount of \$1,095,000. At this level of appropriation over the next several years the plan will be adequately funded.
- I have moved up the funding for upgrading the audio system in the Auditorium as a high priority project. Current estimates and bids will be provided prior to requesting the appropriation of funds.
- Information technology & hardware upgrades are an ongoing program. \$8,500 is recommended to provide the second phase of the replacement of the Computer to Print Plate for the print shop.
- Revaluation is an ongoing process and this \$75,000 allows us to fund the program over the next five years as we have done in the past.
- Funding for road improvements includes \$271,619 for road reclamation and overlay of Dimmock Road.

- \$99,000 is recommended to be designated in order to start the design of the second phase of reconstruction of Douglas Lane.
- 80% funding is available for the engineering of repairs to the Jordan Cove Road bridge under the State's Local Bridge Program. Total cost for design is estimated at \$380,000.
- Funding for replacement chairs for the Community Center is recommended at \$12,155.
- Some partial funding from existing sources to assist with the evaluation of the proposed improvements to the Oswegatchie Fire House is recommended. A building committee needs to be established.
- Repaving of the Jordan Fire House parking lot needs to use DPW to assist with getting best price prior to requesting appropriation.
- A project to change over the existing fuel storage tanks at each fire house is entering its second year of funding. Funding this year or next is dependent on the status of progress made on the first phase.
- Sewer and Water projects will come from funds already on account or through re-designation of existing funds in the CYC and CNR budgets. Although there is currently no money appropriated or designated in the CNR for sewer projects, there is a great deal available for water projects. I endorse the Utility Commission's program to upgrade the pump stations and would recommend that the Utility Commission might consider releasing water funds that have been available to them in some cases for at least a decade and ask that they be applied to continue with their pump station upgrade program in light of the current budget situation.

I anticipate reviewing these recommendations with the Board at the upcoming budget meeting and ultimately expect the Board to ratify the plan. Please feel free to contact Rudie Beers, Finance Director, Tom Wagner, Planning Director or myself with any questions you may have.

Sincerely,



Daniel Steward
First Selectman

Enclosure

Distribution List :

Representative Town Meeting: Moderator, Majority and Minority Leaders
Director of Finance & Chairman, Board of Finance
Superintendent of Schools & Chairperson, Board of Education
Director of Buildings & Grounds, Board of Education
Director of Finance and Operations, Board of Education
Planning Director
Administrative Officer, Emergency Management
Director of Fire Services
Treasurer

Tax Assessor
Town Clerk
Tax Collector
Police Chief & Chair, Board of Police Commissioners
Director of Public Works
Chief Engineer & Chair, Utility Commission
Senior Services Director & Chair, Senior Citizens Commission
Director of Recreation and Parks & Chair, Recreation & Parks Commission
Chair, Flood & Erosion Control Board
Library Director & President of the Library Board
Director of Human Resources
Purchasing Agent

CAPITAL IMPROVEMENT PLAN FOR FY 2013-17
FIRST SELECTMAN'S RECOMMENDED PLAN

RATIFIED 3/29/2012 BOARD OF SELECTMEN

CURRENT YEAR CAPITAL IMPROVEMENTS (CYC)		Proposed FY 2013	Recommended FY 2013	First Selectman's Comments & Recommendations
DESCRIPTION	DEPARTMENT			
FLEET MANAGEMENT	FIRST SELECTMAN	\$1,095,000	\$ 1,095,000	Minimum funding needed
BOARD OF EDUCATION				
SOUNDFIELD SYSTEM-CLARK LANE MIDDLE SCHOOL		\$71,000	\$ -	Insufficient funding. No back up provided
INFORMATION TECHNOLOGY COMMITTEE				
COMPUTER TO PLATE SYSTEM-PRINT SHOP		\$8,500	\$ 8,500	Second year of funding to complete project
POLICE TELESTAFF AUTOMATED SCHEDULING		\$28,750		Move out
SENIOR SERVICES				
REPLACEMENT CHAIRS COMMUNITY CENTER		\$0	\$ 12,155	Project ready for funding moved from CNR
EMERGENCY MANAGEMENT				
UPS SYSTEM FOR COMMUNICATIONS CENTER		\$32,000		Use funds from Radio Project
RECREATION & PARKS COMMISSION				
WATERFORD BEACH PARK: ACCESSIBLE BATHROOM		\$23,817		Move out, sewers planned with O'Neill project. Detailed review of Beach Park facilities needed to determine degree of modernization including flood hazard risks.
YOUTH SERVICES				
LEAD ABATEMENT, EXTERIOR PAINTING & WINDOW REPLACEMENT-SECTION 2		\$86,500		Move out, More detailed cost estimate needed
TOTAL		\$1,345,567	\$1,115,655	
TRANSFER TO CAPITAL & NON-REC. (CNR)				
DESCRIPTION	DEPARTMENT	Proposed	Recommended	
FLEET MANAGEMENT	FIRST SELECTMAN	FY 2013	FY 2013	
COHANZIE SCHOOL REMEDIATION AND DEMOLITION		\$463,100	\$ 463,100	Designation needed in anticipation of grant funding or LoCIP
AUDIO/VISUAL UPGRADE-TOWN HALL AUDITORIUM			\$ 27,374	Move up from FY2014 Priority
FIRE SERVICE				
JORDAN PARKING LOT IMPROVEMENTS		\$80,000	\$ 80,000	Work with DPW to get pricing through State Bid
JORDAN BUILDING DOOR REPLACEMENT		\$20,000		
OSWEGATCHIE-BUILDING IMPROVEMENTS		\$500,000		Establish Building Committee fund in part from remaining Building Improvement funds
FIRE STATIONS - UNDERGROUND TANK REPLACEMENT		\$45,000	\$ 45,000	Continuing project
ASSESSOR/FINANCE				
REVALUATION		\$75,000	\$ 75,000	Continuing project
SENIOR SERVICES				
REPLACEMENT CHAIRS COMMUNITY CENTER		\$12,155	\$ -	Fund from CYC
RECREATION & PARKS COMMISSION				
LEARY PARK IRRIGATION SYSTEM		\$26,000	\$ -	Move out
SEWAGE CONNECTION ONEILL & WATERFORD BEACH		\$170,000		Project represents new capital asset/improvement to Town property. Cost offset against future revenue based on recent lease amendment to allow building expansion
COSPONSORED LEAGUES				
LLSOUTH-NEW LITTLE LEAGUE FIELD		\$50,000	\$ -	Coordinate needs with LL Football-move out no detail
DEPARTMENT OF PUBLIC WORKS				
RECONSTRUCTION: DOUGLAS LANE NO 2		\$99,000	\$ 99,000	
ROAD RECLAMATION/MILL & OVERLAY: DIMMOCK RD		\$271,619	\$ 271,619	
JORDAN COVE RD BRIDGE REPLACEMENT		\$380,000	\$ 380,000	80% funded from local bridge program DOT
SIDEWALK & TRAILS: BOSTON POST RD & GOSHEN ROAD		\$323,000	\$ -	Insufficient funding available
UTILITY COMMISSION				
LOGGER HILL ROAD SEWER LINE REHAB OR REPLACEMENT		\$1,200,000	\$ -	Request pending for funding in FY12
RICHARDS GROVE ROAD PUMP STATION PARTIAL UPGRADE		\$422,000		Current level of funding for sewer projects has depleted available funding
TOTAL		\$4,136,874	\$1,441,093	

CAPITAL IMPROVEMENT PLAN FOR FY 2013-17

RATIFIED 3/29/2012 BOARD OF SELECTMEN

FIRST SELECTMAN'S RECOMMENDED PLAN

DESCRIPTION	DEPARTMENT	FUNDING SOURCE	FY 2013	FY 2014	FY 2015	FY 2016	FY 2017	FY 2013-17
FIRST SELECTMAN								
FLEET MANAGEMENT		1	\$1,095,000	\$1,095,000	\$1,095,000	\$1,095,000	\$1,095,000	\$5,475,000
COHANZIE SCHOOL REMEDIATION AND DEMOLITION		4	\$463,100					\$463,100
ASSESSOR/FINANCE								
REVALUATION		4	\$75,000	\$75,000	\$75,000	\$75,000	\$75,000	\$375,000
BOARD OF EDUCATION								
SOUNDFIELD SYSTEM-CLARK LANE MIDDLE SCHOOL		1		\$71,000				
TIME & ATTENDANCE RECORDING SYSTEM-BOE EMPLOYEES		1		\$40,000				
VOICE OVER IP CLARK LANE MIDDLE SCHOOL		1		\$45,000				
INFORMATION TECHNOLOGY COMMITTEE								
COMPUTER TO PLATE SYSTEM-PRINT SHOP		1	\$8,500					\$8,500
POLICE TELESTAFF AUTOMATED SCHEDULING		1		\$28,750				\$28,750
PRINT SHOP FOLDER INSERTER		1		\$11,900				\$11,900
AUDIOVISUAL UPGRADE-TOWN HALL AUDITORIUM		4	\$27,374					\$27,374
TOWN-WIDE TELECOMMUNICATION UPGRADE		4			\$144,348			\$144,348
POLICE COP LOGIC (WEB-BASED COMPLAINT)		4				\$19,250		\$19,250
FIRE SERVICE								
COMMUNITY FIRE PROTECTION IMPROVEMENTS		4		\$25,000	\$25,000	\$25,000		\$75,000
JORDAN PARKING LOT IMPROVEMENTS		4	\$80,000					\$80,000
JORDAN BUILDING DOOR REPLACEMENT		4		\$20,000				\$20,000
QUAKER HILL-BUILDING ROOF PROJECT		4		\$80,000				\$80,000
GOSHEN-BUNKROOM RENOVATIONS		4		\$20,000				\$20,000
OSWEGATCHIE-BUILDING EMERGENCY GENERATOR		4		\$30,000				\$30,000
OSWEGATCHIE-BUILDING IMPROVEMENTS		8	\$100,000					\$100,000
COHANZIE-BUILDING RENOVATIONS		4			\$100,000			\$100,000
PUBLIC SAFETY COMPLEX-CARPET REPLACEMENT		4		\$50,000				\$50,000
THERMAL IMAGING CAMER REPLACEMENT		2	\$16,000	\$16,000				\$32,000
FIRE STATIONS - UNDERGROUND TANK REPLACEMENT		4	\$45,000					\$45,000
POLICE								
PARKING LOT RESURFACING		4		\$170,000				\$170,000
IMPOUND STORAGE BUILDING #2		4				\$30,000		\$30,000
EMERGENCY MANAGEMENT								
UPS SYSTEM FOR COMMUNICATIONS CENTER		8	\$32,000					\$32,000
FLOOD AND EROSION CONTROL BOARD								
ALEWIFE COVE		4			\$100,000			\$100,000
SENIOR SERVICES								
REPLACEMENT CHAIRS COMMUNITY CENTER		1	\$12,155					\$12,155
YOUTH SERVICES								
LEAD ABATEMENT, EXTERIOR PAINTING & WINDOW REPLACEMENT-SECTION 2		1		\$86,500				\$86,500
HEATING & COOLING SYSTEM-SECTION 2		1			\$34,000			\$34,000
RECREATION & PARKS COMMISSION								
WATERFORD BEACH PARK: ACCESSIBLE BATHROOM		1		\$23,817				\$23,817
REPLACEMENT OF CAUSEWAY RESTROOM WFD. BEACH PARK		4		\$61,000				\$61,000
LEARY PARK IRRIGATION SYSTEM		4		\$26,000	\$24,800	\$11,500		\$62,300
RESTROOMS AT STENGER FARM PARK		4			\$30,000			\$30,000
COSPONSORED LEAGUES								
LLSOUTH-NEW LITTLE LEAGUE FIELD		4		\$50,000				\$50,000
YOUTH FOOTBALL LIGHTING IMPROVEMENTS		4		\$22,000				\$22,000
GARDINERS WOOD COMPLEX, PARKING LOT IMPROVEMENTS & LIGHTING		4			\$80,000			\$80,000
FENCING IMPROVEMENTS- LL SOUTH		4		\$50,000				\$50,000
FIELD LIGHTS - SENIOR SOFTBALL		4				\$80,000		\$80,000
PRESS BOX W/STORAGE-SENIOR SOFTBALL		4					\$20,000	\$20,000
EUGENE O'NEILL THEATRE CENTER								
SEWAGE CONNECTION ONEILL & WATERFORD BEACH		6	\$170,000					\$170,000

CAPITAL IMPROVEMENT PLAN FOR FY 2013-17

FIRST SELECTMAN'S RECOMMENDED PLAN FUNDING

DESCRIPTION	DEPARTMENT	SOURCE	FY 2013	FY 2014	FY 2015	FY2016	FY 2017	FY 2013-17
DEPARTMENT OF PUBLIC WORKS								
MUNICIPAL COMPLEX RENOVATIONS & CLEAN UP		4			\$6,313,774			\$6,313,774
RECONSTRUCTION: DOUGLAS LANE NO 2		4	\$99,000	\$1,199,000				\$1,298,000
RECONSTRUCTION: GARDINERS WOOD ROAD		4			\$169,000	\$1,973,000		\$2,142,000
ROAD RECLAMATION/MILL & OVERLAY: DIMMOCK RD		4	\$271,619					\$271,619
ROAD RECLAMATION/MILL & OVERLAY: DAYTON RD & FARGO RD		4		\$342,115				\$342,115
ROAD RECLAMATION/MILL & OVERLAY: WILLETS AVE		4			\$112,773			\$112,773
ROAD RECLAMATION/MILL & OVERLAY: CROSS ROAD-85 TO I-95		4				\$348,995		\$348,995
ROAD RECLAMATION/MILL & OVERLAY: BLOOMINGDALE RD		4					\$229,613	\$229,613
JORDAN COVE RD BRIDGE REPLACEMENT		4	\$380,000		\$2,292,000			\$2,672,000
SIDEWALK & TRAILS: BOSTON POST RD & GOSHEN ROAD		4		\$323,000				\$323,000
SIDEWALK & TRAILS: ROPE FERRY ROAD		4			\$291,000			\$291,000
SIDEWALK & TRAILS: PILGRIM & DAYTON RDS		4				\$308,000		\$308,000
SIDEWALK & TRAILS: NORMAN, VIVIAN, CROSS, SPITHEAD, GREENTREE		4					\$313,000	\$313,000
SIDEWALK & TRAILS: SHORE RD, JORDAN COVE RD		4					\$345,000	\$345,000
PARKWAY NORTH CONNECTOR		4			\$150,700	\$2,292,000		\$2,442,700
CONCRETE CURB REPLACEMENT		2		\$69,554	\$70,601	\$40,978		\$181,133
UTILITY COMMISSION								
LOGGER HILL ROAD SEWER LINE REHAB OR REPLACEMENT		4	\$0					\$0
RICHARDS GROVE ROAD PUMP STATION PARTIAL UPGRADE		4		\$422,000				\$422,000
FUTURE SSES/CMOM PROGRAM		5	\$33,000	\$33,000	\$33,000	\$33,000	\$33,000	\$165,000
LI SOUND NITROGEN REDUCTION		3	\$48,100	\$47,400	\$46,700	\$46,000	\$45,300	\$233,500
CROSS RD PUMP STATION PARTIAL UPGRADE		4		\$491,000				\$491,000
REPLACE MARILYN RD, WIEMES CT, EJECTOR STATIONS		4				\$975,000		\$975,000
STONEY BROOK PUMP STATION PARTIAL UPGRADE		4					\$511,000	\$511,000
BOLLES COURT PUMP STATION PARTIAL UPGRADE		4				\$468,000		\$468,000
HARVEY AVE PUMP STATION PARTIAL UPGRADE		4					\$446,000	\$446,000
REMAINING 17 PUMP STATIONS		4				\$672,000	\$675,000	\$1,347,000
NLWWTP CTDEP DRAFT ORDER		4					\$300,000	\$300,000
INFLOW AND INFILTRATION MITIGATION & CONTROL		4		\$319,000	\$335,000	\$352,000	\$300,000	\$1,306,000
CO-OP SEWER LATERAL EXTENSIONS		7		\$375,000	\$375,000	\$375,000	\$375,000	\$1,500,000
WASTEWATER SCADA SYSTEM UPGRADE		4		\$241,000	\$241,000	\$241,000		\$723,000
PORTABLE EMERGENCY GENERATORS, SWITCHES, ETC		5		\$150,000				\$150,000
GRAND TOTALS			\$2,955,848	\$6,109,036	\$13,113,696	\$8,485,723	\$4,762,913	\$35,427,216

FIVE YEAR CAPITAL PLAN-SUMMARY

FIRST SELECTMAN'S RECOMMENDED PLAN

SUBTOTALS: FUNDING SOURCE	FUNDING SOURCE	FY 2013	FY 2014	FY 2015	FY2016	FY 2017	FY 2013-17
CURRENT YEAR CAPITAL IMPROVEMENTS (CYC)	1	\$1,115,655	\$1,401,967	\$1,129,000	\$1,095,000	\$1,095,000	\$5,836,622
OPERATING BUDGETS	2	\$16,000	\$85,554	\$70,601	\$40,978	\$0	\$213,133
WASTE WATER BUDGET/OR SEWER CAP. MAINT. FUND	3	\$48,100	\$47,400	\$46,700	\$46,000	\$45,300	\$233,500
TRANSFER TO CAPITAL & NON-REC. (CNR)	4	\$1,441,093	\$4,016,115	\$11,459,395	\$6,895,745	\$3,214,613	\$27,026,961
SHORT AND LONG TERM DEBT FINANCING	5	\$33,000	\$183,000	\$33,000	\$33,000	\$33,000	\$315,000
LOCAL CAPITAL IMPROVEMENT PROGRAM (LoCIP) OR GRANTS	6	\$170,000	\$0	\$0	\$0	\$0	\$170,000
DESIGNATE FUNDS FROM UNDESIGNATED PROJECTS & FUND BALANCE CNR	7	\$0	\$375,000	\$375,000	\$375,000	\$375,000	\$1,500,000
EXISTING FUNDS AVAILABLE IN CNR EXPENDITURE FUND	8	\$132,000	\$0	\$0	\$0	\$0	\$132,000
GRAND TOTALS		\$2,955,848	\$6,109,036	\$13,113,696	\$8,485,723	\$4,762,913	\$35,427,216
SUBTOTALS: DEPARTMENTS							
SELECTMEN		\$1,558,100	\$1,095,000	\$1,095,000	\$1,095,000	\$1,095,000	\$5,938,100
BOARD OF EDUCATION		\$0	\$156,000	\$0	\$0	\$0	\$156,000
INFORMATION TECHNOLOGY COMMITTEE		\$35,874	\$40,650	\$144,348	\$19,250	\$0	\$240,122
ASSESSOR/FINANCE DEPT		\$75,000	\$75,000	\$75,000	\$75,000	\$75,000	\$375,000
LIBRARY		\$0	\$0	\$0	\$0	\$0	\$0
FIRE		\$241,000	\$241,000	\$125,000	\$25,000	\$0	\$632,000
POLICE DEPARTMENT		\$0	\$170,000	\$0	\$30,000	\$0	\$200,000
RECREATION AND PARKS		\$0	\$232,817	\$134,800	\$91,500	\$20,000	\$479,117
SENIOR SERVICES		\$12,155	\$0	\$0	\$0	\$0	\$12,155
YOUTH SERVICES		\$0	\$86,500	\$34,000	\$0	\$0	\$120,500
FLOOD AND EROSION CONTROL BOARD		\$0	\$0	\$100,000	\$0	\$0	\$100,000
DEPARTMENT OF PUBLIC WORKS		\$750,619	\$1,933,669	\$9,399,848	\$4,962,973	\$887,613	\$17,934,722
UTILITY COMMISSION		\$81,100	\$2,078,400	\$2,005,700	\$2,187,000	\$2,685,300	\$9,037,500
EMERGENCY MANAGEMENT		\$32,000	\$0	\$0	\$0	\$0	\$32,000
EUGENE O'NEILL THEATRE CENTER		\$170,000	\$0	\$0	\$0	\$0	\$170,000
GRAND TOTALS		\$2,955,848	\$6,109,036	\$13,113,696	\$8,485,723	\$4,762,913	\$35,427,216
		\$0	\$0	\$0	\$0	\$0	\$0

CAPITAL IMPROVEMENT PLAN FOR FY 2013-17

DEPARTMENT REQUESTS

LISTED IN DEPARTMENTAL PRIORITY ORDER

DESCRIPTION	DEPARTMENT	FUNDING SOURCE	FY 2013	FY 2014	FY 2015	FY 2016	FY 2017	FY 2013-17
FLEET MANAGEMENT	FIRST SELECTMAN	1	\$1,095,000	\$1,095,000	\$1,095,000	\$1,095,000	\$1,095,000	\$5,475,000
COHANZIE SCHOOL REMEDIATION AND DEMOLITION	ASSESSOR/FINANCE	4	\$463,100					\$463,100
REVALUATION	BOARD OF EDUCATION	4	\$75,000	\$75,000	\$75,000	\$75,000	\$75,000	\$375,000
SOUNDFIELD SYSTEM-CLARK LANE MIDDLE SCHOOL		1	\$71,000					
TIME & ATTENDANCE RECORDING SYSTEM-BOE EMPLOYEES		1		\$40,000				
VOICE OVER IP CLARK LANE MIDDLE SCHOOL		1		\$45,000				
INFORMATION TECHNOLOGY COMMITTEE								
COMPUTER TO PLATE SYSTEM-PRINT SHOP		1	\$8,500					\$8,500
POLICE TELESTAFF AUTOMATED SCHEDULING		1	\$28,750					\$28,750
PRINT SHOP FOLDER INSERTER		1		\$11,900				\$11,900
AUDIO/VISUAL UPGRADE-TOWN HALL AUDITORIUM		4		\$27,374				\$27,374
TOWN-WIDE TELECOMMUNICATION UPGRADE		4			\$144,348			\$144,348
POLICE COP LOGIC (WEB-BASED COMPLAINT)		4				\$19,250		\$19,250
FIRE SERVICE								
COMMUNITY FIRE PROTECTION IMPROVEMENTS		4		\$25,000	\$25,000	\$25,000		\$75,000
JORDAN PARKING LOT IMPROVEMENTS		4	\$80,000					\$80,000
JORDAN BUILDING DOOR REPLACEMENT		4	\$20,000					\$20,000
QUAKER HILL-BUILDING ROOF PROJECT		4		\$80,000				\$80,000
GOSHEN-BUNKROOM RENOVATIONS		4		\$20,000				\$20,000
OSWEGATCHIE-BUILDING EMERGENCY GENERATOR		4		\$30,000				\$30,000
OSWEGATCHIE-BUILDING IMPROVEMENTS		4	\$500,000	\$500,000	\$500,000			\$1,500,000
COHANZIE- BUILDING RENOVATIONS		4			\$100,000			\$100,000
PUBLIC SAFETY COMPLEX-CARPET REPLACEMENT		4		\$50,000				\$50,000
THERMAL IMAGING CAMER REPLACEMENT		2	\$16,000	\$16,000				\$32,000
FIRE STATIONS - UNDERGROUND TANK REPLACEMENT		4	\$45,000					\$45,000
POLICE								
PARKING LOT RESURFACING		4		\$170,000				\$170,000
IMPOUND STORAGE BUILDING #2		4				\$30,000		\$30,000
EMERGENCY MANAGEMENT								
UPS SYSTEM FOR COMMUNICATIONS CENTER		1	\$32,000					\$32,000
FLOOD AND EROSION CONTROL BOARD								
ALEWIFE COVE		4		\$100,000				\$100,000
SENIOR SERVICES								
REPLACEMENT CHAIRS COMMUNITY CENTER		4	\$12,155					\$12,155
YOUTH SERVICES								
LEAD ABATEMENT, EXTERIOR PAINTING & WINDOW REPLACEMENT-SECTION 2		1	\$86,500					\$86,500
HEATING & COOLING SYSTEM-SECTION 2		1			\$34,000			\$34,000
RECREATION & PARKS COMMISSION								
WATERFORD BEACH PARK: ACCESSIBLE BATHROOM		1	\$23,817					\$23,817
REPLACEMENT OF CAUSEWAY RESTROOM WFD. BEACH PARK		4		\$61,000				\$61,000
LEARY PARK IRRIGATION SYSTEM		4	\$26,000	\$24,800	\$11,500			\$62,300
RESTROOMS AT STENGER FARM PARK		4			\$30,000			\$30,000
COSPONSORED LEAGUES								
LLSOUTH-NEW LITTLE LEAGUE FIELD		4	\$50,000					\$50,000
YOUTH FOOTBALL LIGHTING IMPROVEMENTS		4		\$22,000				\$22,000
GARDINERS WOOD COMPLEX, PARKING LOT IMPROVEMENTS & LIGHTING		4			\$80,000			\$80,000
FENCING IMPROVEMENTS- LL SOUTH		4		\$50,000				\$50,000
FIELD LIGHTS - SENIOR SOFTBALL		4				\$80,000		\$80,000
PRESS BOX W/STORAGE-SENIOR SOFTBALL		4					\$20,000	\$20,000
EUGENE O'NEILL THEATRE CENTER								
SEWAGE CONNECTION ONEILL & WATERFORD BEACH		4	\$170,000					\$170,000

CAPITAL IMPROVEMENT PLAN FOR FY 2013-17

DEPARTMENT REQUESTS

FUNDING

LISTED IN DEPARTMENTAL PRIORITY ORDER

DESCRIPTION	DEPARTMENT	SOURCE	FY 2013	FY 2014	FY 2015	FY 2016	FY 2017	FY 2013-17
DEPARTMENT OF PUBLIC WORKS								
MUNICIPAL COMPLEX RENOVATIONS & CLEAN UP		4			\$6,313,774			\$6,313,774
RECONSTRUCTION: DOUGLAS LANE NO 2		4	\$99,000	\$1,199,000				\$1,298,000
RECONSTRUCTION: GARDINERS WOOD ROAD		4			\$169,000	\$1,973,000		\$2,142,000
ROAD RECLAMATION/MILL & OVERLAY: DIMMOCK RD		4	\$271,619					\$271,619
ROAD RECLAMATION/MILL & OVERLAY: DAYTON RD & FARGO RD		4		\$342,115				\$342,115
ROAD RECLAMATION/MILL & OVERLAY: WILLETS AVE		4			\$112,773			\$112,773
ROAD RECLAMATION/MILL & OVERLAY: CROSS ROAD-85 TO I-95		4				\$348,995		\$348,995
ROAD RECLAMATION/MILL & OVERLAY: BLOOMINGDALE RD		4					\$229,613	\$229,613
JORDAN COVE RD BRIDGE REPLACEMENT		4	\$380,000		\$2,292,000			\$2,672,000
SIDEWALK & TRAILS: BOSTON POST RD & GOSHEN ROAD		4	\$323,000					\$323,000
SIDEWALK & TRAILS: ROPE FERRY ROAD		4		\$291,000				\$291,000
SIDEWALK & TRAILS: PILGRIM & DAYTON RDS		4			\$308,000			\$308,000
SIDEWALK & TRAILS: NORMAN, VIVIAN, CROSS, SPITHEAD, GREENTREE		4				\$313,000		\$313,000
SIDEWALK & TRAILS: SHORE RD, JORDAN COVE RD		4					\$345,000	\$345,000
PARKWAY NORTH CONNECTOR		4			\$150,700	\$2,292,000		\$2,442,700
CONCRETE CURB REPLACEMENT		4		\$69,554	\$70,601	\$40,978		\$181,133
UTILITY COMMISSION								
LOGGER HILL ROAD SEWER LINE REHAB OR REPLACEMENT		4	\$1,200,000					\$1,200,000
RICHARDS GROVE ROAD PUMP STATION PARTIAL UPGRADE		4	\$422,000					\$422,000
FUTURE SSES/CMOM PROGRAM		5	\$33,000	\$33,000	\$33,000	\$33,000	\$33,000	\$165,000
LI SOUND NITROGEN REDUCTION		3	\$48,100	\$47,400	\$46,700	\$46,000	\$45,300	\$233,500
CROSS RD PUMP STATION PARTIAL UPGRADE		4		\$491,000				\$491,000
REPLACE MARILYN RD, WIEMES CT, EJECTOR STATIONS		4			\$975,000			\$975,000
STONEY BROOK PUMP STATION PARTIAL UPGRADE		4					\$511,000	\$511,000
BOLLES COURT PUMP STATION PARTIAL UPGRADE		4				\$468,000		\$468,000
HARVEY AVE PUMP STATION PARTIAL UPGRADE		4					\$446,000	\$446,000
REMAINING 17 PUMP STATIONS		4				\$672,000	\$675,000	\$1,347,000
NLWWTP CTDEP DRAFT ORDER		4					\$300,000	\$300,000
INFLOW AND INFILTRATION MITIGATION & CONTROL		4		\$319,000	\$335,000	\$352,000	\$300,000	\$1,306,000
CO-OP SEWER LATERAL EXTENSIONS		7		\$375,000	\$375,000	\$375,000	\$375,000	\$1,500,000
WASTEWATER SCADA SYSTEM UPGRADE		4		\$241,000	\$241,000	\$241,000		\$723,000
PORTABLE EMERGENCY GENERATORS, SWITCHES, ETC		5		\$150,000				\$150,000
GRAND TOTALS			\$5,579,541	\$6,001,143	\$13,517,396	\$8,479,223	\$4,449,913	\$38,027,216

FIVE YEAR CAPITAL PLAN-SUMMARY

DEPARTMENT REQUESTS

SUBTOTALS: FUNDING SOURCE

	FUNDING SOURCE	FY 2013	FY 2014	FY 2015	FY 2016	FY 2017	FY 2013-17
CURRENT YEAR CAPITAL IMPROVEMENTS (CYC)	1	\$1,345,567	\$1,191,900	\$1,129,000	\$1,095,000	\$1,095,000	\$5,856,467
OPERATING BUDGETS	2	\$16,000	\$16,000	\$0	\$0	\$0	\$32,000
WASTE WATER BUDGET/OR SEWER CAP. MAINT. FUND	3	\$48,100	\$47,400	\$46,700	\$46,000	\$45,300	\$233,500
TRANSFER TO CAPITAL & NON-REC. (CNR)	4	\$4,136,874	\$4,187,843	\$11,933,696	\$6,930,223	\$2,901,613	\$30,090,249
SHORT AND LONG TERM DEBT FINANCING	5	\$33,000	\$183,000	\$33,000	\$33,000	\$33,000	\$315,000
LOCAL CAPITAL IMPROVEMENT PROGRAM (LoCIP) OR GRANTS	6	\$0	\$0	\$0	\$0	\$0	\$0
DESIGNATE FUNDS FROM UNDESIGNATED PROJECTS & FUND BALANCE CNR	7	\$0	\$375,000	\$375,000	\$375,000	\$375,000	\$1,500,000
EXISTING FUNDS AVAILABLE IN CNR EXPENDITURE FUND	8	\$0	\$0	\$0	\$0	\$0	\$0
GRAND TOTALS		\$5,579,541	\$6,001,143	\$13,517,396	\$8,479,223	\$4,449,913	\$38,027,216

SUBTOTALS: DEPARTMENTS

SELECTMEN		\$1,558,100	\$1,095,000	\$1,095,000	\$1,095,000	\$1,095,000	\$5,938,100
BOARD OF EDUCATION		\$71,000	\$85,000	\$0	\$0	\$0	\$156,000
INFORMATION TECHNOLOGY COMMITTEE		\$37,250	\$39,274	\$144,348	\$19,250	\$0	\$240,122
ASSESSOR/FINANCE DEPT		\$75,000	\$75,000	\$75,000	\$75,000	\$75,000	\$375,000
LIBRARY		\$0	\$0	\$0	\$0	\$0	\$0
FIRE		\$661,000	\$721,000	\$625,000	\$25,000	\$0	\$2,032,000
POLICE DEPARTMENT		\$0	\$170,000	\$0	\$30,000	\$0	\$200,000
RECREATION AND PARKS		\$99,817	\$157,800	\$121,500	\$80,000	\$20,000	\$479,117
SENIOR SERVICES		\$12,155	\$0	\$0	\$0	\$0	\$12,155
YOUTH SERVICES		\$86,500	\$0	\$34,000	\$0	\$0	\$120,500
FLOOD AND EROSION CONTROL BOARD		\$0	\$100,000	\$0	\$0	\$0	\$100,000
DEPARTMENT OF PUBLIC WORKS		\$1,073,619	\$1,901,669	\$9,416,848	\$4,967,973	\$574,613	\$17,934,722
UTILITY COMMISSION		\$1,703,100	\$1,656,400	\$2,005,700	\$2,187,000	\$2,685,300	\$10,237,500
EMERGENCY MANAGEMENT		\$32,000	\$0	\$0	\$0	\$0	\$32,000
EUGENE O'NEILL THEATRE CENTER		\$170,000	\$0	\$0	\$0	\$0	\$170,000
GRAND TOTALS		\$5,579,541	\$6,001,143	\$13,517,396	\$8,479,223	\$4,449,913	\$38,027,216
		\$0	\$0	\$0	\$0	\$0	\$0



To: Boards, Agencies, Commissions and Administrative Staff

Date: October 17, 2011

RE: 5-YEAR CAPITAL IMPROVEMENT PLAN

FISCAL YEARS 2013-2017

Once again it is time to start the preparation of our 5-year Capital Improvement Plan. The purpose of this program is to provide the Board of Selectmen, Board of Finance, and RTM with a comprehensive multi-year plan on anticipated appropriations for Capital Improvements. This process begins now so that the plan can be completed in time for the Fiscal Year 2013 budget review.

Two factors will affect your preparation of this year's plan. First is the increased cost of healthcare on the operational budget and second is the increasing amount of the budget being dedicated to debt service associated with bonding the school construction projects. As a result we have seen significant reductions in the amount of money set aside for capital improvement funding. Over the years the Town has invested wisely in infrastructure improvements when the revenue was there. It is now time to focus our attention on maintaining these improvements so that our investment achieves or exceeds its useful life. To that end, I do not expect new initiatives unless there is a clear and measurable benefit and savings to operations.

PLAN COMPONENTS

Attached you will find the CIP for FY 2012-2016 with the actual approved funding levels for fiscal year 2012. This document shows the First Selectman's recommended plan as ratified by the Board of Selectmen. A similar document will be prepared for FY 2013-2017 for ratification by the Board of Selectmen and use by the Board of Finance and RTM. Projects to be funded in Fiscal Year 2013 should be described in detail. All projects require written justification. List all projects on the attached Project Consolidation Form by anticipated funding source and appropriation year. The attached Project Description Form is to be provided for each project. **This year projects are to be listed in priority order as determined by the Department or Agency.**

FUNDING SOURCE:

The two primary funding sources that are reviewed during the upcoming fiscal year are the Current Year Capital Improvements Account and the Capital and Non-Recurring Expenditure Fund. All capital improvements must be included in your CIP request even if they are to be funded through other sources. A list of Funding Source(s) is on the bottom of the Project Consolidation Form. **While you should indicate on the appropriate form the recommended funding source, the First Selectman is responsible for recommending the funding source.**

Projects to be included must meet the definition of a Capital Improvement. Projects funded through the Capital and Nonrecurring Expenditure Fund are required to be resubmitted for appropriation by the Board of Finance and RTM prior to project initiation. Even though projects must be resubmitted for appropriation, your justification for the project now should be no less detailed or complete. Prior to making a request for an appropriation from this fund you must submit your request to the First Selectman for a letter as to the consistency of the project with the CIP as ratified.

DEFINITION OF A CAPITAL IMPROVEMENT

NOTE: Capital Improvements will not be accepted for amounts below \$10,000. In the past this limit applied only to vehicles and equipment. For items/projects estimated below \$10,000, include these in your operating budgets. Please consult with Rudie Beers, Finance Director on the appropriate line item to use.

For purposes of this plan, a Capital Improvement shall mean one or all of the following:

1. Acquisition or lease of land.
2. All new building construction, including additions to existing buildings.
3. Reconstruction of a building for an alternative use or substantial rehabilitation of an existing building, as opposed to normal routine maintenance of an existing facility.
4. Installation, reconstruction, extension, or improvement of new or existing roads, bridges, drainage structures, flood control projects, sewer lines, water lines, or other public utilities.
5. Creation of new, expansion of or improvement to, existing outdoor uses of land and coastal waters including, recreational facilities, parking facilities, accessory facilities, water quality improvements and cove dredging.
6. All necessary architectural, engineering and feasibility analysis related to a planned Capital Improvement as defined in 1-5 above.
7. Equipment in excess of \$10,000 that is not governed by the Fleet Management Plan.
8. Information technology (IT) software and equipment.

OTHER REQUIREMENTS:

1. Each department has received from the Director of Finance a request to update their vehicle information so that a final FY 2013-2017 **Fleet Management Plan** can be prepared. Each department will get a copy of the final updated plan. The Department's vehicles/equipment replacement plan will be included as part of your CIP submission. If the Department is requesting any variation from what is in the plan it **must** be justified in a written narrative. If there are no variations, each Department must acknowledge their intent to replace vehicles/equipment in FY12 in accordance with the Plan and submit documentation to justify the scheduled replacement in accordance with the Board of Finance guidelines.
2. All computer hardware and software must be submitted through the Information Technology Committee. Contact Rudie Beers, Chairman of the IT Committee.
3. Submission of a project for inclusion into the CIP does not constitute a budget submission for FY 2013. Contact Rudie Beers, Finance Director regarding requirements for submission of a capital project to be funded from an operating budget, the Current Year Capital Improvement Budget, or the Capital and Nonrecurring Expenditure Fund.
4. Your submission does not fulfill your agency's requirement to submit to the Planning and Zoning Commission a separate request for a municipal improvement report pursuant to CGS 8-24. Contact Tom Wagner, Planning Director at 444-5813 on this process.

SUBMISSION DEADLINE:

In order to allow adequate time for review, **submit by email in Word format to my office by November 10, 2011.** If you cannot submit in digital form (6) paper copies will be required (no staples please). **These forms are located on the G Drive under budget forms and are in Word format.** If you do not have any items which require submission please confirm this in writing by November 10, 2011.

Sincerely,

Daniel M. Steward
First Selectman

DISTRIBUTION LIST CAPITAL IMPROVEMENT PROGRAM

Board of Selectmen

Moderator, Representative Town Meeting

Majority Leader, Representative Town Meeting

Minority Leader, Representative Town Meeting

Chairman, Board of Finance

Chairman, Board of Education

Treasurer

Finance Director

Human Resources Director

Superintendent of Schools

Youth Service Director

Fire Administrator, Fire Commission

Flood & Erosion Control Board

Library Director

Police Chief / Emergency Management Director

Public Works Director

Recreation and Parks Director

Senior Services Director

Assessor

Tax Collector

Town Clerk

Chief Engineer, Utility Commission

Director of Building and Grounds, Board of Education

Business Manager, Board of Education

Planning Director

Eugene O'Neil Theater Center

CAPITAL IMPROVEMENT PLAN
PROJECT DESCRIPTION FORM

A) AGENCY:

B) PROJECT NAME:

C) CONTACT PERSON:

D) DEPARTMENT'S PRIORITY: #

E) DESCRIPTION:

- a) Describe the type, purpose, and anticipated accomplishments of the project.
- b) Indicate the progress to date on the project.
- c) List other projects it is coordinated with whether in your Department or with other Departments.
- d) Describe the affect the project will have on your annual Department expenditures:
- e) Attach plan, estimate, service area map and/or other support documentation.

CAPITAL IMPROVEMENT PLAN: PROJECT CONSOLIDATION FORM-FY2013-2017							
DEPARTMENT/AGENCY:							
PROJECT NAME: IN ORDER OF DEPT. PRIORITY	S O U R C E	FY-2013	FY-2014	FY-2015	FY-2016	FY-2017	TOTAL FY 2013-2017
1							
2							
3							
4							
5							
6							
7							
8							
9							
TOTAL							

INDEX TO FUNDING SOURCES,

1 = CURRENT YEAR CAPITAL IMPROVEMENTS

2 = OPERATING BUDGETS,

3 = WASTE WATER BUDGET/SEWER CAPITAL MAINTENANCE FUND

4 = TRANSFER TO CAPITAL & NONRECURRING.

5 = SHORT AND LONG TERM DEBT FINANCING

6 = LOCAL CIP & OTHER GRANTS

7 = DESIGNATE FUNDS FROM UNDESIGNATED PROJECTS CNR;

8 = FUND FROM EXISTING DESIGNATIONS CNR

Capital Improvement Plan
Project Description Form

- A) AGENCY:** Assessor/Finance Dept.
- B) PROJECT NAME:** Revaluation 2017(capital non-recurring)
- C) CONTACT PERSON-** Michael Bekech, Assessor
- D) DEPARTMENTS PRIORITY # 1**
- E) DESCRIPTION:** Revalue all town properties for purposes of meeting State Statutes. Based on current bids for revaluation process, costs have moved to \$40.00 per parcel range for a full revaluation and \$25,000 for an update. Additional appraisals such as Millstone Nuclear Power Station bring the totals to the values as noted. The legislature in Summer 2004 changed the revaluation cycle that now has the next Revaluation Date scheduled for 10/1/2017. This funding does not include the added money for the Millstone appraisal for the 2017 Revaluation but it does begin the funding for the 2017 Revaluation.
- F) Request**
- | | |
|-----------------|----------------------------------|
| FY 12/13 | \$75,000 |
| FY 13/14 | \$75,000 |
| FY 14/15 | \$75,000 |
| FY 15/16 | \$75,000 |
| FY 16/17 | \$75,000 Revaluation date |
- F) History**
- | | |
|-----------------|---|
| FY03/4 | \$ 75,000 (approved) |
| FY04/05 | \$ 75,000 (approved) |
| FY05/06 | \$ 75,000 (approved) |
| FY06/07 | \$ 75,000 (approved) |
| FY 07/08 | \$ 75000 - (approved) Revaluation Date |
| FY 08/09 | \$ 75,000 (approved) |
| FY 09/10 | \$ 75,000 (approved) |
| FY 10/11 | \$75,000 (approved) |
| FY 11/12 | \$75,000 (approved) Revaluation Date |

CAPITAL IMPROVEMENT PLAN: PROJECT CONSOLIDATION FORM-FY2013-2017							
DEPARTMENT/AGENCY: Board of Education, Waterford Public Schools							
PROJECT NAME: IN ORDER OF DEPT. PRIORITY	SOURCE	FY-2013	FY-2014	FY-2015	FY-2016	FY-2017	TOTAL FY 2013-2017
1 Soundfield System for Clark Lane Middle School	1	71,000					71,000
2 Time and Attendance Recording System for Board of Education Employees	1		40,000				40,000
3 Voice Over I.P. Phone System for Clark Lane Middle School	1		45,000				45,000
4							
5							
6							
7							
8							
9							
TOTAL		71,000	85,000				156,000

INDEX TO FUNDING SOURCES,

1= CURRENT YEAR CAPITAL IMPROVEMENTS

2 = OPERATING BUDGETS,

3 = WASTE WATER BUDGET/SEWER CAPITAL MAINTENANCE FUND

4 = TRANSFER TO CAPITAL & NONRECURRING.

5 = SHORT AND LONG TERM DEBT FINANCING

6 = LOCAL CIP & OTHER GRANTS

7 = DESIGNATE FUNDS FROM UNDESIGNATED PROJECTS CNR;

8 = FUND FROM EXISTING DESIGNATIONS CNR

CAPITAL IMPROVEMENT PLAN
PROJECT DESCRIPTION FORM

A) AGENCY: Board of Education, Waterford Public Schools

B) PROJECT NAME: Soundfield System for Clark Lane Middle School.

C) CONTACT PERSON: Ronald Melnik, Director of Finance and Operations

D) DEPARTMENT'S PRIORITY: # 1

E) DESCRIPTION:

- a) Describe the type, purpose, and anticipated accomplishments of the project.
- b) Indicate the progress to date on the project.
- c) List other projects it is coordinated with whether in your Department or with other Departments.
- d) Describe the affect the project will have on your annual Department expenditures:
- e) Attach plan, estimate, service area map and/or other support documentation.

Installation of a "Soundfield System" in 47 Classrooms at Clark Lane Middle School.

CAPITAL IMPROVEMENT PLAN
PROJECT DESCRIPTION FORM

A) AGENCY: Board of Education, Waterford Public Schools

B) PROJECT NAME: Time and Attendance System for BOE Employees.

C) CONTACT PERSON: Ronald Melnik, Director of Finance and Operations

D) DEPARTMENT'S PRIORITY: # 1

E) DESCRIPTION:

- a) Describe the type, purpose, and anticipated accomplishments of the project.
- b) Indicate the progress to date on the project.
- c) List other projects it is coordinated with whether in your Department or with other Departments.
- d) Describe the affect the project will have on your annual Department expenditures:
- e) Attach plan, estimate, service area map and/or other support documentation.

Purchase of software and hardware to collect and report Time and Attendance for Board of Education employees. System will integrate with a financial package.

CAPITAL IMPROVEMENT PLAN
PROJECT DESCRIPTION FORM

A) AGENCY: Board of Education, Waterford Public Schools

B) PROJECT NAME: Voice-Over I.P. Phone System for CLMS

C) CONTACT PERSON: Ronald Melnik, Director of Finance and Operations

D) DEPARTMENT'S PRIORITY: # 3

E) DESCRIPTION:

- a) Describe the type, purpose, and anticipated accomplishments of the project.
- b) Indicate the progress to date on the project.
- c) List other projects it is coordinated with whether in your Department or with other Departments.
- d) Describe the affect the project will have on your annual Department expenditures:
- e) Attach plan, estimate, service area map and/or other support documentation.

To upgrade the phone system at Clark Lane Middle School to a Voice-Over I.P.



Waterford Office of Emergency Management

204 BOSTON POST ROAD • WATERFORD, CT • 06385 • (860) 442-9585 • FAX (860) 443-5327

November 18, 2011

Mr. Daniel Steward, First Selectman
Town of Waterford
15 Rope Ferry Road
Waterford, CT 06385

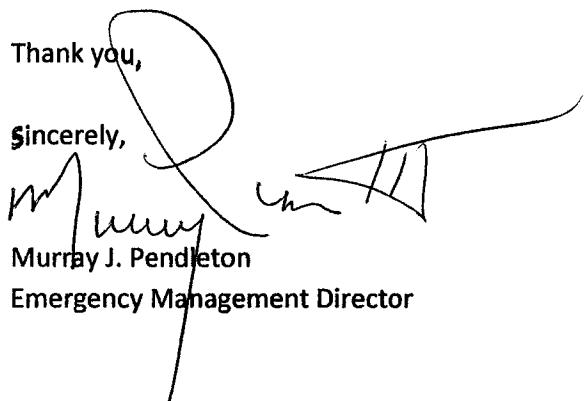
RE: Waterford Emergency Management
Capital Improvement Plan: FY 2013-2017

Dear Mr. Steward:

Attached please find the Waterford Emergency Management's Capital Improvement Plan request for FY 2013-2017, for your review and consideration.

Thank you,

Sincerely,


Murray J. Pendleton
Emergency Management Director

CAPITAL IMPROVEMENT PLAN: PROJECT CONSOLIDATION FORM-FY2013-2017

DEPARTMENT/AGENCY:

PROJECT NAME: IN ORDER OF DEPT. PRIORITY	S O U R C E						TOTAL FY 2013-2017
		FY-2013	FY-2014	FY-2015	FY-2016	FY-2017	
1. UPS System for the Communications Center	1	\$32,000					
2							
3							
4							
5							
6							
7							
8							
9							
TOTAL							

INDEX TO FUNDING SOURCES,

1= CURRENT YEAR CAPITAL IMPROVEMENTS

2 = OPERATING BUDGETS,

3 = WASTE WATER BUDGET/SEWER CAPITAL MAINTENANCE FUND

4 = TRANSFER TO CAPITAL & NONRECURRING.

5 = SHORT AND LONG TERM DEBT FINANCING

6 = LOCAL CIP & OTHER GRANTS

7 = DESIGNATE FUNDS FROM UNDESIGNATED PROJECTS CNR;

8 = FUND FROM EXISTING DESIGNATIONS CNR

CAPITAL IMPROVEMENT PLAN
PROJECT DESCRIPTION FORM

A) AGENCY: Emergency Management

B) PROJECT NAME: UPS System for the Communications Center

C) CONTACT PERSON: Chief Pendleton

D) DEPARTMENT'S PRIORITY: # ASAP

E) DESCRIPTION:

- a) Describe the type, purpose, and anticipated accomplishments of the project.
- b) Indicate the progress to date on the project.
- c) List other projects it is coordinated with whether in your Department or with other Departments.
- d) Describe the affect the project will have on your annual Department expenditures:
- e) Attach plan, estimate, service area map and/or other support documentation.

On November 17, 2011 the Waterford Emergency Management Department received notification that our current service contract for our UPS Communication Center System is not eligible for a preventative maintenance contract renewal. The system which is in excess of 10 years old and outdated contains parts which are no longer in production. It should be clear in the event of a power failure the age and condition of this equipment could clearly have an effect on the ability of the communications system and services to continue uninterrupted. The difference between a UPS and the emergency generator is the UPS allows the system to continue until such time that the emergency generator supplies power to the system.

Although our initial intent was to cover this system with what is referred to as the On-site Gold Plan Plus the company simply will not provide such coverage. In light of the fact that this is a critical piece of equipment we must plan to replace it as soon as possible in order to qualify for the On-site Gold Plan Plus. This plan will ultimately provide a reasonable service contract after the warranty expires.

We've considered 2 basic options and recommend the one that is best suited for Waterford is Option 1 with 30kVA UPS Extended Battery Cabinet (EBC72) and 3 Breaker Wall Mounted Maintenance Bypass Switch. (See attachment).

The amount requested, although not final because of freight and installation fee, is approximately \$32,000. Thank you for the consideration for this project.

R.M. CLARK ASSOCIATES, INC.

Electrical Manufacturers' Representative
87 E Silver Street, Westfield, MA 01085
(413) 642-3574 Fax (413)-895-0093

QUOTATION

Page: 1
Number: AK11321
Date: November 17, 2011
Prepared by: Andy Koch
Phone: 413-642-3574
Email: akoch@rmclark.com
Quote valid for: 30 Days

Mr. Tom Demebeck
City of Waterford

Subject: Eaton Powerware UPS Proposal

Dear Tom

R.M. Clark Associates, Inc. is pleased to provide you with our proposal to meet your Uninterruptible Power Systems requirements. We appreciate the opportunity to present a solution to your needs that offers comprehensive power protection, maximum efficiency, and unmatched performance.

We have included for your review an overview of the Model 9355's valuable benefits, along with comprehensive information on Powerware's ability to support your needs now and in the future.

Option 1

Powerware Model 9355, 30 KVA, 208/208V, 3 Phase UPS with 11 Minute Internal Battery, SNMP Communications Card, 5x8 Start-up Service, One (1) Year Parts and Labor Warranty.....\$13,069.00

Option 2 (Same size as present system)

Powerware Model 9355, 20 KVA, 208/208V, 3 Phase UPS with 18 Minute Internal Battery, SNMP Communications Card, 5x8 Start-up Service, One (1) Year Parts and Labor Warranty.....\$12,183.00

Optional Equipment

30 kVA UPS Extended Battery Cabinet (EBC36) will increase Battery runtime to 31 Min.
\$4,179.00

30 kVA UPS Extended Battery Cabinet (EBC72) will increase Battery runtime to 56 Min.
\$6,377.00

20 kVA UPS Extended Battery Cabinet (EBC36) will increase Battery runtime to 56 Min.
\$4,179.00

20 kVA UPS Extended Battery Cabinet (EBC72) will increase Battery runtime to 82 Min.
\$6,377.00

Three (3) Breaker Wall Mounted Maintenance Bypass Switch
\$1,950.00

(All pricing does not include Tax, Freight and Installation charges)

CAPITAL IMPROVEMENT PLAN: PROJECT CONSOLIDATION FORM-FY2013-2017

DEPARTMENT/AGENCY: FIRE SERVICE

PROJECT NAME:	S O U R C E	FY-2013	FY-2014	FY-2015	FY-2016	FY-2017	TOTAL FY2013-2017
1 BOFC - Community Fire Protection Improvements	4	-	\$25,000	\$25,000	\$25,000	-	\$75,000
2 Jordan – Parking lot Replacement	4	\$80,000	-	-	-	-	\$80,000
3 Jordan – Building Door Replacement	4	\$20,000	-	-	-	-	\$20,000
4 Quaker Hill – Building Roof Project	4	-	\$80,000	-	-	-	\$80,000
5 Goshen – Bunkroom Renovations	4	-	\$20,000	-	-	-	\$20,000
6 Oswegatchie – Emergency Generator	4	-	\$30,000	-	-	-	\$30,000
7 Oswegatchie – Building Improvements	4	\$500,000	\$500,000	\$500,000	-	-	\$1,500,000
8 Cohanzie – Building Renovations	4	-	-	\$100,000	-	-	\$100,000
9 Public Safety Complex – Carpet Replacement	4	-	\$50,000	-	-	-	\$50,000
10 Thermal Imaging Camera Replacement	2	\$16,000	\$16,000	-	-	-	\$32,000
11 Fire Stations - Underground Tank Improvements	4	\$45,000	-	-	-	-	\$45,000
TOTAL		\$661,000	\$741,000	\$600,000	\$25,000	-	\$2,027,000

CAPITAL IMPROVEMENT PLAN
PROJECT DESCRIPTION FORM

A) AGENCY: Fire Service

B) PROJECT NAME: Jordan – Parking Lot Replacement

C) CONTACT PERSON: Timothy Sullivan, Chief

D) PROJECT CATEGORY:

Facilities Improvements	<input checked="" type="checkbox"/>	Apparatus, Vehicle & Equipment Replacement	
Building Improvements	<input type="checkbox"/>	Highway Construction & Improvements	
Land Acquisition	<input type="checkbox"/>	Sidewalks & Trails	
Road Reconstruction-Sewer Extensions	<input type="checkbox"/>	Schools	
Water System	<input type="checkbox"/>	Sewerage System	

E) DESCRIPTION:

This request is to remove the existing parking lot surface, prep the surface and reapply a new parking lot surface. The existing parking lot surface is beyond repair and is very poor. This is a very high traffic flow parking lot and the condition that exists is very unstable. There are sink holes and most of the surface in the rear has just crumbling.

The attached quotes from American USA Paving and B&W Paving were obtained for pricing on this project.



**PROPOSAL-CONTRACT
B&W PAVING & LANDSCAPING, LLC**

NAME: Waterford Fire House	EMAIL: TRC18@snet.net
ADDRESS: Jordan Village, Waterford, CT 06385	ATTENTION: Tim Condon
ADDRESS:	JOB LOCATION: Fire House @ Jordan Village
PHONE: 860-442-0888	DATE: November 16, 2011

INCLUDES:

Reclaim 19,000 SQ
Dispose of excess debris
Grade and compact with attention to drainage
Pave 2 in. class one binder
Pave 2 in. class one surface

Remove and replace 508 LF of asphalt curbing

Excavate for concrete slab, 2,293 SF
Install 8" processed gravel
Grade and compact
Form and pour 8" thick concrete slab with wire

TOTAL COST: \$78,600

- THIS CONTRACT CONSTITUTES THE ENTIRE AGREEMENT BETWEEN PURCHASER AND B & W PAVING & LANDSCAPING, LLC.
- B & W IS NOT RESPONSIBLE FOR ANY DELAYS BEYOND OUR CONTROL, I.E. FIRE, FLOODS, WEATHER, ACCIDENTS, STRIKES, WAR AND OTHER SIMILAR CAUSES.
- WHEN EXTRA WORK IS REQUIRED, OUTSIDE OF THE SCOPE OF THE CONTRACT, REASONABLE COMPENSATION IS NECESSARY AND UNDERSTOOD. THIS WILL BE DISCUSSED WITH OWNER OR GENERAL CONTRACTOR IF THE SITUATION ARISES.
- IF B & W DOES NOT GRADE THE BASE, WE RESERVE THE RIGHT TO INSPECT SAID BASE AND DECLARE IT UNSUITABLE FOR ASPHALT. THE PURCHASER AND B & W WILL DISCUSS THE SITUATION AND DECIDE HOW IT CAN BEST BE REMEDIED. IF B & W DOES ADDITIONAL WORK TO BASE, NOT INCLUDED IN CONTRACT, AN ADDITIONAL CHARGE WILL BE INCURRED.
- B&W NOT RESPONSIBLE FOR DRAINAGE ISSUES IF GRADE IS LESS THAN 1.5%
- ANY CLAIMS FOR DEFECTIVE MATERIALS OR IMPROPER WORKMANSHIP SHALL BE WAIVED UNLESS NOTICE IS GIVEN WITHIN 10 BUSINESS DAYS.
- FAILURE OF PURCHASER TO MAKE PAYMENTS AS AGREED WILL RELEASE B & W FROM ANY FURTHER PERFORMANCE UNDER THIS CONTRACT UNTIL SITUATION IS REMEDIED TO THE SATISFACTION OF B&W.

B&W Paving & Landscaping, LLC * PO Box 70 * Mystic, CT. 06355
Phone 860-572-9942 * Fax 860-536-5833 * Member: BBB

- WORK WILL BE PERFORMED DURING PERIODS OF PREMIUM OR OVERTIME WAGES, SUCH AS SATURDAYS, SUNDAYS AND HOLIDAYS, ONLY AT B & W'S OPTION. IF SUCH WORK IS REQUESTED BY PURCHASER, REASONABLE COMPENSATION WILL BE REQUIRED FOR THE EXTRA INCURRED COST.
- B & W WILL NOT BE RESPONSIBLE FOR DEFECTS IN PAVING RESULTING FROM ADVERSE WEATHER CONDITIONS, OR FROM INADEQUATE BASE PREPARATION (IF NOT DONE BY B & W). B & W RESERVES THE RIGHT TO REFRAIN FROM PAVING DURING WHAT IT CONSIDERS TO BE ADVERSE WEATHER CONDITIONS.

THIS SIGNED PROPOSAL / CONTRACT MUST BE MADE AN ADDENDUM TO ANY
SUBCONTRACTOR AGREEMENT.

ALL PRICES SUBJECT TO 6% SALES TAX UNLESS EXEMPT, EXEMPTION CERTIFICATE REQUIRED

NOTE:

UNLESS OTHERWISE SPECIFIED IN ABOVE SCOPE OF WORK, PRICE DOES NOT INCLUDE TRAFFIC CONTROL, FEES, PERMITS, OR ENGINEERING. ANY FEES INCURRED TRYING TO COLLECT THIS DEBT WILL BE THE RESPONSIBILITY OF CUSTOMER.

NAME:

TITLE:

COMPANY:

DATE:

TERMS AND CONDITIONS OF SALE ARE HEREBY ACCEPTED



AMERICAN U★S★A PAVING



(860) 347-PAVE
(860) 537-6887
(860) 659-3000
George Lee
CT Lic. #00539834

Contract

25 Toll Gate Road
South Glastonbury, CT 06073

Page No. _____ of _____ Pages

PROPOSAL SUBMITTED TO Fire House		PHONE 860-867-6203	DATE 11-1-11
STREET 89 Rope ferry rd.		JOB NAME Timothy Condon	
CITY, STATE AND ZIP CODE Waterford		JOB LOCATION Price is only good this yr	
STARTING DATE	COMPLETION DATE	JOB PHONE	

We hereby propose to furnish materials and labor necessary for the completion of:

- Resurface 2"-3". *two layers*
- Pave over processed gravel 2"-3".
- Processed gravel 2"-6".
- All work done by machine.
- Rip out old blacktop where needed. *All parking lot*
- Dig out bad areas.
- Repair area.
- Seal coating.
- Tack coat edge/foundation.
- One year warranty against pot holes.
- Tack coating driveway.
- Weed killer.
- Excavating 2" 3"

if you want all areas paved 75,000

Make checks payable to your salesman.

No out of state checks accepted.

Measurements	
<u>34</u>	<u>34</u>
<u>39</u>	<u>18</u>
<u>1326</u>	<u>612</u>
<u>2482</u>	<u>544</u>
ft	ft
of concert work	
extra 15,500	
8 inches	
Approx. Sq. Ft. 21,122	

WE PROPOSE hereby to furnish material and labor - complete in accordance with above specifications, for the sum of:

Payment to be made as follows: _____ dollars (\$ _____).

Half Down _____

Balance on completion. _____

All material is guaranteed to be as specified. All work to be completed in a workmanlike manner according to standard practices. There will be an extra charge for any alteration or additional work done above and beyond the aforementioned contract. All additional work agreements whether verbal or in writing will be subject to an extra charge and must be paid in full on or before the job completion. Additionally, in the event of a job delay due to unforeseen circumstances such as weather conditions or unstable ground leading to an extension of time for job start or completion, then there is an automatic agreement by and between the parties that the contract execution date will be extended by the time attributable to such circumstances. Upon completion of the job no warranty on water flow or standing water or fire marks or trees or vegetation growth.

Authorized Signature _____

Acceptance of Proposal - The above prices, specifications and conditions are satisfactory and are hereby accepted. American USA Paving is authorized to do the work as specified. Payment will be made as agreed above. If you, the customer, default, customer agrees to pay all costs of collection, including reasonable attorney fees and interest at the rate of 2.8% per month on the unpaid balance, in addition to other charges incurred by contractor. Customer to carry fire, tornado and other necessary insurance.

Customer Signature _____

YOU, THE BUYER, MAY CANCEL THIS TRANSACTION AT ANY TIME PRIOR TO MIDNIGHT OF THE THIRD BUSINESS DAY AFTER THE DATE OF THIS TRANSACTION. SEE THE ATTACHED NOTICE OF CANCELLATION FORM FOR AN EXPLANATION OF THIS RIGHT. (SATURDAY IS A LEGAL BUSINESS DAY IN CONNECTICUT).

Date of Acceptance Signature _____ Customer Signature _____

CAPITAL IMPROVEMENT PLAN
PROJECT DESCRIPTION FORM

A) AGENCY: Fire Service

B) PROJECT NAME: Jordan – Exterior Door Replacement

C) CONTACT PERSON: Timothy Sullivan, Chief

D) PROJECT CATEGORY:

Facilities Improvements	Apparatus, Vehicle & Equipment Replacement	
Building Improvements	X Highway Construction & Improvements	
Land Acquisition	Sidewalks & Trails	
Road Reconstruction-Sewer Extensions	Schools	
Water System	Sewerage System	

E) DESCRIPTION:

This request is to remove the existing exterior steel doors and replace with new steel doors. The existing doors are very rusted and do not properly function. One of these doors is in such poor condition that the door cannot open, so it has been secured and locked.

The attached quote from Lou Nassetta was obtained for pricing on this project.

Lou Nassetta

11 NORTH ROAD
WATERFORD, CT 06385
Tel: (860) 608-8975

November 10, 2011

Jordan Firehouse
Ropeferry Road
Waterford, CT 06385
ATTN: Jason Cioci

RE: Door Replacement

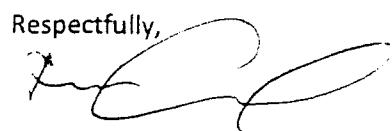
SUBJECT: COST PROPOSAL

SCOPE OF WORK: Provide labor, material and equipment to replace 4 passage doors.

COST:	Door #1	Material	\$2,885.70
		Labor	\$1,275.00
	Door #2	Material	\$2,460.84
		Labor	\$1,100.00
	Door #3	Material	\$2,885.70
		Labor	\$1,275.00
	Door #4	Material	\$2,885.70
		Labor	\$1,275.00

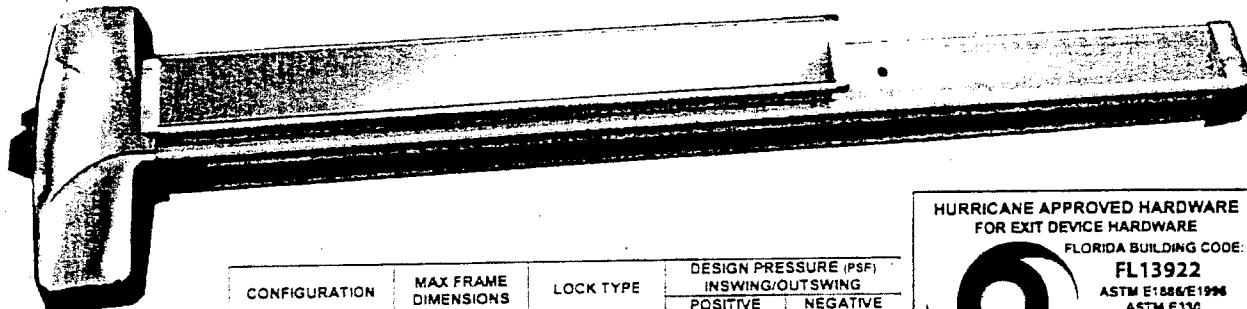
Thank you for your consideration. If I can be of further service, or to schedule work, please contact me at **860-608-8975**.

Respectfully,



Lou Nassetta

**7700 & F7700 Rim Type Panic And Fire Exit Devices
7760 & F7760 Vertical Rod Panic And Fire Exit Devices
ANSI A156.3, Grade 1**



CONFIGURATION	MAX FRAME DIMENSIONS	LOCK TYPE	DESIGN PRESSURE (PSF) INSWING/OUTSWING	
			POSITIVE	NEGATIVE
SINGLE	40" x 38"	7700V Series	+70.0	-70.0
DOUBLE	76" x 38"	7700V Series	+70.0	-70.0

**HURRICANE APPROVED HARDWARE
FOR EXIT DEVICE HARDWARE**

FLORIDA BUILDING CODE:
FL13922
ASTM E1886/E1996
ASTM E330
TAS 201, 202, 203
UL108
UL10C

Features & Construction

- ◆ For use in institutions, schools, universities, commercial buildings where heavy duty reliable operation is a requirement.
- ◆ ANSI A156.3, Grade 1 heavy duty exit device in surface vertical rod and rim designs.
- ◆ Rail assemblies are heavy duty extruded aluminum body with stainless steel push pad and end caps are made of brass, bronze or stainless steel. Rail travels in direction of the door swing.
- ◆ Trims are thru bolted for additional security.
- ◆ Conforms & Meets ADA Regulations.
- ◆ UL Listed for Panic and UL10C, UBC 7-2-1777 codes.
- ◆ UL Listed for Fire Exit Hardware, 3 hours.
- ◆ (A) label rating on pairs of doors up to 8'0" x 8'0" swinging in same or opposite directions.
- ◆ Uncompromising security and safety in single and double fire exit doors.
- ◆ Chassis mounted unit construction. Rim and vertical rod devices are non-handed and easily reversible.
- ◆ Available in many different functions. Lever and escutcheon trims are grade 1 construction with clutch free-wheeling mechanism. Rose and escutcheon covers 161 cut out

Suggested Architect's Specifications

- ◆ All exit devices shall be of touch bar design with smooth operation, and be operative over $\frac{2}{3}$ of the door's clear opening width.
- ◆ All exit devices must be listed under "Panic Hardware" in the Accident Equipment List of Underwriters' Laboratories, Inc. Where labeled doors are used as exits, they must be equipped with labeled Fire Exit Hardware and UL10C. UBC-7-2-1777 codes..
- ◆ All springs shall be of stainless steel throughout.
- ◆ All exit devices shall be of chassis mounted unit construction.
- ◆ All exit devices shall be ANSI A1563.3, Grade 1.

**MEETS
The Buy American Act**

Lifetime Warranty

7700 & F7700 Series •

Rim Type Exit Device

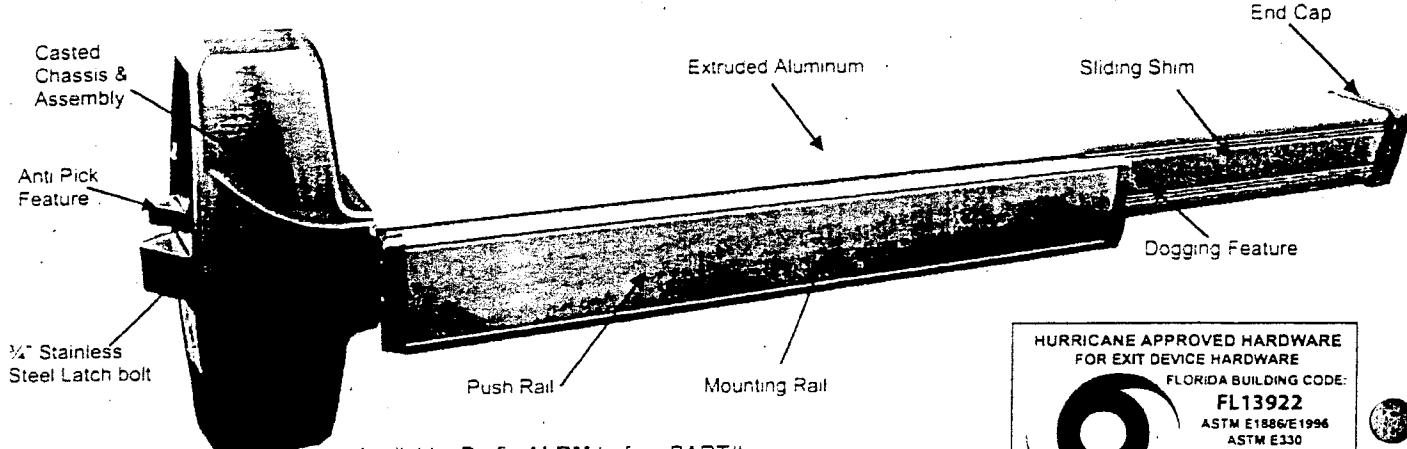


3 HOUR FIRE RATED
UL10C, UBC7-2-1997



Specifications

- For Doors**..... 1 $\frac{3}{4}$ " thick standard optional 2 $\frac{1}{2}$ " thick. 4 $\frac{1}{2}$ " minimum stile width for single or double door application using standard 938 strike and pull lever, thumbpiece and escutcheon trims.
- Chassis**..... Non-ferrous alloy (Panic Grade), steel (Fire Grade).
- Cover**..... Brass, bronze or stainless steel. Covers stock hollow metal doors with 161 cut out.
- Mounting**..... Furnished with wood and machine screws. Available thrubolts on request. Thru bolts are included and standard packaging on fire rated devices.
- End Cap**..... Brass, bronze or stainless steel construction.



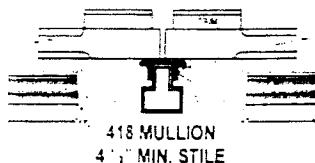
- Alarm**..... Available. Prefix ALRM before PART#.
- Hand**..... Non-handed, center hub.
- Latch Bolt**..... Stainless steel $\frac{3}{4}$ " throw with anti-pick feature.
- Spring**..... Stainless steel throughout.
- ANSI Standard**..... A156.3, Grade 1.
- Rail Assembly**..... Heavy gauge extruded aluminum body with stainless steel push pad.
- Strike**..... 938 packed standard (See strike page).
- Dogging Feature**..... Allen-type key furnished standard on non-fire labeled devices.
- Mullion**..... Available.
- UL and ULC Listed**..... 7700 Series for panic. F7700 Series for fire exit hardware for all types of 4' x 8' single doors and 8' x 8' double doors for up to 3 hour fire labeled installations. conforms to standards UL10C and UBC 7-2-1777 codes
- Finish**..... Architectural plated finishes.



Lifetime Warranty

Minimum Stile Requirements
4" escutcheon trims & 4 $\frac{1}{2}$ " for thumbpiece and escutcheon trims

DOUBLE DOOR



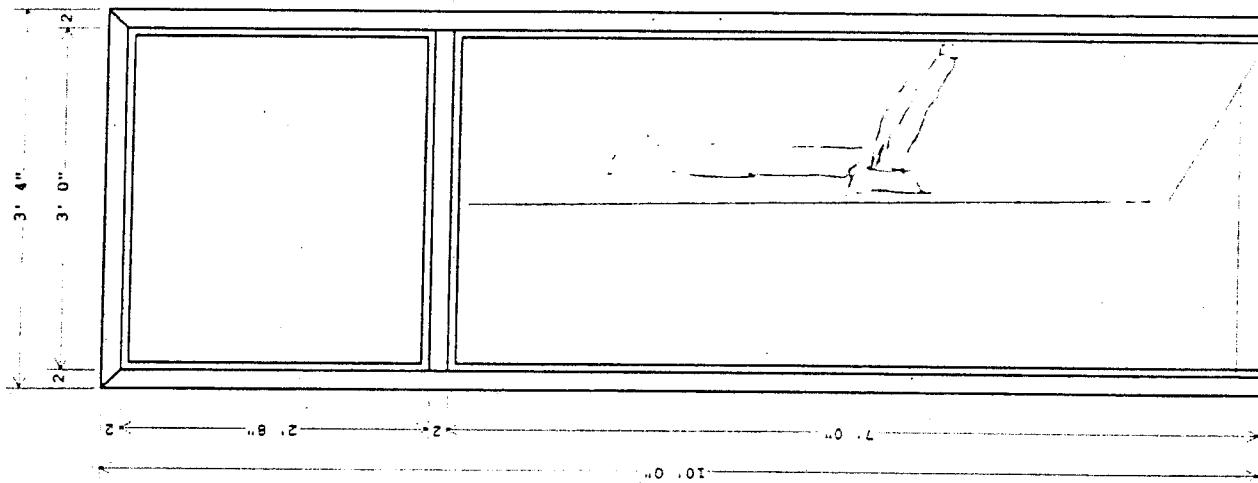
424 MULLION
4 1/2" MIN. STILE

SINGLE DOOR



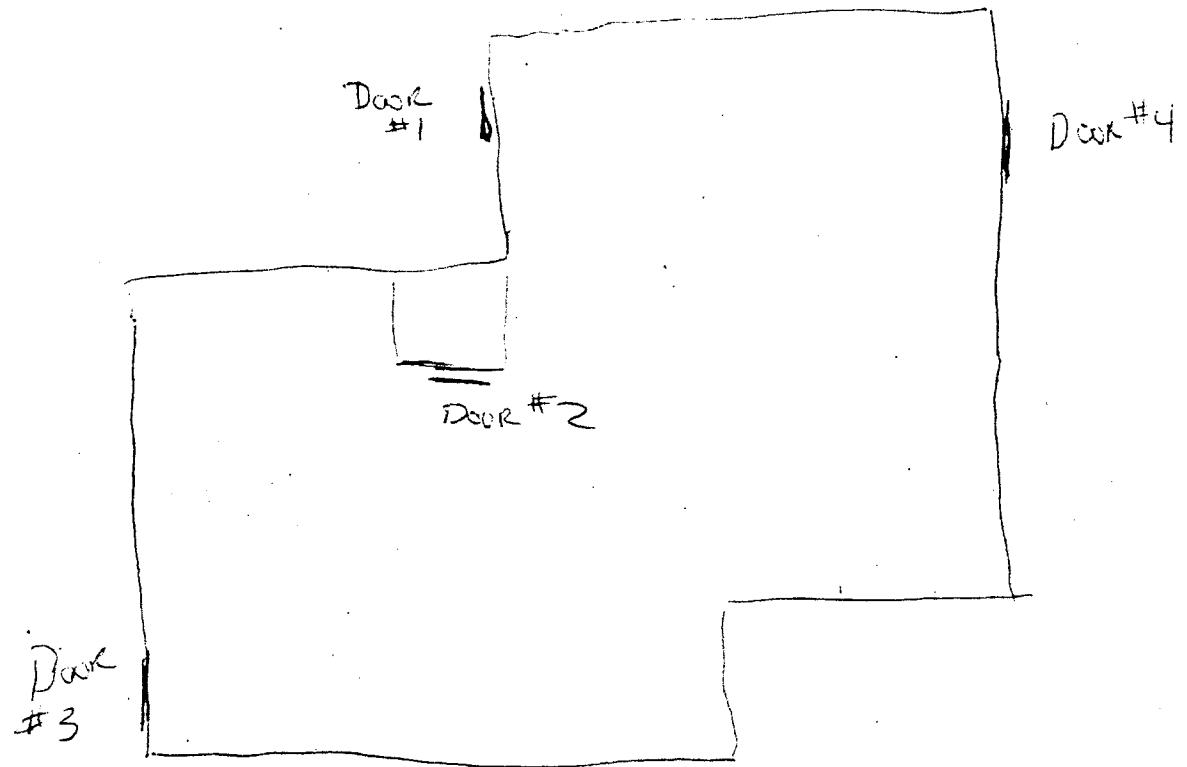
DIMENSIONS

Touchbar height to finished floor	41" (1034mm) at center		
Touchbar projection			
neutral	3 $\frac{5}{8}$ " (92mm)		
depressed	2 $\frac{1}{8}$ " (73mm)		
Center case	8 $\frac{1}{4}$ " x 2 $\frac{3}{4}$ " x 2 $\frac{1}{2}$ " (205mm x 68mm x 62mm)		
MODEL	ACTUAL LENGTH	MAX CUT DOWN	DOOR SIZE
7700EO36 and F7700EO36	32"	6"	30" to 36" door width
7700EO48 and F7700EO48	44"	10"	38" to 48" door width



Park Roway Inc.	Project: Jordan File House
51 Route 32 - P.O. Box 321	Elevation, Opening #1 Transom Lite Frame
Quaker Hill, CT 06375-0321	Drawn by: William H. Terry A.H.C
860 442-8593	Date: 10/28/11 16 11
	Scale: 1' 7" = 1"
	Code: STRAT
	Code: 1TL
	Page 1

Door LEGEND

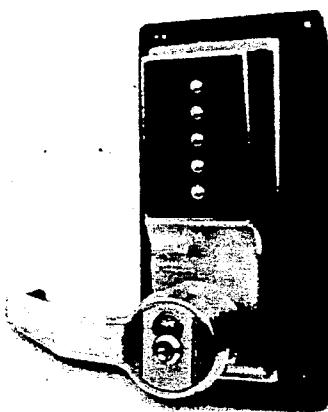


RTE 156





Simplex
LP1000 Series



Features

- PIN Access
- Mechanical Pushbutton
- Single Access Code
- Exit Trim
- Lever

Access Control:

Mechanical pushbutton lock—eliminates problems and costs associated with issuing, controlling, and collecting keys and cards. Provides exterior access by combination, while allowing free egress with exit device (not supplied).

No Battery:

Fully mechanical lock eliminates the material and labor expense of battery replacements

Locking Device Options:

Rim Exit Device: Compatible with most leading brands of exit devices—see list below (Exit Device and Mounting hardware not included)

* For the most accurate listing of compatible exit devices, see this page on our website.

- Von Duprin Exit Only (EO) or Exit Only Fire (EOF) 22, 88 NL, 98, 99, 987, 9957
- Dorma 9100, 9300, 9400
- Detex Advantex Series 10, 20, 21, 40, 50, 51
- Monarch XX-R*, FXX-R, 18-R*, F-18-R*, XX-V*, F-XX-V*, 18-V*, F-18-V* (* Requires special key hub adapter)

Cannot be used with mortise design or most vertical rod Exit Devices

ATTENTION: Compatibility is subject to change if modifications are made by Exit Device manufacturers without Kaba Access Control's knowledge

Number of Codes:

Single access code—one easy to manage code for all users

Handing:

Exit Device may be dogged without affecting the LP1000

Key Override (Optional):

Factory handed; not field reversible

Operation Modes:

Small format interchangeable cores, large format removable cores

Economical:

Single credential—access

A cost effective access control solution

Mechanical Features

Construction:

Heavy-duty cast front housing; heavy-duty mounting plate; fixed ADA-compliant lever

Numeric Keypad:

Vandal resistant, solid metal pushbuttons

Handing:

Factory handed; not field reversible

Finishes:

Bright Chrome 026 (625), Bright Brass 03 (605), Antique Brass 05 (609), and Satin Chrome 26D (626)

Latch & Exit Device:

Not supplied (See list for compatibility above)

Strike:

Supplied with Exit Device

Exit Device Hub Rotation:

Up to 60°, counterclockwise for left hand and clockwise for right hand

Tailpiece:

2 1/4" (70 mm) tailpiece for Exit Device operation from outside

optional 5" (125 mm) tailpiece for freezer doors (purchased separately)

5" (127 mm)

Minimum Stile Recommended:

5" (127 mm)

Weight:

8 lbs. (3.6 kg)

CAPITAL IMPROVEMENT PLAN
PROJECT DESCRIPTION FORM

A) AGENCY: Fire Service

B) PROJECT NAME: Oswegatchie – Building Renovations

C) CONTACT PERSON: Mark Schenking, Chief

D) PROJECT CATEGORY:

Facilities Improvements	<input checked="" type="checkbox"/>	Apparatus, Vehicle & Equipment Replacement
Building Improvements	<input type="checkbox"/>	Highway Construction & Improvements
Land Acquisition	<input type="checkbox"/>	Sidewalks & Trails
Road Reconstruction-Sewer Extensions	<input type="checkbox"/>	Schools
Water System	<input type="checkbox"/>	Sewerage System

E) DESCRIPTION:

This request is to obtain funding to begin repairs on the fire station building. The building has been partial condemned by the Building official and since the Town hired a contractor to apply stabilization bracing through the structure. The building has a poor roof design and is in danger of failing with any heavy loading of sustained driving winds. A town building committee has been established and is not able to conduct any sort of actions without funding.

At the request of the First Selectman, the company has been attempting to obtain grant funding to assist with these renovations, but has not had any funding awarded to date. There are not many grants that are available at this time for fire station renovations, but the company will continue to seek out any possibilities that might arise.

The attached information was obtained from the engineers and architects that were called in when the conditions were noted.

**Purchase Order
P.O. # FY11 / 126008**

Date 01/07/2011
Vendor 8462

Town of Waterford
15 Rope Ferry Road
Waterford, CT 06385

Order From:

NOBLE CONSTRUCTION & MANAGEMENT CORP
PO BOX 843
ESSEX, CT 06426

Deliver To:

Oswegatchie Fire Co.,#4, Inc
Attn: Chief
441 Boston Post Rd
Waterford, CT 06385

Oswegatchie Fire Co.,#4, Inc Tel: 860-

F.O.B. Shipping Point	SHIP VIA	TERMS	CUSTOMER #		ORDERED FOR
ITEM #	DESCRIPTION	QTY.	UNIT	UNIT PRICE	EXTENDED PRICE
	Proposal Acceptance for Temporary Shoring at the Oswegatchie Fire Station Please see quote dated November 4, 2010, Item approved by the Fire Commission on December 15, 2010.	1.00		\$58,750.00	\$58,750.00

FILE COPY

Accounting Distributions

Account Number	Account Description	Amount	Freight	Total
20501-57727-205-060-01-00-27	OSWEGATCHIE FIRE HOUSE ROOF I	58,750.00	0.00	58,750.00
	Total Distributions	\$58,750.00	\$0.00	\$58,750.00

Messages:

PURCHASE ORDER NUMBER must appear on all related correspondence, shipping papers and invoices.

VENDORS: Please read terms and conditions under which this order is issued available on the web site www.waterfordct.org under Purchasing Documents.

SUBTOTAL	\$58,750.00
DISCOUNT	\$0.00
FREIGHT	\$0.00
TOTAL	\$58,750.00

Page 1 of 1

DEPARTMENT

Authorized Signature:

Authorized Signature:

Kate Rotella

FIFTEEN ROPE FERRY ROAD



WATERFORD, CT 06385-2886

November 4, 2010

Mr. Ronald Fedor
Chairman, Board of Finance
15 Rope Ferry Road
Waterford, CT 06385

RE: Oswegatchie Fire House Building Repairs

Dear Mr. Chairman and Members of the Board of Finance,

I have reviewed the request of the Board of Fire Commissioners to appropriate \$90,000.00 from the funds designated in the Capital and Non-Recurring Expenditure Fund, Line Item #20501-57506, Building Improvements. The plans have been redrawn to reflect a more temporary solution that will afford us the ability to inspect the project. This request is consistent with the Capital Improvement Plan as ratified by the Board of Selectmen.

The Director of Finance has confirmed that there are funds available in the line item designated above.

Thank you for your consideration.

Sincerely,



Daniel Steward
First Selectman

Enclosure

cc: Bruce Miller – Administrator of Fire Services
Rudie Beers – Finance Director
Paul A. Suprin, Selectman
Paul Konstantakis, Selectman

NOV - 8 2010

FIFTEEN ROPE FERRY ROAD



WATERFORD, CT 06385-2886

November 4, 2010

Mr. Ronald R. Fedor, Chairman
Board of Finance
15 Rope Ferry Road
Waterford, CT 06385

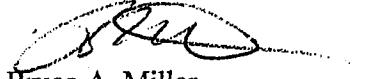
Dear Mr. Fedor:

At a meeting of the Board of Fire Commissioners held April 28, 2010, it was unanimously voted to request an appropriation in the amount of \$90,000.00 from Designated Line Item #20501-57506 Building Improvements to provide the necessary roof repairs in accordance with a structural analysis performed by J. M. Albaine Engineering, Inc. at the Oswegatchie Fire Department.

At this time, there have been several meetings of the building committee, the engineers and various town officials to make progress on this situation. The shoring plan has been redesigned and the roof trusses have been reviewed in more detail which will be explained further at your next meeting. They are seeking revised quotes on the redesigned shoring plan and fully expect that the requested funding figure will be able to be greatly reduced. As soon as the revised plan and quotations are received, they will be forwarded to your board.

If there are any questions, please feel free to contact me.

Sincerely,


Bruce A. Miller
Administrator of Fire Services



OSWEGATCHIE FIRE CO., NO. 4, INC.

441 BOSTON POST ROAD
WATERFORD, CONNECTICUT 06385
TELEPHONE 442-0666

September 14, 2010

Mr. Daniel Steward, First selectman; members of the Board of Selectmen
15 Rope Ferry Road
Waterford, Connecticut 06385

Dear Mr. Steward and members of the Board of Selectmen:

Please consider this letter as a request for an emergency appropriation to stabilize the framing on a portion of the Oswegatchie Fire Department constructed primarily in 1928 and which has been deemed to have a potential for severe catastrophic failure in the event of a heavy snow load or unusually high winds. The assessment of a severe condition was determined by Jose' Albaine, structural engineer contracted by the town to assess the structural integrity of the roof rafters and ceiling joists as well as the general structural integrity of the Oswegatchie facility. Full determination of the framing integrity cannot be fully assessed until some wall components and ceilings are removed, however, some wall deflection is noticeable, a portion of the sill plate is "punky" but is not necessarily of primary concern at this juncture. However, ceiling deflection and cracked and broken beams have been observed and photographed and represent a significant workplace hazard to both employees and volunteers.

On Friday, September 3, 2010, Waterford Building Inspector, Frank Hoagland came to the firehouse and warned fire personnel of the potential for structural failure based on the weather prediction for winds associated with Hurricane Earl. Personnel were advised to evacuate the building in the event of cracking or other framing noises.

In the interest of safety for all involved and to protect the interests of the Town of Waterford in the Oswegatchie section, it is respectfully requested immediate action on this item be considered.

One estimate for the stabilization to shore up the deflecting sections of the structure and to stabilize the numerous broken rafters/beams is \$86,696. As noted above, full cost for long term repair of this facility cannot be determined until ceilings are removed and some wall sections exposed. Shoring the building will allow fire and medical services to continue to function within reasonable safety parameters, but represents a short term solution to an immediate long term problem and will allow time for the full extent of facility condition to be assessed.

Your attention to this matter is greatly appreciated.

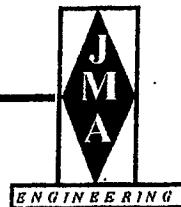
Sincerely yours,

Mark Schenking, Chief

SEP 16 2010

J.M. ALBAINE ENGINEERING, LLC
14 Lincoln Road, Waterford, CT 06385

Phone & Fax: 860-439-1111 Mobile: 860-705-1631
Email: jma724@snet.net



May 23, 2010

Mr. Mark Shenking
Chief of Fire Station
Oswegatchie Fire Station
441 Boston Post Road
Waterford, CT 06385

RE: Structural Shoring for Damaged Roof Section
Project # JMA10-327-06

Dear Mark,

The submitted shoring drawings S-1 to S-6, dated April 28, 2010, were done in order to avoid potential collapse during transient loading, such as snow and/or wind, construction loads during the repair of the cracked/broken rafters. These shoring were designed to remain in place for an undetermined amount of time while allowing the full functioning of the Apparatus Room.

It is true that the option provided requires expensive shoring placement, and their cost of construction may be reduced if some of the design loadings can be eliminated (such as snow loads), which implies that the roof repair should be conducted before the winter. Previously, we reached the conclusion that it will overall less costly to repair the roof in place than to remove the whole roof as there are other roofs attached to the affected roof. We have estimated that there are approximately 44 rafter/frames that may require reinforcing and new rafter ties for stability. An estimate of the construction work can be provided based on this basic assumption plus the additional work to install hurricane resistant ties to comply with current building code requirements.

Another option that the Town and your department may consider is that of removing the whole damaged roof completely, and to install a new roof system. This will entail less shoring work (though lateral bracing/shoring of the second floor walls will be required), and the provision of a safety procedure during the dismantling of the existing roof. Additional support will be needed for the adjacent roof that frames into the affected roof section. Under this option, we recommend that metal connected light frame trusses be provided and installed under our design criteria.

A third option that you may want to contemplate is the conversion of the old building section into a one story building, and constructing a new on the ground one-story Apparatus Room. All these options can be assessed more accurately as the Town and your department decide what best course of action to follow. We can provide a fair estimate for all three options outlined here once many of the unknown parameters are defined more specifically.



If you have any questions or desire further information regarding this correspondence, please contact this office at your convenience.

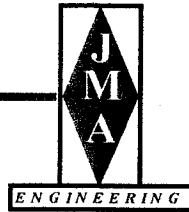
Respectfully submitted,

Jose-Miguel Albaine, M.S., P.E.
Structural Engineer

cc. Robert Avena

J.M. ALBAINE ENGINEERING, LLC
14 Lincoln Road, Waterford, CT 06385

Phone & Fax: 860-439-1111 Mobile: 860-705-1631
Email: jma724@snet.net



March 6, 2011

Mr. Mark Shenking
Chief of Fire Station
Oswegatchie Fire Station
441 Boston Post Road
Waterford, CT 06385

RE: Structural Shoring for Damaged Roof Section

Ref. Drawings:

JM Albaine Engineering, Dwgs S-1 to S-6 dated 10/25/10
As-built Dwgs. S-1 to S-8, dated March 5, 2011 Rev. 2
Project # JMA10-327-06

Dear Mark,

On February 26 and March 5, 2011, I conducted the final inspection of the installed shoring frame work. I have attached the revised As-built drawings (S1 to S-8) showing the final shoring installation.

We also observed that the exterior North wall has a bow (outward displacement at the top of wall) of approximately 3", with the East and West exhibiting bow in the order of 2" or more. Here are a couple of options deemed to provide a practical repair in term of construction, economics, and long term performance.

OPTION "A"

Reinforce existing roof in place as shown on Dwg. S-5. This option will not remove the current deflection (sag) on the ridge, but it will provide a strong roof capable of resisting the current building code load requirement. The "roof "sag" will not be noticeable from the inside as the new inside ceiling finish will be plumb and straight.

Note that this is a conceptual design and that more analysis is required to develop the final design reinforcement to fit the existing condition.

OPTION "B"

Remove the entire roof frame from both affected buildings, and provide temporary support in the areas where adjacent roof abut the affected roof.

Under this option two paths may be followed:



1. Rebuilt a new roof framework using conventional "stick framing" and/or engineering lumber where required, and adjusting the configuration to conform to the bowed exterior supporting walls.
2. Installed a new system of prefabricated metal connected wood trusses, in which case we'll recommend that all the exterior walls be replaced with new 2 x 6 walls.

In all cases, we recommend that the upper exterior wall be either reinforced (Option A & Option B, case 1), or completely replaced (Option B, case 2), since the structural integrity of these walls have been seriously compromised by the large thrust (lateral) forces from the roof framing vertical downward displacement.

For the two lower bearing exterior walls (East & West walls) housing the Apparatus Room, we recommend that these walls be repaired and reinforced as well, since the sill plates at the floor level shows advanced degree of deterioration.

Please note that the installed temporary shoring served the function of stabilizing the roof to prevent a sudden roof collapse. The shoring are designed to provide a safe working area/platform during the projected work to repair or replace the roof; this framework has been designed for temporary application as other elements such as the exterior walls are subject to local or global failure (such as from high wind pressure or suction induced by hurricane forces).

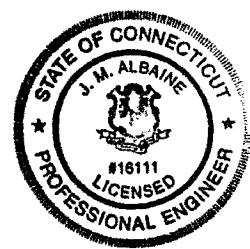
We will work with Noble Construction to develop an estimate of the construction cost for the options described in this report. We strongly recommend that work proceeds expediently since the installed shoring are only designed for temporary application.

If you have any questions or desire further information regarding this report, please contact me at your convenience.

Respectfully submitted,

J.M. Albaine

Jose-Miguel Albaine, M.S., P.E.
Structural Engineer



J.M. Albaine

cc. Robert Avena
Frank Hoagland
Steve Cardelle
Rocco Braciaelli
Ed Noble

Spectrum Engineering LLC
Consulting Engineers

Dr. Frank H. Watkinson
Richard A. Ziegler, P.E.

January 12, 2010

Crawford & Company
P.O. Box 539
Glastonbury, Connecticut 06033

Attn: Melvin Hilliard

RE: Collapsing roof
Insured: Oswegattchie Fire Company
Date of Loss: 11/9/09 Discovered
McNeil Claim No. 2584111
Your File: 1580289
Our File: 09205

Dear Melvin,

Attached in its entirety is a report by Michael Horton and Associates and authored by C. Michael Horton, P.E.

Mr. Horton has determined that the roof system is significantly overstressed not only by today's Codes but by any of the older Codes. There also have been some installations which have reduced the capacity of the roof. Theoretically, this roof should have collapsed long ago.

While in the attic I noted that there were cracked rafters and the inside of the cracks were covered with dust. There are also separations which contain dust, see attached Photos 9 and 10. Several of the rafters had been sistered, Photo 8. These are all indications that the failures are old and, in some instances, had been addressed with repairs.

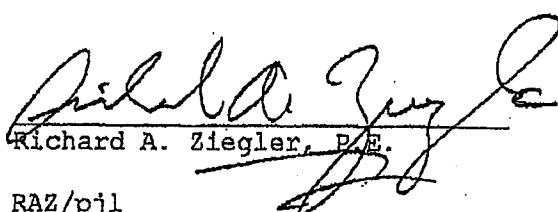
1111 South Main Street Cheshire, Connecticut 06410 Tel (203) 272-1111 Fax (203) 250-9876
Email: spectrum_engineering@yahoo.com

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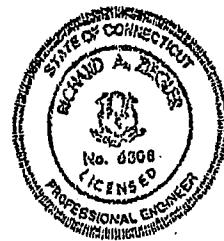
Although not mentioned in Mr. Horton's report, it is probable that the cheapest repair would be to remove the roof and install new trusses but that decision is up to a contractor as the repair time would be much quicker and far less labor intensive.

I am also including my color photographs with descriptive index for your review.

Cordially,


Richard A. Ziegler, P.E.

RAZ/pjl



Enclosures: C. Michael Horton, P.E. Report
12 Color Photographs with descriptive index

PHOTO INDEX

09205

- 1,2 South and east facades. Damage is to 1931 roof
(arrow).
- 3 Looking south at ceiling of recreation room.
- 4-6 No visible cracks in ceiling. Walls slightly out of
plumb tilting out.
- 7 One crack where original building meets addition.
- 8 Sistered rafter and supplemental ceiling hanger.
- 9,10 Cracked rafters on west side; heavy accumulation of
dust in crack.
- 11 Typical sister nailed to rafter.
- 12 Gap in framing around box contains heavy dust.

January 07, 2010

Mr. Richard A. Ziegler, P.E.
Spectrum Engineering Group
1111 South Main Street
Cheshire, CT 06410

Re: Client: Crawford & Company
Insured: Oswegatchie Fire Company
Waterford, Connecticut
441 Boston Post Road, Waterford, Ct.
Review of Roof Structure Oswegatchie Fire Company
MHAI Project No. 10-03

Dear Mr. Ziegler:

Pursuant to your request, we visited the above site with you on Tuesday, January 5, 2010 at 11:00 A.M. to observe the damage to the roof structure above the meeting hall space on the second floor. The failed roof rafters were reportedly discovered during an inspection by the Assistant Fire Chief, Rocco

STRUCTURE DESCRIPTION

The firehouse is a two story wood frame structure with several additions to the original building. Portions of the original structure has failed roof rafters. The assembly hall, below the roof in question, is 28 x 70 ft in plan with the rear 24 ft being an addition reportedly constructed during the 1980s. There is a joint in the flooring across the space which indicates the original rear wall of the building. In the attic space there is an old gable end wall at the rear addition to the original. This gable aligns with the floor joint referenced above. An aerial photo, attached, shows the relationship between pieces of the total building structure.

The roof structure consists of 2x6 rafters spaced 24 inches on center. The rafters are tied symmetrically about the ridge with original 1x6 boards and again at the ceiling with 2x6 joists which connect to the rafter approximately 32 inches in from each side wall. The ceiling joists are hung from the rafters either side of the ridge with the original 1x6 boards and a supplemental 2x4 added later. See the enclosed section SK-2 for a graphic illustration of the framing. The roof sheathing is wood board sheathing, much of which appears to have been salvaged from demolition of other structures. Reportedly, there is a plywood layer over this sheathing with asphalt shingles for the roof material.

This roof structure is abutted on the north end by the hipped roof addition and on the east by a flat roof above the two story addition. This flat roof is overbuilt for a portion of the original roof length, estimated to be 20-24 ft., for half of the sloping roof height. We believe that this area originally had a projecting piece which may have enclosed the stair to the second floor.

The roof slope is approximately 6 on 12. See plan and section SK-1 & Sk-2 for clarification.

Analysis

We have modeled the roof structure as a series of frames spaced 24 inches on center and consisting of all the framing for the roof and ceiling. See SK-2. The typical frame load consists of dead and live

MICHAEL HORTON & ASSOCIATES INC

151 MEADOW STREET BRANFORD CT. 06405
TEL: 203 481-8600 FAX 203 481 0686

Crawford & Company/ McNeil & Company
Insured: Oswegatchie Fire Company
Spectrum File 09205
MHAI Project No. 10-03
January 07, 2010
Page 2

loads. The dead load on the frames consists of the load of the roof deck and shingles, the framing members, the ceiling, and mechanical items such as sprinkler piping and ductwork. In addition there is one Air conditioning Unit supported on the ceiling joists directly under the ridge with its load spread over three or four frames. The live load (snow) on the roof surface is 30 psf. We did not assume any live load on the ceiling as the only live load is for servicing equipment. There is no storage or other use of the attic space.

Our analysis finds the roof rafters to be severely overstressed for the combined dead and live loads. The overstress for this code loading is 270% assuming the allowable bending for the rafters as 1200 psi. The calculated stress for the design condition is 4,460 psi. Those frames supporting the Air Conditioning unit are further overstressed.

For the area where the flat roof is overbuilt onto the subject roof, the original rafters do not extend from the ridge to the exterior wall. These rafters are cut off and have been sistered with new material. We believe that this location originally had a projecting piece, which created a dormer/ gable section on the roof. This piece was removed when the east addition was added and the cut thru the roof infilled. We were not able to inspect the details of this area due to finishes covering the framing and do not know how the ceiling joists are supported for this area.

There is an exhaust grille in the ceiling approximately 48 inches square which has a chase thru the attic and penetrates the roof. We believe that a single rafter and ceiling joist were cut to create this chase. We did not see any evidence of reinforcement for the framing either side of the chase for support of the load from the cut members.

CONCLUSIONS:

The existing roof framing consisting of the rafters, ceiling joists collar ties and hangers are undersized for the code required loading. The bending stresses in the rafters are far beyond that allowed by the current or any older code. The ceiling joists are raised above the top of the exterior bearing walls and frame and therefore result in rafter bending between the exterior wall and the connection point. This framing method requires very large rafters to safely support the roof loading.

There are 10 rafters with visible splits or fractures along their length. The lower portion of the rafters, where the ceiling joist connects is not visible due to insulation coverage. This lower portion of the rafter is the most highly stressed portion of the framing and could have further failures which are not visible.

The existing framing in the area of the roof overbuild is questionable but we could not inspect the exact condition to evaluate the framing. It is likely that this condition is worse than the typical analyzed above.

The added loading from the Attic air conditioning unit increases the overload for the framing.

The cutout for the exhaust frame is presumed to have one rafter and ceiling joist cut to accommodate the chase. We assume the cut framing members are headed off to the adjacent rafters and joists. This condition applies 50 percent added load to the adjacent framing.

This roof structure does not comply with the building code loading. The failed framing members further increase the load on adjoining framing. The entire roof needs to be upgraded for the dead and live loading.

Crawford & Company/ McNeil & Company
 Insured: Oswegatchie Fire Company
 Spectrum File 09205
 MHAI Project No. 10-03
 January 07, 2010
 Page 2

CORRECTIVE WORK

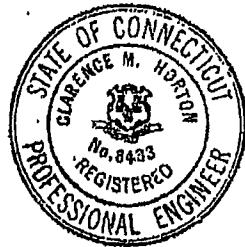
The rafters are grossly undersized and would have to be sistered with a single 9-1/4-lvl for their full length to meet the bending stresses. The installation of these new members would require shoring the ceiling, cutting loose all the hangers and connections to sister each rafter. Either the ceiling or the roof would have to be removed in order to install the added members. Once the sistered rafters are in place then the ceiling hangers can be reinstalled and all disturbed work restored.

If you have any questions please call.

Sincerely,

Clarence M. Horton Jr.
 C. Michael Horton P.E.
 Michael Horton Associates Inc.

Enclosures



Noble Construction			Unit	QTY	Unit Cost	Total Cost	Contingency	Design	CM/General Conditions	Total	
Repair							20%	20%	40%		
					Repair	\$ 385,117.00	\$ 77,023.40	\$ 77,023.40	\$ 154,046.80	\$ 693,210.60	\$ 831,852.72
					ADA	\$ 244,700.00	\$ 48,940.00	\$ 48,940.00	\$ 97,880.00	\$ 440,460.00	\$ 138,642.12
					Future	\$ 356,800.00	\$ 71,360.00	\$ 71,360.00	\$ 142,720.00	\$ 642,240.00	
							\$ 197,323.40	\$ 197,323.40	\$ 394,646.80	\$ 1,775,910.60	
Roof					Total	\$ 986,617.00					
	Demolition	LS	1	\$ 50,000.00	\$ 50,000.00	wall paneling, shingles, sheathing					
Structure	LF	70	\$ 1,000.00	\$ 70,000.00							
Shingles & Sheathing	SF	2835	\$ 15.00	\$ 42,525.00	also includes flashings needed						
Repair Flat Roof at east wall	SF	350	\$ 35.00	\$ 12,250.00	also includes flashings needed						
Hall											
Sprinkler	LS	1	\$ 10,000.00	\$ 10,000.00	Remove piping to allow for repair. Relocate drops in new lower ceiling						
Electrical	LS	1	\$ 10,000.00	\$ 10,000.00	rewire to accept new lighting						
Lighting	EA	26	\$ 500.00	\$ 13,000.00	install energy efficient lighting						
HVAC	LS	1	\$ 10,000.00	\$ 10,000.00	Remove units, reinstall units and relocate HVAC drops						
HVAC Unit	LS	1	\$ 10,000.00	\$ 10,000.00							
Ceiling	SF	1980	\$ 15.00	\$ 29,700.00	standard 2'x4' drop ceiling						
Walls Strengthen	LF	194	\$ 20.00	\$ 3,880.00	Reinforcing and pluming of walls as needed						
Walls	SF	1746	\$ 10.00	\$ 17,460.00	Sheetrock, taping, and paint						
Floor demo	SF	1911	\$ 2.00	\$ 3,822.00							
Floor	SF	1911	\$ 10.00	\$ 19,110.00	Wood Floor						
Upstairs Hallway											
Carpet	SF	342	\$ 5.00	\$ 1,710.00	Carpet at \$42 per yard and \$3 per yard for removal of existing						
Day Room											
Ceilings											
Patch Fire rated sheet rock	SF	12	\$ 50.00	\$ 600.00	2 layers of 5/8" sheetrock with taping between coats						
Drop ceiling	SF	775	\$ 10.00	\$ 7,750.00	Should be carried do to amount of work to be preformed above						
Walls	SF	780	\$ 10.00	\$ 7,800.00	Sheetrock, taping and paint						
Floor	SF	650	\$ 5.00	\$ 3,250.00	Carpet at \$42 per yard and \$3 per yard for removal of existing						
Kitchen											
Ceilings	SF	120	\$ 15.00	\$ 1,800.00	2 layers of 5/8" sheetrock						
Floor	SF	234	\$ 20.00	\$ 4,680.00	Porcelain Tile remove and reinstall						
Apparatus Bay											
Ceilings	SF	50	\$ 25.00	\$ 1,250.00	2 layers of 5/8" sheetrock with taping between coats						
Walls											
Strengthen	LF	100	\$ 20.00	\$ 2,000.00	Reinforcing and pluming of walls as needed						
Sill plate	LF	100	\$ 20.00	\$ 2,000.00	Reinforcing and pluming of walls as needed						
Finishes	SF	600	\$ 10.00	\$ 6,000.00	Sheetrock, taping and paint						
Floor	EA	20	\$ 10.00	\$ 200.00	Patch hole in concrete						
Dispatch											
Ceilings											
Patch Fire rated sheet rock	SF	10	\$ 25.00	\$ 250.00	2 layers of 5/8" sheetrock with taping between coats						
Drop ceiling	SF	128	\$ 10.00	\$ 1,280.00	Should be carried do to amount of work to be preformed above						
Walls	SF	192	\$ 10.00	\$ 1,920.00	Sheetrock, taping and paint						
Floor	SF	448	\$ 10.00	\$ 4,480.00	Epoxy flooring, subject to direct water						

Basement						
Ceiling	SF	20	\$ 25.00	\$ 500.00	2 layers of 5/8" sheetrock with taping between coats	
Floor	SF	1000	\$ 5.00	\$ 5,000.00	Carpet at \$42 per yard and \$3 per yard for removal of existing	
Bathrooms Upstairs						
Men's						
Size/Layout	LS	1	\$ 5,000.00	\$ 5,000.00		
Plumbing	LS	1	\$ 2,500.00	\$ 2,500.00		
Fixtures						
Toilet	LS	1	\$ 1,500.00	\$ 1,500.00		
Urinal	LS	1	\$ 1,500.00	\$ 1,500.00		
Sink	LS	1	\$ 1,500.00	\$ 1,500.00		
Faucet	LS	1	\$ 300.00	\$ 300.00		
Grab Rails	LS	1	\$ 750.00	\$ 750.00		
Toilet Partitions	LS	1	\$ 2,000.00	\$ 2,000.00		
Floor	SF	160	\$ 10.00	\$ 1,600.00		
Ceiling	SF	150	\$ 10.00	\$ 1,500.00		
Woman's						
Size/Layout	LS	1	\$ 4,000.00	\$ 4,000.00		
Plumbing	LS	1	\$ 2,500.00	\$ 2,500.00		
Fixtures						
Toilet	LS	1	\$ 1,500.00	\$ 1,500.00		
Sink	LS	1	\$ 1,500.00	\$ 1,500.00		
Faucet	LS	1	\$ 300.00	\$ 300.00		
Grab Rails	LS	1	\$ 750.00	\$ 750.00		
Floor	SF	100	\$ 10.00	\$ 1,000.00		
Ceiling	SF	120	\$ 10.00	\$ 1,200.00		
ADA						
Bathrooms						
Men's						
Size/Layout	LS	1	\$ 5,000.00	\$ 5,000.00		
Plumbing	LS	1	\$ 2,500.00	\$ 2,500.00		
Fixtures						
Toilet	LS	1	\$ 1,500.00	\$ 1,500.00		
Urinal	LS	1	\$ 1,500.00	\$ 1,500.00		
Sink	LS	1	\$ 1,500.00	\$ 1,500.00		
Faucet	LS	1	\$ 300.00	\$ 300.00		
Grab Rails	LS	1	\$ 750.00	\$ 750.00		
Toilet Partitions	LS	1	\$ 2,000.00	\$ 2,000.00		
Floor	SF	160	\$ 10.00	\$ 1,600.00		
Woman's						
Size/Layout	LS	1	\$ 4,000.00	\$ 4,000.00		
Plumbing	LS	1	\$ 2,500.00	\$ 2,500.00		
Fixtures						
Toilet	LS	1	\$ 1,500.00	\$ 1,500.00		
Sink	LS	1	\$ 1,500.00	\$ 1,500.00		
Faucet	LS	1	\$ 300.00	\$ 300.00		
Grab Rails	LS	1	\$ 750.00	\$ 750.00		
Floor	SF	100	\$ 10.00	\$ 1,000.00		

Access											
Exterior Ramp		LS	1	\$ 7,000.00	\$ 7,000.00						
Interior Ramp		LS	1	\$ 3,500.00	\$ 3,500.00						
Elevator		LS	1	\$ 200,000.00	\$ 200,000.00						
Lever Style Door Knobs		EA	30	\$ 200.00	\$ 6,000.00						
Future											
Heating System											
Boiler		LS	1	\$ 25,000.00	\$ 25,000.00						
Piping		LS	1	\$ 10,000.00	\$ 10,000.00						
CUH		EA	3	\$ 1,000.00	\$ 3,000.00						
Radiators		LF	300	\$ 25.00	\$ 7,500.00						
Cooling											
Add in Day Room		LS	1	\$ 10,000.00	\$ 10,000.00						
Add in Dispatch		LS	1	\$ 2,000.00	\$ 2,000.00						
Replace old units		EA	4	\$ 8,000.00	\$ 32,000.00						
Windows											
Throughout		LS	1	\$ 20,000.00	\$ 20,000.00						
Flat Roofs											
All		SF	3200	\$ 20.00	\$ 64,000.00						
Kitchens											
First Floor											
Cabinets		LF	34	\$ 400.00	\$ 13,600.00	Demo of existing, sanitary cabinets and cabinets					
Plumbing		LS	1	\$ 1,000.00	\$ 1,000.00						
Fixtures				\$	-						
Sink		LS	1	\$ 300.00	\$ 300.00						
Faucet		LS	1	\$ 150.00	\$ 150.00						
Second Floor											
Floors		SF	240	\$ 20.00	\$ 4,800.00						
Cabinets		LF	50	\$ 400.00	\$ 20,000.00	Demo of existing, sanitary cabinets and cabinets					
Plumbing		LS	1	\$ 1,000.00	\$ 1,000.00						
Fixtures				\$	-						
Sink		LS	1	\$ 300.00	\$ 300.00						
Faucet		LS	1	\$ 150.00	\$ 150.00						
Overhaul of appliances		LS	1	\$ 1,500.00	\$ 1,500.00						
Garage Doors											
Doors		EA	6	\$ 4,000.00	\$ 24,000.00						
Motors		EA	6	\$ 2,000.00	\$ 12,000.00						
Egress											
Enclosed Secondary Stair		LS	1	\$ 100,000.00	\$ 100,000.00						
Shed											
Roof		SF	750	\$ 4	\$ 3,000.00						
Rotten walls		LS	1	\$ 1500	\$ 1,500.00						

Conc. Mon.
Rec. (114)
Nail Recet

Storage
Shed

Compressor
Shed

Dumpster
Pad

Conc. Mon.
Rec.

129.47'
520.930W

Conc. Mon.
Rec

51.64'

CHD
Rec.

69.90'
565.2042E

SNET
2775

Sign

550.4455E
190.00'

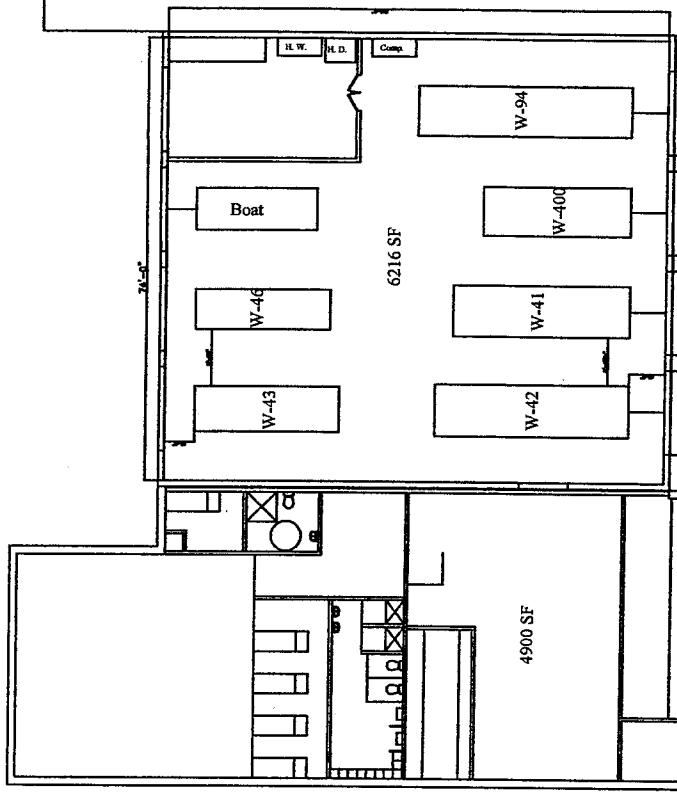
SNET
2776

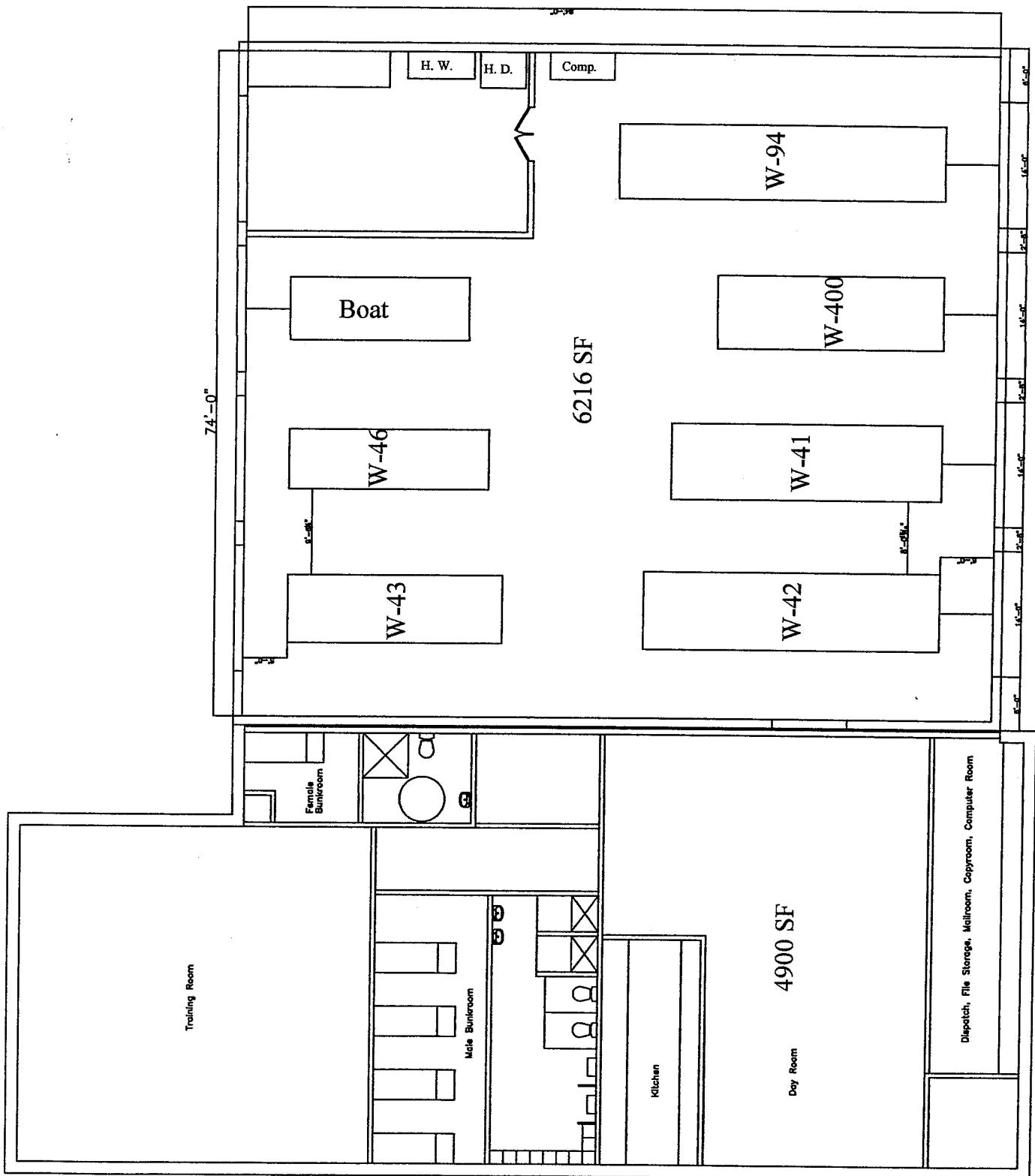
Nail
Set
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Y= 691768.72

190.00'

539.1505W

192.85'



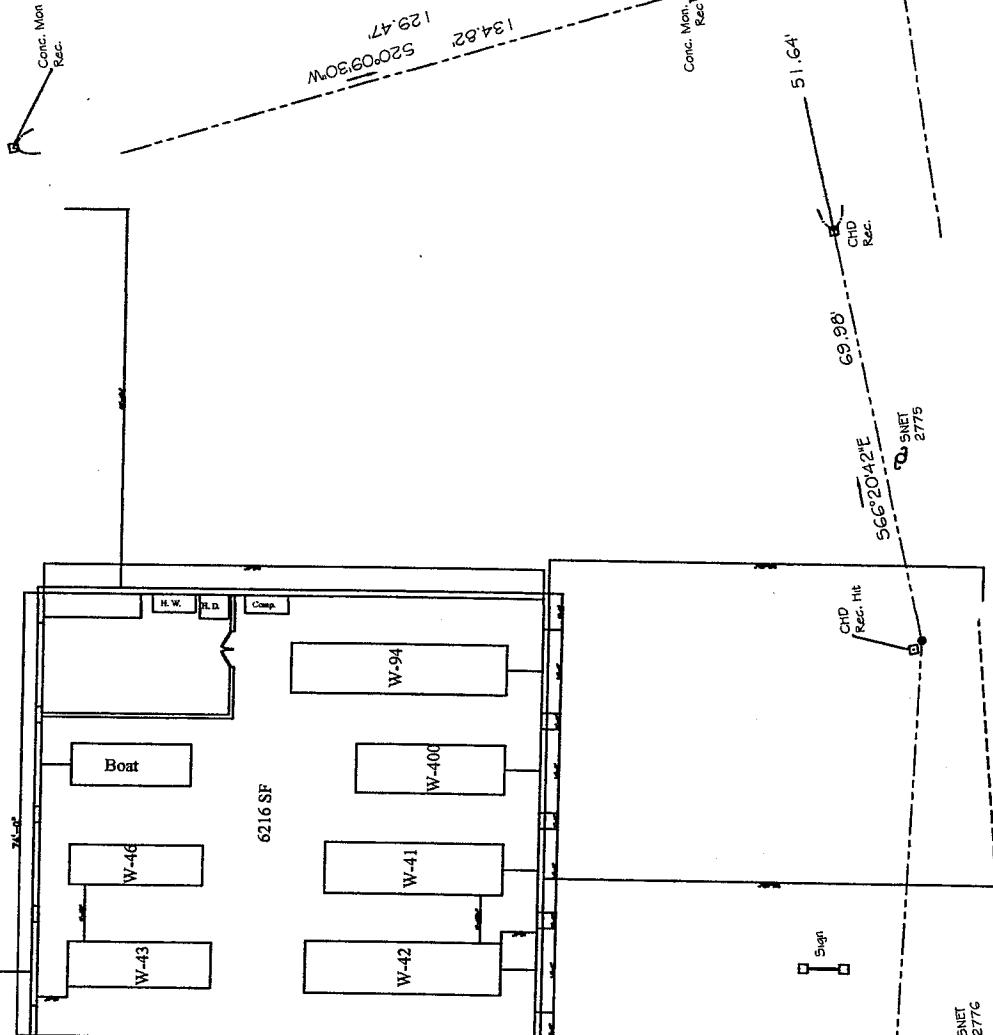


Compressor
Shed

Dumpster
Pad

Storage
Shed

Conc. Mon.
Rec. (Int)
Nail Reset



192.85' S39°15'05" W

Siren
Pole

Nail
Set
X = 1158359.13
Y = 691768.72

SNET
2777

CAPITAL IMPROVEMENT PLAN
PROJECT DESCRIPTION FORM

A) AGENCY: Fire Service

B) PROJECT NAME: Fire Stations' – Underground Tank Replacement

C) CONTACT PERSON: Bruce Miller, Director of Fire Services

D) PROJECT CATEGORY:

Facilities Improvements	<input checked="" type="checkbox"/>	Apparatus, Vehicle & Equipment Replacement	
Building Improvements	<input type="checkbox"/>	Highway Construction & Improvements	
Land Acquisition	<input type="checkbox"/>	Sidewalks & Trails	
Road Reconstruction-Sewer Extensions	<input type="checkbox"/>	Schools	
Water System	<input type="checkbox"/>	Sewerage System	

E) DESCRIPTION:

This request is to provide for the replacement and updating of the Underground Fuel Storage Tanks that exist at the various fire stations. Some of these tanks have been observed to have taken in water due to the tanks not meeting the most current code standards.

A quotation from Service Station Equipment, Inc. of Uncasville, CT is attached.

Service
Station
Equipment, Inc. *



Petroleum Contractors Since 1984
Web Site: SSE-INC.Net
Members of NFPA, PEI, CBIA, ICPA
Veeder Root/ Gilbarco ASC

1-800-801-TANK

October 27, 2010

Town of Waterford
Bureau of Fire Prevention
Attn: Bruce A. Miller, Admin of Fire Services
204 Boston Post Road
Waterford, CT 06385

RE: Budgetary Pricing for Up Dates at Fire Station

Dear Bruce,

This scope of work is a work in progress. If a station's scope is not to your spec, we can modify. Bear in mind these budgetary numbers are with standard labor rates. If this scope becomes Davis Bacon Wage Scale, prices will have to be adjusted. These specs are budgetary and usage. This document is not set up to bid off of. These are high end numbers.

Jordan Fire Department -

- Remove/Defume/Dispose of (2) 1000 gallon U/G Tanks
- Closure reporting on both tanks / Soil Samples
- Backfill area
- Install concrete pad approximate size 16' x 6' x 6" thick
- Install (2) 500 gallon D/W tanks or (1) 1000 gallon tank D/W skid tank
- Repipe into boiler room and to generator
- Discontinue diesel pump
- Remove and destroy (no value)

Total for above.....\$17,500.00

Oswegchie Fire Department -

- Remove both tanks out back (1) gas (1) diesel - 1000 gallon each
- Remove pumps (no value)
- Remove (1) 275 in driveway for generator
- Remove (1) 2000 gallon heating oil tank to left of building - Perform sampling and closure on all
- Install (1) 2000 gallon D/W skid tank to right rear of building on concrete pad
- Install U/G piping (double wall composite piping)
- Running (2) sets of pipe to generator room
- Running (2) sets of pipe approx 75' to boiler room

MLL

33 Leffingwell Road, Uncasville, CT 06382

Phone: 860-848-2278 Fax: 860-848-4449

CT Home Improvement Lic. # 582539 CT Plumbing Lic. # 208469 CT Weights & Measures DLR # 106 RI "Gas Station" Lic.

- Reconstruction of pavement
- Day tank with return pumps (if needed)

Total for above.....\$35,000.00

Goshen Fire Department -

- Remove (1) older tank
- Remove pump
- Use newest tank has 30 year life span
- Redo tank top with sumps and install new double wall composite piping to boiler and generator
- Install provision for TLS / Install TLS
- Reconstruct back area with new concrete and manholes
- Repave area

Total for above.....\$28,000.00

Or we can put above ground in (not sure with all that telecommunications equipment and rear exiting)

Cohanzie Fire Department -

- If public work updates with Fuel Master System
- Veeder Root need up dating

Total for above.....\$2,400.00

- Overfill valve on all (3) tanks needed

Total for above.....\$3,800.00

Quaker Hill -

As a removal town wide fueling site, I really believe it will be more cost effective to install (1) 10,000 gallon under ground tank split 5 gas/5 diesel. Install a new island with (2) new dispensers and a card reader and a yard light in the middle.

All power phones will be pick up on the building on the left

Total for above.....\$145,000.00

On the exiting tank in the main fire station, this whole system meets no specs. The vent/fill must be outside. Also the pump needs to be outside.

We can do something along the lines of Jordan:

- Remove U/G 1000 gallon tank heating oil
- Remove 500 above ground S/W side out of boiler room
- No real place for above ground
- Install new 1000 gallon D/W underground and tie into generator and heating system to it

Total for above.....\$28,415.00

I revised these numbers so it may give you sticker shock, but these sites have been all but neglected since I installed these tanks years ago. Let me know if you want me to restructure any of this.

If you have any questions feel free to call me at 860 848-2278.

Tanks a lot:

Martin D. McKinney, Jr.
Service Station Equipment, Inc.

cc: Ronald R. Cusano

MLL

33 Leffingwell Road, Uncasville, CT 06382

Phone: 860-848-2278 Fax: 860-848-4449

CT Home Improvement Lic. # 582539 CT Plumbing Lic. # 208469 CT Weights & Measures DLR # 106 RI "Gas Station" Lic.

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FIRST SELECTMAN'S RECOMMENDATIONS ON THE 2013-17 FLEET MANAGEMENT PLAN

TO: Paul A. Suprin, Selectman
Paul Konstantakis, Selectman

FROM: Daniel M. Steward, First Selectman

RE: Recommended Fleet Management Replacements & Variances 2013-17

DATE: January 9, 2012

I am attaching the Fleet Management Plan 2013-17 which is included in the five-year Capital Improvement Plan.

The departmental submissions under the 2013-17 Capital Improvement Plan referenced the changes which I have outlined on the attached. For the most part, variances are requested for the following reasons:

- a) To defer replacement of assets currently in good condition and do not warrant replacement at this time
- b) To redefine replacement of deferred assets to reduce repair costs
- c) To refine replacement costs

A summary of my recommendations relating to the replacements scheduled in the FY13 Fleet Management Plan and departmental variance requests is as follows:

1) BOARD OF EDUCATION:

- 1) A request from Jerome Belair to replace BOE4, a 2003 Ford F250 in accordance with its normal replacement schedule. I approve this request due to excessive repair costs.
- 2) A request from Jerome Belair to defer replacement of BOE11, a 2002 Kawasaki Mule to FY14 due to the fact that it is in good operating condition. I approve this request.

2) FIRE SERVICES:

- 1) A request from Bruce Miller, Director of Fire Services to defer replacement of Car105, a 2005 Ford Crown Victoria used by the Fire Inspectors on an as needed basis to FY14 due to the fact that it is in good operating condition. I approve this request.

**FIRST SELECTMAN'S RECOMMENDATIONS TO THE 2013-17
FLEET MANAGEMENT PLAN**

January 9, 2012

Page 2.

FIRE SERVICES, CONTINUED:

- 2) A request from Bruce Miller, Director of Fire Services, to replace Car 55 a 2005 Ford Expedition used by the Fire Marshal in accordance with the Plan. I approve this request due to the condition of the vehicle and its repair cost history.
- 3) A request from Bruce Miller, Director of Fire Services, to defer replacement of Car 85 a 2005 Ford Crown Victoria used by the Director of Fire Services to FY15 due to the fact that it is in good operating condition. I approve this request.
- 4) A request from Bruce Miller, Director of Fire Services, to replace W-94 in accordance with the Plan. The vehicle is currently being used by Oswegatchie Fire Company as a Service/Command Vehicle. Mr. Miller requests a variance in the cost of replacement from \$512,000 to \$375,000 based upon scaling down the unit to meet current operational demands. I approve this request as the downsizing will adequately meet the needs of the Department and the Town.
- 5) A request from Bruce Miller, Director of Fire Services, to replace W-31 in accordance with the Plan. The vehicle is currently used by the Goshen Fire Company. It is a 1990 Simon Duplex Pumper. I approve this request as it has been deferred for the last three fiscal years.

3) POLICE DEPARTMENT:

- 1) A request from Chief Pendleton to defer replacement of Car 22 a 2005 Ford Expedition to FY14 due to its low mileage. I approve this request.
- 2) A request from Chief Pendleton to replace Car 29 a 2005 Ford Crown Victoria in accordance with the Plan. I approve this request on the basis of the vehicle's high mileage and consistency in meeting its replacement parameter.
- 3) A request from Chief Pendleton to replace Car 6 a 2008 Ford Crown Victoria in accordance with the Plan. I approve this request on the basis of the vehicle's high mileage and consistency in meeting its replacement parameter.
- 4) A request from Chief Pendleton to replace Car 9 a 2008 Ford Crown Victoria in accordance with the Plan. I approve this request on the basis of the vehicle's high mileage and consistency in meeting its replacement parameter.

**FIRST SELECTMAN'S RECOMMENDATIONS TO THE 2013-17
FLEET MANAGEMENT PLAN**

January 9, 2012

Page 3.

- 5) A request from Chief Pendleton to defer replacement of PD1 a 1993 Cross Box Trailer due to its good operating condition. I approve this request.

4) PUBLIC WORKS:

- 1) A request from Ronald Cusano, Director of Public Works to replace H-77 a 1993 Cross Country Box Trailer in accordance with the Plan. I approve this request on the basis of its age and condition.

5) P & Z, BUILDING DEPT. AND TOWN HALL ADMINISTRATIVE:

- 1) A variance request from Thomas V. Wagner, Planning Director, to replace A3 a 2003 re-assigned Ford Crown Victoria originally on the Plan as "Do Not Replace" with a new 2012 Ford Fusion. I approve this variance as it is consistent with the recommendation of our consultants as noted in the new Fleet Sizing and Utilization Plan.
- 2) A variance request from Thomas V. Wagner, Planning Director, to replace A6 a 2003 re-assigned Ford Crown Victor originally on the Plan as "Do Not Replace" with a new 2012 Ford Fusion. I approve this variance as it is consistent with the recommendation of our consultants as noted in the new Fleet Sizing and Utilization Plan.

6) RECREATION AND PARKS:

- 1) A variance request from Ryan McNamara, Assistant Director of Recreation and Parks to defer replacement of P61 a 1997 Kubota Tractor which was deferred from FY12 to FY14 as its condition does not warrant replacement at this time. I approve this variance.
- 2) A variance request from Ryan McNamara, Assistant Director of Recreation and Parks to defer replacement of P60 a 1997 Kubota Tractor which was scheduled for replacement in FY13 to FY14 and replace instead P62, a 2000 Toro 325D mower originally scheduled for replacement in 2016, but due to excessive down time and repair costs needs to be replaced next year. I approve this variance to maximize operational efficiency.
- 3) A variance request from Ryan McNamara, Assistant Director of Recreation and Parks to increase the assets in the plan for the purchase of a new 21 HP Kubota that can be multi-functional and will allow the department to perform its assigned duties within the confines of existing staffing levels. The Kubota is estimated to cost \$31,291.00. I approve this request as this piece of equipment will allow Recreation and Parks to perform its required duties with the limited maintenance staff available.

**FIRST SELECTMAN'S RECOMMENDATIONS TO THE 2013-17
FLEET MANAGEMENT PLAN**

January 09, 2012

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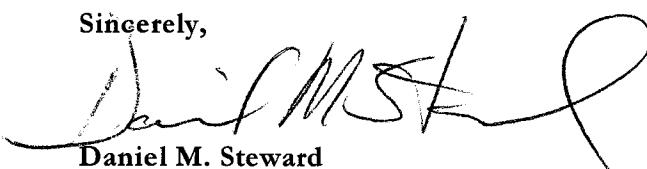
- 4) A variance request from Ryan McNamara, Assistant Director of Recreation and Parks to defer R8 scheduled for replacement in FY13 to FY14 as its present condition does not warrant replacement at this time. I approve this request.

7) UTILITY COMMISSION:

- 1) A variance request from Neftali Soto, Chief Engineer, Utility Commission to defer replacement of PG1 a 1982 Onan Trailer for hauling generators due to low usage hours. I approve this request.
- 2) A variance request from Neftali Soto, Chief Engineer, Utility Commission to defer replacement of PG2 a 1991 Onan Trailer for hauling generators due to low usage hours. I approve this request.
- 3) A variance request from Neftali Soto, Chief Engineer, Utility Commission to defer replacement of PP1 a 1985 Gorman Rupp Pump due to low usage hours and the excellent condition of the asset. I approve this request.
- 4) A variance request from Neftali Soto, Chief Engineer, Utility Commission to defer replacement of PP2 a 1990 Gorman Rupp Pump due to low usage hours and the excellent condition of the asset. I approve this request.

In summary, these changes will have the net effect of reducing the replacement cost value of the Plan in FY 2013 by \$476,369. These recommendations are consistent with the mission of the Plan and will allow us to average vehicle and equipment replacements at a sustained budgetary requirement of \$1,095,000 per year for the next several years. Attached is a summary financial analysis of the next five years of funding versus replacement costs.

Sincerely,



Daniel M. Steward
First Selectman

DMS:rab
Attachments

**FIRST SELECTMAN'S RECOMMENDATIONS TO THE 2013-17
FLEET MANAGEMENT PLAN**

January 09, 2012

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Cc: Michael Bekech, Tax Assessor
Jerome Belair, Superintendent of Schools
James Miner, Director of Buildings and Grounds, BOE
Deputy Chief John Mariano, Cohanzie Fire Company
Thomas V. Wagner, Planning Director
Chief Neil Wiseman, Goshan Fire Company
Chief Timothy Sullivan, Jordan Fire Company
Chief Mark Shenking, Oswegatchie Fire Company
Chief Matthew Carson, Quaker Hill Fire Company
Bruce Miller, Fire Administrator
Murray J. Pendleton, Chief of Police
Sgt. Steven Bellos, Waterford Police Department
Ronald R. Cusano, Director of Public Works
Brian W. Flaherty, Director of Recreation and Parks
Sally Ritchie, Director of Senior Services
Neftali Soto, Chief Engineer, Utility Commission

FLEET MANAGEMENT PLAN AS RECOMMENDED BY THE FIRST SELECTMAN FOR FY13 CAPITAL PROGRAM

Asset ID Number	License Plate No.	Agency Code	Agency Name	Class Code	Class Description	Asset Model Year	In-Service Date	Gross Vehicle Cost	Manufacturer	Model	Serial #	Mileage as of 10/15/11	Fleet Management Plan Metrics				
													12	19	13	19	20
A4	114WFD		Assessor	9999	Do Not Replace	2003	06/23/03	\$ 22,153	FORD	CROWN VICTORIA	2FAH971W83X209380	97,559					
BOE1	23WFD	60	Board of Ed	PCK-F	PICK UP AND JEEP (FIRE)	2011	08/15/11	\$ 38,465	Ford	F250	1FDBF2B64BE82407	1,266					
BOE2	24WFD	60	Board of Ed	VAN-DPW	VAN	2005	04/21/05	\$ 29,056	FORD	ECONOLINE	1FDWF35L75HA78392	24,266					
BOE3	25WFD	60	Board of Ed	PCK-F	PICK UP AND JEEP (FIRE)	2001	11/01/00	\$ 25,817	Ford	F350	1FDWF36SX1EA36569	96,737					\$ 56,979
BOE4	26WFD	60	Board of Ed	PCK-F	PICK UP AND JEEP (FIRE)	2003	06/01/03	\$ 23,697	Ford	F250	1FTNF21L73EA71972	62,100	\$ 39,730				
BOE5	27WFD	60	Board of Ed	VAN-DPW	VAN	2011	01/20/11	\$ 18,425	Ford	E250 Van	1FTNE2EWXBDA45393	6,185					
BOE6	28WFD	60	Board of Ed	PCK-F	PICK UP AND JEEP (FIRE)	2008	10/26/07	\$ 31,800	Ford	F250	1FDNF21508EB26217	31,466					
BOE7	29WFD	60	Board of Ed	PCK-F	PICK UP AND JEEP (FIRE)	2005	01/10/05	\$ 25,679	FORD	F350	1FTWX31545EB72091	43,772					\$ 41,828
BOE8	30WFD	60	Board of Ed	MDD-DPW	MEDIUM DUTY DUMP	2000	02/01/00	\$ 25,336	Ford	Dump Truck	1FDWF36S3YEC20505	57,700					\$ 69,217
BOE9	117WFD	60	Board of Ed	PCK-F	PICK UP AND JEEP (FIRE)	2005	01/11/05	\$ 24,913	FORD	F250	1FDNF21595EB30178	59,727					\$ 41,828
BOE10	N/A	60	Board of Ed	SKD STR	SKID STEER LOADER	2007	05/11/07	\$ 22,000	New Holland	L170 Skid Star Bobcat	N7M455552	303.6/Hrs.					\$ 29,064
BOE11	N/A	60	Board of Ed	UTL-DPW	UTILITY CART	2002	09/20/02	\$ 13,649	Kawasaki	Mule 4x4 Diesel	KAF950A3	2,859					
W-500	1360	COH	Cohanzie Fire Company	9999	Do Not Replace	2009	05/20/09	\$ 192,168	Ford	Ambulance	1FDAF56R595A45556	19,355					
W-52 aka W-51	1028	COH	Cohanzie Fire Company	1500PMP-F	1500 GPM PUMPER (FIRE)	1996	07/12/96	\$ 280,055	Pierce	Dash Pumper	4P1CT02E4TA000232	46,873					\$ 485,979
W-53	817	COH	Cohanzie Fire Company	BRS-F	BRUSH TRUCK (FIRE)	2009	05/13/09	\$ 95,854	Ford	Super Duty F-550	1FDWF57R29E435364	3,002					
W-55	2655	COH	Cohanzie Fire Company	T&A-F	TOWER LADDER/AERIAL (FIRE)	2004	12/29/04	\$ 667,929	SUTPHEN	Ladder Truck	1S9A7LLD442003078	16,078					
W-56	42WFD	COH	Cohanzie Fire Company	UTL-F	UTILITY UNIT (FIRE)	2011	10/12/11	\$ 47,426	Ford	F350	1FT8X3BT6BEA81633	3,136					
W-57	595	COH	Cohanzie Fire Company	SPC-F	SPECIALITY UNIT (FIRE)	2010	10/05/10	\$ 511,983	Ferrara Igniter Custom	Rescue Truck	1F9505328AH140755	16,078					
Car 105 (Q105)	105WFD	23/FM	FIRE MARSHAL	CAR-F2	CAR (FIRE)	2005	07/15/05	\$ 19,886	FORD	CROWN VICTORIA	2FAHP71W15X106188	36,521					\$ 33,682
Car 115 (Q115)	115WFD	23/FM	FIRE MARSHAL	CAR-F2	CAR (FIRE)	2006	01/17/06	\$ 20,696	FORD	CROWN VICTORIA	2FAHP71W86X122289	31,570					\$ 33,682
Car 55 (Q55)	55WFD	23/FM	FIRE MARSHAL	CAR-F	CAR (FIRE)	2005	09/15/04	\$ 26,464	FORD	EXPEDITION	1FMPU16565LA13705	88,888	\$ 33,353				
Car 85 (Q85)	85 WFD	23/FM	FIRE MARSHAL	CAR-F	CAR (FIRE)	2005	10/25/04	\$ 22,747	FORD	CROWN VICTORIA	2FAFP74W55X114846	33,546					\$ 33,682
A1	1WFD	1	FIRST SELECTMAN	CAR-DPW	CAR	2010	10/06/10	\$ 19,658	FORD	Fusion	3FAHPOJGXBR129270	13,583					\$ 24,177
Avon	N/A	GOS	Goshen Fire Company	9999	Do Not Replace	1991				Avon	AVB10669J091	10 hours					
Boat Trailer	1957	GOS	Goshen Fire Company	TRL-F	TRAILER (FIRE)	2008	11/02/07	\$ 4,900	EZ Loader	Boat Trailer	1ZEAAMXG58A008657	N/A					
Utility Trailer	N/A	GOS	Goshen Fire Company	TRL-F	TRAILER (FIRE)	1999	11/01/89		Parker	Utility Trailer	13ZSA1014X100005	N/A					
W-300	1340	GOS	Goshen Fire Company	9999	Do Not Replace	2002	09/27/02	\$ 140,000	Ford	Ambulance	1FDXE45F92HA21112	33,959					
W-31	1303	GOS	Goshen Fire Company	1500PMP-F	1500 GPM PUMPER (FIRE)	1990	03/12/91	\$ 250,150	Simon	Duplex Pumper	1D91P11E1L1008202	41,493	\$ 451,329				
W-32	371	GOS	Goshen Fire Company	1500PMP-F	1500 GPM PUMPER (FIRE)	1998	02/23/99	\$ 307,935	Spartan	1,500 GPM Pumper	4S7AT419XWC026143	15,298					\$ 524,519
W-33	2654	GOS	Goshen Fire Company	UTL-F	UTILITY UNIT (FIRE)	2007	08/29/06	\$ 36,973	FORD	F450							
W-36	1494	GOS	Goshen Fire Company	UTL-F	UTILITY UNIT (FIRE)	2007	08/29/06	\$ 36,973	FORD	F350 PICK-UP	1FTWW31P17E6A7953	19,963					
W-93	N/A	GOS	Goshen Fire Company	BOT-F	BOAT (FIRE)	2007	06/29/07	\$ 103,334	Lake Assault	24' Landing Craft	LKGKGFD99G707	371 Hours					
W-100	600	JOR	Jordan Fire Company	9999	Do Not Replace	2004	09/28/04	\$ 144,000	Ford	E450 Ambulance	1FDXE45P9484B29712	1,901					
W-11	1451	JOR	Jordan Fire Company	1500PMP-F	1500 GPM PUMPER (FIRE)	1993	01/02/94	\$ 245,093	Simon	Duplex - 1,500 GPM Pumper	1D91P61E3P3008643	53,436					
W-15	1454	JOR	Jordan Fire Company	T&A-F	TOWER LADDER/AERIAL (FIRE)	1996	01/01/96	\$ 499,354	Pierce	Lance Ladder Truck	4P1CT02G4TA000233	35,177					\$ 964,795
W-16	1484	JOR	Jordan Fire Company	UTL-F	UTILITY UNIT (FIRE)	2005	04/22/05	\$ 43,594	Ford	F350	1FTWW31P15EC55708	45,322					\$ 50,552
W-400	589	OSW	Oswegatchie Fire Company	9999	Do Not Replace	2008	01/08/08	\$ 192,378	Ford	F-550 4x4 Ambulance	1FDAF57R18EC1919	31,597					\$ 524,519
W-41	1103	OSW	Oswegatchie Fire Company	1500PMP-F	1500 GPM PUMPER (FIRE)	1998	02/16/99	\$ 279,030	Spartan	1,500 GPM Pumper	4S7AT2294WC026142	21,581					
W-42	1349	OSW	Oswegatchie Fire Company	1500PMP-F	1500 GPM PUMPER (FIRE)	2007	05/14/08	\$ 246,972	Seagrave	1,500 Marauder Pumper	1F9E528T97CST2165	10,393					
W-43	1026	OSW	Oswegatchie Fire Company	BRS-F	BRUSH TRUCK (FIRE)	2006	06/06/06	\$ 125,245	Ford	F550 Brush Truck	1FDAX57P66ED51762	5,327					
W-46	1348	OSW	Oswegatchie Fire Company	UTL-F	UTILITY UNIT (FIRE)	2006	01/23/06	\$ 44,606	Ford	F350	1FDWW31P96EB60911	32,568					
W-94	1347	OSW	Oswegatchie Fire Company	SPC-F	SPECIALITY UNIT (FIRE)	1988	09/15/89	\$ 275,093	Simon	Duplex	1D91P11J3J1008891	26,176	\$ 375,000				
CAR 1	377MHN	29	POLICE	CAR-DPW	CAR	2009	10/20/08	\$ 22,991	FORD	CROWN VICTORIA	2FAHP71V49X113715	62,203					\$ 32,151
CAR 10	10WF	29	POLICE	CRU-DPW	CRUISER	2011	09/09/10	\$ 23,068	FORD	CROWN VICTORIA	2FABP7BV88X101156	30,410					\$ 35,733
CAR 11	11WFD	29	POLICE	CRU-DPW	CRUISER	2011	07/19/11	\$ 21,631	FORD	CROWN VICTORIA	2FABP7BV7BX173224	8,599					\$ 25,076
CAR 12	12WFD	29	POLICE	CRU-DPW	CRUISER	2011	09/09/10	\$ 23,068	FORD	CROWN VICTORIA	2FABP7BV0BX101152	29,100					\$ 35,733
CAR 13	13WFD	29	POLICE	CRU-DPW	CRUISER	2011	07/19/11	\$ 21,631	FORD	CROWN VICTORIA	2FABP7BV9BX173225	4,131					\$ 25,076
CAR 14	14WFD	29	POLICE	CRU-DPW	CRUISER	2009	02/27/09	\$ 22,770	FORD	CROWN VICTORIA	2FAHP71V59X128028	77,516		\$ 33,272			
CAR 15	15WFD	29	POLICE	CAR-DPW	CAR	2008	12/18/07	\$ 23,412	FORD	EXPLORER	1FMEU73E68UA17301	18,933					\$ 32,151
CAR 16	16WFD	29	POLICE	CRU-DPW	CRUISER	2008	08/13/08	\$ 20,744	FORD	CROWN VICTORIA	2FAHP71V18X178603	53,034		\$ 33,682			
CAR 17	17WFD	29	POLICE	9999	Do Not Replace	1981	01/02/81	\$ 16,901	CHEVROLET	CORVETTE (Drug Forfeiture)	1G1AY8764BS410774	Open Odometer					
CAR 18	18WFD	29	POLICE	9999	Do Not Replace	2008	11/15/10	\$ 19,000	CHEVROLET	SILVERADO 1500 PICK-UP	3GCEK13328G306359	88,292					
CAR 19	19WFD	29	POLICE	CAR-DPW	CAR	2011	09/09/10	\$ 23,068	FORD	CROWN VICTORIA	2FABP7BV2BX101153	19,926					\$ 35,733
CAR 2	2WF	29	POLICE	CAR-DPW	CAR	2011	07/19/11	\$ 21,631	FORD	CROWN VICTORIA	2FABP7BV1BX173221	2,010					\$ 25,076
CAR 20	20WFD	29	POLICE	CRU-DPW	CRUISER	2011	09/09/10	\$ 23,068	FORD	CROWN VICTORIA	2FABP7BV7BX101150	3,151					\$ 35,733
CAR 21	21WFD	29	POLICE	SUV-DPW	SUV	2011	11/14/11	\$ 28,552	FORD	EXPEDITION	1FMJU1G55B5EF53860	18					

FLEET MANAGEMENT PLAN AS RECOMMENDED BY FIRST SELECTMAN FOR FY13 CAPITAL PROGRAM

Asset ID Number	License Plate No.	Agency Code	Agency Name	Class Code	Class Description	Asset Model Year	In-Service Date	Gross Vehicle Cost	Manufacturer	Model	Serial #	Mileage as of 10/15/11	2013	2014	2015	2016	2017
CAR 22	22WFD	29	POLICE	SUV-DPW	SUV	2005	03/02/05	\$ 29,288	FORD	EXPEDITION XLS	1FMPU16595LA65040	80,418	\$ 37,726				
CAR 23	932PHE	29	POLICE	SUV-DPW	SUV	2003	07/01/02	\$ 22,153	FORD	CROWN VICTORIA	2FAHP71W83X209380	9,915					
CAR 24	931PHE	29	POLICE	CAR-DPW*	CAR	2003	07/01/02	\$ 21,025	FORD	CROWN VICTORIA	2FAFP71W73X105891	7,681					
CAR 25	583UTH	29	POLICE	CAR-DPW*	CAR	2007	09/20/06	\$ 19,686	CHEVROLET	IMPALA	2G1WC58R479148677	74,993		\$ 34,692			
CAR 26	584UTH	29	POLICE	CAR-DPW	CAR	2007	09/20/06	\$ 19,686	CHEVROLET	IMPALA	2G1WC58R379143504	105,802	\$ 34,692				
CAR 27	873PK	29	POLICE	CAR-DPW	CAR	2009	02/11/09	\$ 19,090	FORD	ESCAPE	1FMCU92779KB46666	33,884				\$ 33,035	
CAR 28	402UX	29	POLICE	CAR-DPW	CAR	2007	09/20/06	\$ 19,686	CHEVROLET	IMPALA	2G1WC58R079139717	64,028		\$ 34,692			
CAR 29	935GHH	29	POLICE	CAR-DPW	CAR	2005	01/27/05	\$ 24,718	FORD	CROWN VICTORIA	2FAFP74W65X129341	104,263	\$ 29,423				
CAR 3	3WFD	29	POLICE	CAR-DPW	CAR	2011	07/13/11	\$ 21,631	FORD	CROWN VICTORIA	2FABP7BV5BX173223	3,985					
CAR 30	156MCM	29	POLICE	CAR-DPW	CAR	2010	11/30/09	\$ 19,701	FORD	TAURUS	1FAHP2D2W1AG112586	20,812					
CAR 31	539WNX	29	POLICE	CAR-DPW	CAR	2008	11/05/07	\$ 24,079	FORD	CROWN VICTORIA	2FAFP74V28X122849	64,824				\$ 35,733	
CAR 4	4WFD	29	POLICE	CRU-DPW	CRUISER	2011	07/13/11	\$ 21,631	FORD	CROWN VICTORIA	2FABP7BV3BX173222	3,760				\$ 25,076	
CAR 5	5WFD	29	POLICE	CRU-DPW	CRUISER	2011	09/09/10	\$ 23,068	FORD	CROWN VICTORIA	2FABP7BV6BX101155	29,115					
CAR 6	6WFD	29	POLICE	CRU-DPW	CRUISER	2008	10/30/07	\$ 20,841	FORD	CROWN VICTORIA	2FAFP71V68X118798	100,907	\$ 33,272				
CAR 7	7WFD	29	POLICE	CRU-DPW	CAR	2007	12/15/06	\$ 20,455	FORD	CROWN VICTORIA	2FAHP71W17X100992	63,382				\$ 34,692	
CAR 8	8WFD	29	POLICE	CRU-DPW	CAR	2011	09/09/10	\$ 23,068	FORD	CROWN VICTORIA	2FABP7BV4BX101154	39,116				\$ 36,624	
CAR 9	9WFD	29	POLICE	CRU-DPW	CAR	2008	10/30/07	\$ 20,841	FORD	CROWN VICTORIA	2FAHP71V48X118797	104,892	\$ 33,272				
CAR 33	236DJW	29	POLICE	9999	Do Not Replace	1993	02/27/08	\$ 18,751	Toyota - Drug Forfeiture	CAMRY	JT2SK12EXP0134941	145,211					
PD1	152WFD	29	POLICE	BOX-DPW	BOX TRAILER	1993	01/02/93	\$ 3,282	CROSS	59 TRAILER	1C9FS0917P1431062	N/A					
CV1	118WFD	29	POLICE	9999	Do Not Replace	1998	08/23/98	\$ 128,999	Ford	E450 Command Vehicle	1FDXE40F2WHB92670	50,429					
A18	45WFD	30	PUBLIC WORKS	9999	Do Not Replace	2002	04/30/02	\$ 23,656	FORD	CROWN VICTORIA	2FAFP74W62X159256	94,326					
A2	106WFD	30	PUBLIC WORKS	9999	Do Not Replace	2001	12/06/00	\$ 22,523	JEEP	CHEROKEE	1J4F48551LS65909	130,067					
A8	110WFD	30	PUBLIC WORKS	9999	Do Not Replace	2003	01/20/03	\$ 21,759	FORD	CROWN VICTORIA	2FAFP71W03X140532	76,851					
H1	N/A	30	PUBLIC WORKS	FRK-DPW	FORK LIFT	1967	01/20/99	\$ 17,062	HYSTER - Purchased Used	S20A Fork Lift	A10D5889L	1,833/Hrs.					
H11	81WFD	30	PUBLIC WORKS	PCK-DPW	PICK UP	2007	09/27/06	\$ 29,837	FORD	F250 4X4	1FTSF21P87EA84225	53,166					
H12	80WFD	30	PUBLIC WORKS	PCK-DPW	PICK UP	2003	01/02/03	\$ 36,000	FORD	F450 4x2	1FDXF46F63EB17892	30,237				\$ 45,268	
H13	79WFD	30	PUBLIC WORKS	PCK-DPW	PICK UP	2002	11/09/01	\$ 25,000	DODGE	RAM 2500	3B7KF26Z62M228318	126,034				\$ 44,166	
H14	129WFD	30	PUBLIC WORKS	MDD-DPW	MEDIUM DUTY DUMP	1999	03/25/99	\$ 73,731	IH	4X2 - 4,700 GVW DUMP	1HTSCAAL5XH668941	75,108	\$ 78,942				
H147	147WFD	30	PUBLIC WORKS	RLL-DPW	ROLLER	1991	06/07/91	\$ 2,194	ROSCO	TRAILER	1YB411327M1B17617	N/A					
H15	121WFD	30	PUBLIC WORKS	MDD-DPW	MEDIUM DUTY DUMP	1999	03/25/99	\$ 73,731	IH	4X2 - 4,700 GVW DUMP	1HTSCAAL3XH668940	88,969	\$ 78,599				
H16	123WFD	30	PUBLIC WORKS	MDD-DPW	MEDIUM DUTY DUMP	2002	11/09/01	\$ 90,377	IH	4X2 - 4,900 GVW DUMP	1HTSDAAL2XH529376	65,543				\$ 111,523	
H17	83WFD	30	PUBLIC WORKS	MDD-DPW	MEDIUM DUTY DUMP	2003	04/16/03	\$ 73,982	STERLING	L 7500	2FZAARBS03AL76553	51,900					
H18	125WFD	30	PUBLIC WORKS	MDD-DPW	MEDIUM DUTY DUMP	2010	06/17/09	\$ 99,117	IH	4300 SBA Dump	1HTMMAAL8AH188776	11,000					
H19	78WFD	30	PUBLIC WORKS	PCK-DPW	PICK UP	1997	12/09/97	\$ 19,829	FORD	F-250 4X4	1THF26H7VEC72176	128,644					
H24	56WFD	30	PUBLIC WORKS	SWP-DPW	SWEEPER	2003	10/18/02	\$ 118,670	ELGIN	PELICAN P SWEEPER	P-3928-S	10,551					
H25	57WFD	30	PUBLIC WORKS	9999	Do Not Replace	1994	08/20/94	\$ 117,071	ELGIN	PELICAN SWEEPER	S-8054-S	16,878					
H26	62WFD	30	PUBLIC WORKS	9999	Do Not Replace	1959	01/01/59	N/A	FERGUSON	TO-35 TRACTOR MOWER	SGM 193199	4,929/Hrs.					
H27	63WFD	30	PUBLIC WORKS	TRC-DPW	TRACTOR	2008	05/20/08	\$ 82,026	JOHN DEERE	MODEL 5603 TRACTOR	LV5603R268632	1,327/Hrs.					
H30	87WFD	30	PUBLIC WORKS	SDMP-DPW	5 TON DUMP TRUCK	2000	06/01/00	\$ 124,378	FRIGHTLINER	5-TON DUMP - FL80	1V6UJBB2YH17532	59,672					
H31	88WFD	30	PUBLIC WORKS	SDMP-DPW	5 TON DUMP TRUCK	2000	05/23/00	\$ 86,950	IH	4X2 - 4,900 GVW DUMP	1HTSDAAR2YH288328	69,570					
H32	95WFD	30	PUBLIC WORKS	SDMP-DPW	5 TON DUMP TRUCK	2000	05/31/00	\$ 86,950	IH	4X2 - 4,900 GVW DUMP	1HTSDAAR4YH288329	70,951					
H34	94WFD	30	PUBLIC WORKS	SDMP-DPW	5 TON DUMP TRUCK	2004	03/19/04	\$ 91,116	STERLING	L7500	2FZAATDC84AN04719	25,142					
H35	92WFD	30	PUBLIC WORKS	SDMP-DPW	5 TON DUMP TRUCK	1990	12/01/89	\$ 61,468	IH	5-TON DUMP	1HTGBDBR3LH255911	133,857					
H36	124WFD	30	PUBLIC WORKS	SDMP-DPW	5 TON DUMP TRUCK	1996	04/01/96	\$ 47,505	IH	4X2 - 4,900 GVW DUMP	1HTSDAAR2TH409125	56,918				\$ 139,511	
H37	131WFD	30	PUBLIC WORKS	SDMP-DPW	5 TON DUMP TRUCK	1997	12/01/96	\$ 50,524	IH	4X2 - 4,900 GVW DUMP	1HTSDAAR1VH461302	58,293				\$ 141,603	
H38	130WFD	30	PUBLIC WORKS	SDMP-DPW	5 TON DUMP TRUCK	1996	06/01/96	\$ 47,505	IH	4X2 - 4,900 GVW DUMP	1HTSDAAR4TH409126	52,179				\$ 139,511	
H39	96WFD	30	PUBLIC WORKS	SDMP-DPW	5 TON DUMP TRUCK	2009	07/23/08	\$ 115,800	INTERNATIONAL	7400 SFA	1HTWDAAR29J077349	11,524					
H40	97WFD	30	PUBLIC WORKS	SDMP-DPW	5 TON DUMP TRUCK	2003	06/13/03	\$ 91,116	STERLING	L 7500	2FZAATAK93AL96265	34,943					
H42	99WFD	30	PUBLIC WORKS	216SPC-DPW	18-YEAR SPECIALITY UNIT	2010	12/31/09	\$ 104,350	INTERNATIONAL	7600 SFA 6x4	1HSWXJS1BA273491	15,892					
H43DT	161WFD	30	PUBLIC WORKS	TRL-DPW	TRAILER	2001	09/14/00	\$ 26,375	FRUEHAUF	DUMP TRAILER	1JU26F615737748	N/A					
H43LB	165WFD	30	PUBLIC WORKS	9999	Do Not Replace	1954	01/02/54	\$ 1,295	FRUEHAUF	LOW BED TRAILER	FW96419	N/A					
H43RO	151WFD	30	PUBLIC WORKS	180SPC-DPW	15-YEAR SPECIALITY UNIT	2000	11/10/00	\$ 14,925	BENLEE	TRAILER TA60TC34	189A13420YB183504	8,422/Hrs.				\$ 56,396	
H44	158WFD	30	PUBLIC WORKS	CHP-DPW	CHIPPER	2005	11/18/04	\$ 24,000	VERMEER	BC1000XL - CHIPPER	1VRY1119X51005548	837/Hrs.					
H45	60WFD	30	PUBLIC WORKS	BKH-DPW	BACKHOE	2009	03/15/10	\$ 89,180	VOLVO	BL70 BACKHOE/LOADER	VCEOB70A00020080	794/Hrs.					
H47	58WFD	30	PUBLIC WORKS	LDR-DPW	TRACTOR/MOWER	2008	09/09/08	\$ 39,900	JOHN DEERE	S603 LOADER/TRACTOR	LV5603R270211	771/Hrs.					
H-33	135WFD	30	PUBLIC WORKS	GRN-DPW	STUMP GRINDER	2004	11/12/04	\$ 23,651	CARLTON	7500 - Stump Grinder	1JG642122C1167474	96/Hrs.					
H48	N/A	30	PUBLIC WORKS	RLL-DPW	ROLLER	1991	07/07/91	\$ 12,950	ROSCO	5-7 TON ROLLER	\$ 34,578	805/Hrs.					
H49	N/A	30	PUBLIC WORKS	9999	Do Not Replace	1970	01/02/70	\$ 36,000	INGRAM	5-7 TON ROLLER	401073F27	1543/Hrs.					
H50	101WFD	30	PUBLIC WORKS	VAC-CC	Vac-All Cab and Chassis	2009	04/30/08	\$ 63,890	IH	7400 SFA 4x2	1HTWCAAR79J077348	3,078					

FLEET MANAGEMENT PLAN AS RECOMMENDED BY THE FIRST SELECTMAN FOR FY13 CAPITAL PROGRAM

Asset ID Number	License Plate No.	Agency Code	Agency Name	Class Code	Class Description	Asset Model Year	In-Service Date	Gross Vehicle Cost	Manufacturer	Model	Serial #	Mileage as of 10/15/11	2013	2014	2015	2016	2017
H-50V	N/A	30	PUBLIC WORKS	VAC-BODY	Vac-All Body	2004	04/22/04	\$109,898	Vac-All Body for H-50	LV10C	N/A	1,643/Hrs.	\$ 123,691				
H51	90WFD	30	PUBLIC WORKS	216SPC-DPW	18-YEAR SPECIALITY UNIT	2010	06/04/10	\$108,550	IH	Model AL60 Aerial Lift	1HTMMAAR3AH247768	3,288					
H52	162WFD	30	PUBLIC WORKS	CMP-DPW	COMPRESSOR	2004	12/16/04	\$ 14,420	INGERSOLL RAND	P185WJD COMPRESSOR	350036GU0221	407/Hrs.					
H53	66WFD	30	PUBLIC WORKS	PLW-DPW	SIDEWALK SNOW PLOW	2003	12/12/03	\$ 81,897	TRACKLESS	MTSTD SIDEWALK PLOW	MTSTD-2330	1,121/Hrs.					
H54	67WFD	30	PUBLIC WORKS	PLW-DPW	SIDEWALK SNOW PLOW	1986	11/01/86	\$ 32,977	BOMBARDIER	SW-48FA	\$ 1,860,984	978/Hrs.					\$ 72,692
H55	127WFD	30	PUBLIC WORKS	CHP-DPW	CHIPPER	1998	04/21/98	\$ 17,045	VERMEER CHIPPER	BC1230	1VRK15152W1001604	860/Hrs.					
H56	N/A	30	PUBLIC WORKS	9999	Do Not Replace	1977	01/01/77	\$ 2,770	LAYTON	D.550B SUPER PAVER	D7621-K-6	No Hour Meter					
H58	146WFD	30	PUBLIC WORKS	CMP-DPW	COMPRESSOR	2008	10/14/08	\$ 16,985	Ingersoll Rand	XP185WJD Port. Compressor	402988UGS222	63/Hrs.					
H6	108WFD	30	PUBLIC WORKS	PCK-DPW	PICK UP	2005	01/24/05	\$ 30,175	FORD	F250 4X4	1FTSF21P45EB72105	101,400					
H60	64WFD	30	PUBLIC WORKS	9999	Do Not Replace	1970	01/02/70	\$ 34,775	CLEVELAND	D-560 GRADER	70-560B-138-3696	1,984/Hrs.					
H64	61WFD	30	PUBLIC WORKS	LDR-DPW	LOADER	1997	01/10/97	\$110,238	VOLVO	L90C LOADER WHEEL	TD63KBE12630	5,931/Hrs.					\$ 58,888
H65	59WFD	30	PUBLIC WORKS	LDR-DPW2	LOADER	2004	09/07/04	\$115,135	VOLVO	L90E LOADER	D6DLAE2969011	3,608/Hrs.					
H77	168WFD	30	PUBLIC WORKS	BOX-DPW	BOX TRAILER	1993	06/14/93	\$ 3,282	CROSS COUNTRY	59 PAPER TRAILER	1C9FS0919P1431063	N/A	\$ 5,500				
H9	77WFD	30	PUBLIC WORKS	180SPC-DPW	15-YEAR SPECIALITY UNIT	2011	11/24/10	\$ 76,014	IH	4300 SBA 4x2 w/Platform Body	1HTMMAAL9BH388342	5,512					
H43T	164WFD	30	PUBLIC WORKS	UTL-F	UTILITY UNIT (FIRE)	2006	10/05/05	\$ 37,398	TOWMASTER	T-70DTG DUMP TRAILER	4KNTF22276L160354	N/A					
S19	72WFD	30	PUBLIC WORKS	9999	Do Not Replace	2001	04/09/01	\$141,824	VOLVO	WX64 PACKER	4V2DC6UEX1N321692	123,790					
S20	71WFD	30	PUBLIC WORKS	SLP-DPW	SIDE LOAD PACKER	2007	01/16/07	\$194,875	VOLVO AUTOCAR	WX-64 PACKER	5VCDGMF67H204567	66,570		\$ 240,425			
S21	93WFD	30	PUBLIC WORKS	SLP-DPW	SIDE LOAD PACKER	2007	01/16/07	\$191,749	VOLVO AUTOCAR	WX-64 PACKER	5VCDGMF57H203734	64,318		\$ 240,425			
S22	70WFD	30	PUBLIC WORKS	SLP-DPW	SIDE LOAD PACKER	2011	03/15/11	\$218,336	PETERBUILT	McNEILUS SIDE LOADER	3BPZL50X9BF129187	6,931					\$ 255,067
S23	73WFD	30	PUBLIC WORKS	RLP-DPW	REAR LOAD PACKER	2009	09/17/08	\$131,557	IH	REFUSE PACKER	1HTWGAAT19J093030	10,741					\$ 133,413
S24	74WFD	30	PUBLIC WORKS	RLP-DPW	REAR LOAD PACKER	2009	09/19/08	\$131,557	IH	REFUSE PACKER	1HTWGAAT39J093031	13,116					\$ 133,413
S25	75WFD	30	PUBLIC WORKS	RCY-DPW	RECYCLING TRUCK	2011	03/03/11	\$218,336	PETERBUILT	McNEILUS SIDE LOADER	3BPZL50X7BF129186	2,696					
S26	76WFD	30	PUBLIC WORKS	RCY-DPW	RECYCLING TRUCK	2011	03/03/11	\$218,336	PETERBUILT	McNEILUS SIDE LOADER	3BPZL50X0BF129188	5,005					
S28	N/A	30	PUBLIC WORKS	9999	Do Not Replace	1993	10/14/93	\$114,650	DRESSER	TD15E CRAWLER/DOZER	4450009P030608	12,125/Hrs.					
S29	69WFD	30	PUBLIC WORKS	FLP-DPW	FRONT LOAD PACKER	2001	05/27/01	\$149,165	VOLVO	WX64 FRONT LOADER	4V2DC6UE1N321693	96,222					\$ 131,392
S30	65WFD	30	PUBLIC WORKS	B&E-DPW	BULLDOZER/EXCAVATOR	2001	12/28/01	\$159,000	VOLVO	EW170 EXCAVATOR	\$ 26,289,408	80,048/Hrs.					\$ 264,703
H78	171WFD	30	PUBLIC WORKS	9999	Do Not Replace	1974	01/01/74	Homemade	COX	7214 SMBT	W-23	533/Hrs.					
UT2	163WFD	30	PUBLIC WORKS	TRL-DPW	TRAILER	2009	07/27/09	\$ 11,857	CAM	8CAM824DOTT - Utility Trailer	5JPB1U92X9P024572	N/A					
A20	104WFD	30	PUBLIC WORKS	9999	Do Not Replace	1992	07/01/92	\$ 26,946	GMC	Suburban	1GKGK26K9NJ721324	120,320					
A7	107WFD	PZ/BH	CAR-DPW	CAR		2012	10/13/11	\$ 26,928	FORD	FUSION HYBRID	3FADPOL3XCR187566	1					
A9	111WFD	PZ/BH	CAR-DPW	CAR		2012	10/13/11	\$ 26,928	FORD	FUSION HYBRID	3FADPOL31CR187567	95					
A10	112WFD	PZ/BH	SUV-DPW	SUV		2011	11/18/10	\$ 18,433	FORD	ESCAPE	1FMCU9C7XBAK75810	3,000					
A3	103WFD	PZ/BH	9999	CAR		2003	11/30/03	\$ 22,153	FORD	CROWN VICTORIA	2FAHP71W83X209377	106,258	\$ 14,271				
A6	128WFD	PZ/BH	CAR-DPW	CAR		2003	06/23/03	\$ 22,153	FORD	CROWN VICTORIA	2FAHP71W13X209379	98,279	\$ 14,271				
W-200	590	QH	Quaker Hill Fire Company	9999	Do Not Replace	2004	09/28/04	\$144,000	Ford	Ambulance - E450	1FDX4E5P048H29713	30,813					
W-21	N/A	QH	Quaker Hill Fire Company	1500PMP-F	1500 GPM PUMPER (FIRE)	2006	10/02/06	\$396,447	FERRARA	Igniter Custom Pumper	1F94047266H140373	11,705					
W-23	1025	QH	Quaker Hill Fire Company	BRS-F	BRUSH TRUCK (FIRE)	2006	07/01/06	\$125,245	Ford	F550 Brush Truck	1FDAX57P86D51763	2,934					
W-25	N/A	QH	Quaker Hill Fire Company	1500PMP-F	1500 GPM PUMPER (FIRE)	1993	11/01/93	\$372,000	Pierce	1,500 GPM Pumper/Aerial	4P1CT02VXPA000631	27,199		\$ 720,690			
W-26	1285	QH	Quaker Hill Fire Company	UTL-F	UTILITY UNIT (FIRE)	2006	02/01/06	\$ 44,076	Ford	F350	1FDWW31P76EB60910	21,162					
P74	N/A	37	REC/PARKS	TRP RAKE	TRAP RAKE	2005	07/11/05	\$ 11,505	Toro	Sand Pro 3020	\$ 250,000,494	546/Hrs.					\$ 14,318
P50	42WFD	37	REC/PARKS	9999	Do Not Replace	1980			JOHN DEERE	1050 TRACTOR	1D50S003477	4,308/Hrs.					
P51	119WFD	37	REC/PARKS	TRC-DPW	TRACTOR	2003	09/30/03	\$ 31,317	JOHN DEERE	4710 TRACTOR	CWV1JD4510AS	1,238/Hrs.					
P52	N/A	37	REC/PARKS	9999	Do Not Replace	1987			JOHN DEERE	332 TRACTOR	M0032C424601	1,179/Hrs.					
P53	N/A	37	REC/PARKS	MOW LG	MOWER, LARGE	2006	09/22/06	\$ 44,168	TORO	GROUNDMASTER 4100	\$ 260,000,727	1,938/Hrs.					\$ 52,427
P54	N/A	37	REC/PARKS	MOW-DPW	MOWER	2003	08/25/03	\$ 12,919	FERRIS	IS 5000 72" Mower	\$ 682	1,227/Hrs.					
P55	N/A	37	REC/PARKS	MOW LG	MOWER, LARGE	2005	05/13/05	\$ 42,511	TORO	4000D Mower	3841024000562	2,017/Hrs.					\$ 49,421
P60	N/A	37	REC/PARKS	MOW LG	MOWER, LARGE	2002	07/12/02	\$ 37,344	TORO	455D Mower	3045520000247	2,023/Hrs.	\$ 46,300				
P61	43WFD	37	REC/PARKS	MOW2-DPW	MOWER	1997	02/07/97	\$ 21,690	KABOTA	L4200 Mower	3022829540	1,866/Hrs.	\$ 44,559				
P62	N/A	37	REC/PARKS	MOW2-DPW	MOWER	2000	07/19/00	\$ 19,341	TORO	325D Mower	200000278	13,351/Hrs.	\$ 24,548				
P63	N/A	37	REC/PARKS	MOW2-DPW	MOWER	2000	04/24/00	\$ 19,341	TORO	325D Mower	200000310	13,631/Hrs.					\$ 22,916
P64	N/A	37	REC/PARKS	SED-DPW	SEEDER	2006	07/12/06	\$ 10,628	SEEDA-VATOR	SE-60E	483	504/Hrs.					\$ 15,280
P65	N/A	37	REC/PARKS	MOW2-DPW	MOWER			\$ 31,291					\$ 31,291				
R1	32WFD	37	REC/PARKS	9999	Do Not Replace	2003	01/20/03	\$ 21,759	FORD	CROWN VICTORIA	2F AFP71W03X140529	87,156					
R10	36WFD	37	REC/PARKS	PCK-DPW	PICK UP	2001	03/17/00	\$ 20,662	DODGE	RAM 2500 4X4	3B7KF26221M256325	103,198		\$ 40,620			
R11	37WFD	37	REC/PARKS	PCK-DPW	PICK UP	2004	02/06/04	\$ 27,037	CHEVROLET	SMALL DUMP	1G BJK34224E259100	59,850					
R14	38WFD	37	REC/PARKS	MDD-PKS	MEDIUM DUTY DUMP	2009	03/13/09	\$ 43,799	FORD	F450 DUMP	1FDA47R49E0A8998	18,929					
R15	100WFD	37	REC/PARKS	PCK-DPW	PICK UP	2008	01/29/08	\$ 35,358	FORD	F250	1FTSF21RX8EC87491	42,652					
R16	44WFD	37	REC/PARKS	PCK-PKS	PICK UP	2008	01/11/08	\$ 35,358	FORD	F250 PICK-UP	1FTSF21R88EC87490	31,884					
R2	33WFD	37	REC/PARKS	SUV-DPW	SUV	2009	05/27/09	\$ 19,932	FORD	ESCAPE - Utility Body	1FMCU93G79KC38309	11,943					\$ 25,249

FLEET MANAGEMENT PLAN AS RECOMMENDED BY THE FIRST SELECTMAN FOR FY13 CAPITAL PROGRAM

Asset ID Number	License Plate No.	Agency Code	Agency Name	Class Code	Class Description	Asset Model Year	In-Service Date	Gross Vehicle Cost	Manufacturer	Model	Serial #	Mileage as of 10/15/11	2013	2014	2015	2016	2017
R6	41WFD	37	REC/PARKS	9999	Do Not Replace	1999	01/02/99	\$ 27,530	DODGE	3,500 VAN	2B5WB35Z3XK581749	73,849					
R8	34WFD	37	REC/PARKS	SUV-DPW	SUV	2004	08/20/04	\$ 18,000	CHEVROLET	Silverado	1GCEK14TO4Z260345	77,938	\$ 38,763				
R9	35WFD	37	REC/PARKS	MDD-DPW	MEDIUM DUTY DUMP	2006	03/08/06	\$ 35,175	GMC	SIERRA	1GDKJ34D66E163756	49,092					
R30	31934	35	SRCTZ	VAN-DPW	VAN	2009	10/21/08	\$ 46,912	FORD/SUPREME	CANDIDATE BUS	1FD3E35L38DB23570	43,449					
R4	26820	35	SRCTZ	VAN-DPW	VAN	2009	09/16/09	\$ 51,975	FORD/SUPREME	SENATOR BUS WITH LIFT	1FDEE35S49DA64658	28,275					
R5	23979	35	SRCTZ	VAN-DPW	VAN	2006	10/31/06	\$ 43,960	FORD	E350 CUTAWAY VAN	1FDWE35S16DB03982	75,772					\$ 56,979
C1	53WFD	31	WUC	SUV-DPW	SUV	2000	01/02/00	\$ 22,670	FORD	EXPLORER	1FMZU71XOYUA60411	52,790					
C2	116WFD	31	WUC	SUV-DPW	SUV	2011	03/29/11	\$ 26,617	FORD	EXPEDITION XLT 4x4	1FMJU1G51BEF25120	3,215					
C3	51WFD	31	WUC	240SPC-DPW	20-YEAR SPECIALITY UNIT	2012	On Order	\$ 244,676	INTERNATIONAL	7,500 SBA With Hi-Vac Attach							
T5	46WFD	31	WUC	PCK-DPW	PICK UP	2006	06/20/06	\$ 20,939	FORD	F-150	1FTPX14546NB57564	35,207					
PG1	144WFD	31	WUC	GEN-DPW	GENERATOR	1982	01/02/82	\$ 19,125	ONAN	TRAILER	1013131	N/A	\$ 25,000				
PG2	179WFD	31	WUC	GEN-DPW	GENERATOR	1991	01/02/91	\$ 20,000	ONAN	TRAILER	16MG1067LD017031	N/A	\$ 25,000				
PP1	145WFD	31	WUC	PMP-DPW	PUMP	1985	01/02/85	\$ 21,250	GORMAN RUPP	PUMP TRAILER	774243	N/A	\$ 26,200				
PP2	148WFD	31	WUC	PMP-DPW	PUMP	1990	01/02/90	\$ 21,250	GORMAN RUPP	PUMP TRAILER	968976N	N/A	\$ 26,200				
T1	48WFD	31	WUC	PCK-DPW	PICK UP	2006	06/20/06	\$ 35,665	CHEVROLET	SILVERADO PICK-UP 2500	16BHK24DX6E251727	38,856					
T2	49WFD	31	WUC	PCK-DPW	PICK UP	2011	07/27/11	\$ 55,594	GMC	K-3500 SIERRA 4wd	16D322CL8BF24396	1,700					
T3	82WFD	31	WUC	PCK-DPW	PICK UP	2004	01/31/04	\$ 28,597	CHEVROLET	2500HD PICK-UP	1GBHK24UX4E178792	98,892					
T4	52WFD	31	WUC	PCK-DPW	PICK UP	2003	10/23/02	\$ 30,768	CHEVROLET	2500HD PICK-UP	1GBHK24U63E12220	119,141					\$ 45,761

FY2013 FLEET MANAGEMENT REPLACEMENTS AND REQUESTS / VARIANCES AS RECOMMENDED BY THE FIRST SELECTMAN

DEPARTMENT	CURRENT YEAR PLAN REPLACEMENTS	SCHEDULED VALUE	1st SELECTMAN'S Recommendation	VEHICLE ID	CURRENT MILEAGE	REPAIR COSTS HISTORY	VARIANCE REQUESTS
ASSESSOR	NO FY13 REPLACEMENTS						
TOTAL ASSESSOR:		\$0	\$0				
BOARD OF EDUCATION	2003 FORD F250 2002 KAWASAKI MULE	\$39,730 \$18,900	\$39,730 \$0	BOE4 BOE11	62,100 2,859/hrs	\$5,578 N/A	NONE Defer Replacement to FY14
TOTAL BOARD OF EDUCATION:		\$58,630	\$39,730				
COHANZIE FIRE COMPANY	NO FY13 REPLACEMENTS						
TOTAL COHANZIE FIRE CO.:		\$0	\$0				
FIRST SELECTMAN	NO FY13 REPLACEMENTS						
TOTAL FIRST SELECTMAN:		\$0	\$0				
FIRE SERVICES	2005 FORD CROWN VICTORIA 2005 FORD EXPEDITION 2005 FORD CROWN VICTORIA	\$33,190 \$33,353 \$33,353	\$0 \$33,353 \$0	Car 105 Car 55 Car 85	36,521 88,888 33,546	\$1,689 \$6,375 \$1,241	Defer Replacement to FY14 NONE Defer Replacement to FY15
TOTAL FIRE SERVICES:		\$99,896	\$33,353				
GOSHEN FIRE COMPANY	1990 1,500 GPM SIMON PUMPER	\$451,329	\$451,329	W-31	41,493	\$50,119	NONE - has been deferred for three years
TOTAL GOSHEN:		\$451,329	\$451,329				
JORDAN FIRE COMPANY	NO FY13 REPLACEMENTS						
TOTAL JORDAN FIRE:		\$0	\$0				
OSWEGATCHIE FIRE COMPANY	1988 SIMON DUPLEX	\$512,000	\$375,000	W-94	26,176	\$13,449	Reduce Cost and Approve Downsizing
TOTAL OSWEGATCHIE:		\$512,000	\$375,000				
POLICE DEPARTMENT	2005 FORD EXPEDITION 2005 FORD CV-CAR 2008 FORD CV-CRUISER 2008 FORD CV-CRUISER 1993 CROSS BOX TRAILER	\$37,726 \$29,423 \$33,272 \$33,272 \$3,500	\$0 \$29,423 \$33,272 \$33,272 \$0	CAR 22 CAR 29 CAR 6 CAR 9 PD1	80,418 104,263 100,907 104,892 N/A	\$6,715 \$3,932 \$9,001 \$7,283 N/A	Defer Replacement to FY14 NONE NONE NONE Defer Replacement to FY18
TOTAL POLICE:		\$137,193	\$95,967				
PUBLIC WORKS	1993 CROSS BOX TRAILER	\$5,500	\$5,500	H77	N/A	582.35	NONE
TOTAL PUBLIC WORKS:		\$5,500	\$5,500				
P/Z, BH & TOWN HALL ADM.	2003 FORD CROWN VICTORIA 2003 FORD CROWN VICTORIA	Re-assigned Re-assigned	\$14,271 \$14,271	A3 A6	106,258 99,279	\$3,921 \$3,105	New vehicle in place of re-assigned as per Plan New vehicle in place of re-assigned as per Plan
TOTAL P/Z, BH & TOWN ADM:		\$0	\$28,542				
QUAKER HILL FIRE COMPANY	NO FY13 REPLACEMENTS						
TOTAL QUAKER HILL FIRE CO.:		\$0	\$0				

FY2013 FLEET MANAGEMENT REPLACEMENTS AND REQUESTED VARIANCES AS RECOMMENDED BY THE FIRST SELECTMAN

DEPARTMENT	CURRENT YEAR PLAN REPLACEMENTS	SCHEDULED VALUE	1st SELECTMAN'S Recommendation	VEHICLE ID	CURRENT MILEAGE	REPAIR COSTS HISTORY	VARIANCE REQUESTS
RECREATION & PARKS							
	2002 TORO 455D LG. MOWER	\$46,300	\$0	P60	2,023/HRS.	\$4,983	Defer Replacement to FY14
	1997 KUBOTA TRACTOR/MOWER	\$44,559	\$0	P61	1,866/HRS.	\$5,310	Defer Replacement to FY14
	2000 TORO 325D SMALL MOWER	\$0	\$24,548	P62	13,351/HRS.	\$2,947	Move Replacement from FY16 to FY13
	NEW MULTI-USE MOWER	\$0	\$31,291	P65			Add New Mower to Plan to Facilitate Operations
	2004 CHEVROLET SILVERADO	\$38,763	\$0	R8	77,938	\$4,088	Defer Replacement to FY14
TOTAL RECREATION & PARKS:		\$129,622	\$55,839				
SENIOR SERVICES	NO FY13 REPLACEMENTS						
TOTAL SENIOR SERVICES:		\$0	\$0				
UTILITY COMMISSION							
	1982 ONAN TRAILER/GEN	\$25,000	\$0	PG1	320/HRS.	N/A	Defer Replacement to FY14
	1991 ONAN TRAILER/GEN	\$25,000	\$0	PG2	192/HRS.	N/A	Defer Replacement to FY14
	1985 GORMAN RUPP PUMP	\$26,200	\$0	PP1	150/HRS.	N/A	Defer Replacement to FY14
	1990 GORMAN RUPP PUMP	\$26,200	\$0	PP2	125.6/HRS.	N/A	Defer Replacement to FY14
TOTAL UTILITY COMMISSION:		\$102,400	\$0				
GRAND TOTAL PLAN TO F/S RECOMMENDATION:		\$1,496,570	\$1,085,260				

FLEET MANAGEMENT BASELINE REPORT FOR FISCAL YEARS 2013-2017

Asset ID Number	License Plate No.	Agency Code	Agency Name	Class Code	Class Description	Asset Model Year	In-Service Date	Gross Vehicle Cost	22	9	12	16	24
									\$ 1.56	\$ 1.61	\$ 0.90	\$ 1.80	\$ 2.99
									\$ 1,561,629	\$ 1,608,167	\$ 901,224	\$ 1,799,284	\$ 2,987,665
A6	114WFD	4	ASSESSOR	9999	POLICE CAR	2003	07/01/02	\$21,025					
BOE1	23WFD	60	Board of Ed	PCK-F	PICK UP AND JEEP (FIRE)	2002	08/01/02	\$25,583	\$ 39,926				
BOE2	24WFD	60	Board of Ed	VAN-DPW	VAN	2005	04/21/05	\$29,056					
BOE3	25WFD	60	Board of Ed	PCK-F	PICK UP AND JEEP (FIRE)	2001	11/01/00	\$25,817					
BOE4	26WFD	60	Board of Ed	PCK-F	PICK UP AND JEEP (FIRE)	2003	06/01/03	\$23,697	\$ 39,730				
BOE5	27WFD	60	Board of Ed	VAN-DPW	VAN	2011	01/20/11	\$18,425					
BOE6	28WFD	60	Board of Ed	PCK-F	PICK UP AND JEEP (FIRE)	2008	10/26/07	\$31,800					
BOE7	29WFD	60	Board of Ed	PCK-F	PICK UP AND JEEP (FIRE)	2005	01/10/05	\$25,679				\$ 41,828	
BOE8	30WFD	60	Board of Ed	MDD-DPW	MEDIUM DUTY DUMP	2000	02/01/00	\$25,336				\$ 69,217	
BOE9	117WFD	60	Board of Ed	PCK-F	PICK UP AND JEEP (FIRE)	2005	01/11/05	\$24,913				\$ 41,828	
BOE10	N/A	60	Board of Ed	SKD STR	SKID STEER LOADER	2007	05/11/07	\$22,000					\$29,064
BOE11	N/A	60	Board of Ed	UTL-DPW	UTILITY CART	2002	09/20/02	\$13,649	\$ 18,900				
W-500	1360	COH	Cohanzie Fire Company	9999		2009	05/20/09	\$192,168					
W-52 aka W-51	1028	COH	Cohanzie Fire Company	1500PMP-F	1500 GPM PUMPER (FIRE)	1996	07/12/96	\$280,055		\$ 485,979			
W-53	817	COH	Cohanzie Fire Company	BRS-F	BRUSH TRUCK (FIRE)	2009	05/13/09	\$95,854					
W-55	2655	COH	Cohanzie Fire Company	T&A-F	TOWER LADDER/AERIAL (FIRE)	2004	12/29/04	\$667,929					
W-56	1282	COH	Cohanzie Fire Company	UTL-F	UTILITY UNIT (FIRE)	2010	10/06/10	\$47,426					
W-57	595	COH	Cohanzie Fire Company	SPC-F	SPECIALITY UNIT (FIRE)	2010	10/05/10	\$511,983					
Car 105 (Q105)	105WFD	23/FM	FIRE MARSHAL	CAR-F2	CAR (FIRE)	2005	07/15/05	\$19,886	\$ 33,190				
Car 115 (Q115)	115WFD	23/FM	FIRE MARSHAL	CAR-F2	CAR (FIRE)	2006	01/17/06	\$20,696		\$ 33,682			
Car 55 (Q55)	55WFD	23/FM	FIRE MARSHAL	CAR-F	CAR (FIRE)	2005	09/15/04	\$26,464	\$ 33,353				
Car 85 (Q85)	85 WFD	23/FM	FIRE MARSHAL	CAR-F	CAR (FIRE)	2005	10/25/04	\$22,747	\$ 33,353				
A1	1WFD	1	FIRST SELECTMAN	CAR-DPW	CAR	2011	10/06/10	\$19,658					
Avon	N/A	GOS	Goshen Fire Company	9999		1991							
Boat Trailer	1957	GOS	Goshen Fire Company	TRL-F	TRAILER (FIRE)	2008	11/02/07	\$4,900					
Utility Trailer	N/A	GOS	Goshen Fire Company	TRL-F	TRAILER (FIRE)	1999	11/01/89						
W-300	1340	GOS	Goshen Fire Company	9999		2002	09/27/02	\$140,000					
W-31	1303	GOS	Goshen Fire Company	1500PMP-F	1500 GPM PUMPER (FIRE)	1990	03/12/91	\$250,150	\$ 451,329				
W-32	371	GOS	Goshen Fire Company	1500PMP-F	1500 GPM PUMPER (FIRE)	1998	02/23/99	\$307,935					\$524,519
W-33	2654	GOS	Goshen Fire Company	SPC-F1	SPECIALITY UNIT (FIRE)	1989	08/12/89	\$146,282					
W-36	1494	GOS	Goshen Fire Company	UTL-F	UTILITY UNIT (FIRE)	2007	08/29/06	\$36,973					
W-93	N/A	GOS	Goshen Fire Company	BOT-F	BOAT (FIRE)	2007	06/29/07	\$103,334					
W-100	600	JOR	Jordan Fire Company	9999		2004	09/28/04	\$144,000					
W-11	1451	JOR	Jordan Fire Company	1500PMP-F	1500 GPM PUMPER (FIRE)	1993	01/02/94	\$245,093					
W-12	1452	JOR	Jordan Fire Company	9999		1984	08/01/84	\$160,000					
W-15	1454	JOR	Jordan Fire Company	T&A-F	TOWER LADDER/AERIAL (FIRE)	1996		\$499,354				\$ 964,795	
W-16	1484	JOR	Jordan Fire Company	UTL-F	UTILITY UNIT (FIRE)	2005	04/22/05	\$43,594					\$50,552
W-400	589	OSW	Oswegatchie Fire Company	9999		2008	01/08/08	\$192,378					
W-41	1103	OSW	Oswegatchie Fire Company	1500PMP-F	1500 GPM PUMPER (FIRE)	1998	02/16/99	\$279,030					\$524,519
W-42	1349	OSW	Oswegatchie Fire Company	1500PMP-F	1500 GPM PUMPER (FIRE)	2007	05/14/08	\$416,972					
W-43	1026	OSW	Oswegatchie Fire Company	BRS-F	BRUSH TRUCK (FIRE)	2006	06/06/06	\$125,245					
W-46	1348	OSW	Oswegatchie Fire Company	UTL-F	UTILITY UNIT (FIRE)	2006	01/23/06	\$44,606					
W-94	1347	OSW	Oswegatchie Fire Company	SPC-F	SPECIALITY UNIT (FIRE)	1988	09/15/89	\$275,093	\$ 512,000				
CAR 1	377MHN	29	POLICE	CAR-DPW	CAR	2009	10/20/08	\$22,991					\$32,151
CAR 10	10WFD	29	POLICE	CRU-DPW	CRUISER	2010	09/09/10	\$23,068				\$ 35,733	
CAR 11	11WFD	29	POLICE	CRU-DPW	CRUISER	2011	07/19/11	\$21,631					\$37,631
CAR 12	12WFD	29	POLICE	CRU-DPW	CRUISER	2011	09/09/10	\$23,068				\$ 35,733	
CAR 13	13WFD	29	POLICE	CRU-DPW	CRUISER	2011	07/19/11	\$21,631					\$33,035
CAR 14	14WFD	29	POLICE	CRU-DPW	CRUISER	2009	02/27/09	\$22,770		\$ 33,272			

FLEET MANAGEMENT BASELINE INFORMATION FOR FISCAL YEARS 2013-2017

Asset ID Number	License Plate No.	Agency Code	Agency Name	Class Code	Class Description	Asset Model Year	In-Service Date	Gross Vehicle Cost	2013	2014	2015	2016	2017
CAR 15	15WFD	29	POLICE	CAR-DPW	CAR	2008	12/18/07	\$23,412				\$ 32,151	
CAR 16	16WFD	29	POLICE	CRU-DPW	CRUISER	2008	08/13/08	\$20,744		\$ 33,682			
CAR 17	17WFD	29	POLICE	9999	CORVETTE (ASSET FORFEITURE)	1981	01/02/81	\$16,901					
CAR 18	18WFD	29	POLICE	PCK-DPW	PICK UP (Used Vehicle)	2002	11/15/10	\$19,000					
CAR 19	19WFD	29	POLICE	CAR-DPW	CAR	2011	09/09/10	\$23,068				\$ 35,733	
CAR 2	2WFD	29	POLICE	CAR-DPW*	CAR	2011	07/19/11	\$21,631					
CAR 20	20WFD	29	POLICE	CRU-DPW	CRUISER	2011	09/09/10	\$23,068				\$ 35,733	
CAR 21	21WFD	29	POLICE	SUV-DPW	SUV	2011	11/08/11	\$28,553					
CAR 22	22WFD	29	POLICE	SUV-DPW	SUV	2005	03/02/05	\$29,288	\$ 37,726				
CAR 23	932PHE	29	POLICE	CAR-DPW*	CAR	2011	12/23/10	\$23,976					
CAR 24	931PHE	29	POLICE	CAR-DPW*	CAR	2011	12/23/10	\$23,976					
CAR 25	583UTH	29	POLICE	CAR-DPW	CAR	2007	09/20/06	\$19,686		\$ 34,692			
CAR 26	584UTH	29	POLICE	CAR-DPW	CAR	2007	09/20/06	\$19,686		\$ 34,692			
CAR 27	873PJK	29	POLICE	CAR-DPW	CAR	2009	01/30/09	\$19,090					\$33,035
CAR 28	402UUX	29	POLICE	CAR-DPW	CAR	2007	09/20/06	\$19,686		\$ 34,692			
CAR 29	935GHH	29	POLICE	CAR-DPW	CAR	2005	01/27/05	\$24,718	\$ 29,423				
CAR 3	3WFD	29	POLICE	CRU-DPW	CRUISER	2011	07/12/11	\$21,631					\$37,631
CAR 30	156MCM	29	POLICE	CAR-DPW	CAR	2010	11/30/09	\$19,701					
CAR 31	539WNX	29	POLICE	CAR-DPW	CAR	2008	11/05/07	\$24,079				\$ 35,733	
CAR 4	4WFD	29	POLICE	CRU-DPW	CRUISER	2011	07/12/11	\$21,631					\$37,631
CAR 5	5WFD	29	POLICE	CRU-DPW	CRUISER	2011	09/09/10	\$23,068				\$ 36,624	
CAR 6	6WFD	29	POLICE	CRU-DPW	CRUISER	2008	10/30/07	\$20,841	\$ 33,272				
CAR 7	7WFD	29	POLICE	CAR-DPW	CAR	2007	12/15/06	\$20,455		\$ 34,692			
CAR 8	8WFD	29	POLICE	CRU-DPW	CRUISER	2011	09/09/10	\$23,068				\$ 36,624	
CAR 9	9WFD	29	POLICE	CRU-DPW	CRUISER	2008	10/30/07	\$20,841	\$ 33,272				
CAR 33	236DJW	29	POLICE	9999	TOYOTA (ASSET FORFEITURE)	1993	02/27/08	\$18,751					
PD1	152WFD	29	POLICE	BOX-DPW	BOX TRAILER	1993	01/02/93	\$3,282	\$ 3,500				
CV1	118WFD	29	POLICE	9999	COMMAND VEH. FORMER AMBULANCE	1998	08/23/98	\$128,999					
A18	45WFD	30	PUBLIC WORKS	9999		2002	04/30/02	\$23,656					
A2	106WFD	30	PUBLIC WORKS	9999		2001	12/06/00	\$22,523					
A8	110WFD	30	PUBLIC WORKS	9999		2003	01/20/03	\$21,759					
H1	N/A	30	PUBLIC WORKS	FRK-DPW	FORK LIFT	1967	01/20/99	\$17,062					
H10	54WFD	30	PUBLIC WORKS	9999		2003	07/19/02	\$21,314					
H11	81WFD	30	PUBLIC WORKS	PCK-DPW	PICK UP	2007	09/27/06	\$29,837					
H12	80WFD	30	PUBLIC WORKS	PCK-DPW	PICK UP	2003	01/02/03	\$36,000					\$45,268
H13	79WFD	30	PUBLIC WORKS	PCK-DPW	PICK UP	2002	11/09/01	\$25,000				\$ 44,166	
H14	129WFD	30	PUBLIC WORKS	MDD-DPW	MEDIUM DUTY DUMP	1999	03/25/99	\$73,731		\$ 78,942			
H147	147WFD	30	PUBLIC WORKS	RLL-DPW	ROLLER TRAILER	1991	06/07/91	\$2,194	\$ 25,133				
H15	121WFD	30	PUBLIC WORKS	MDD-DPW	MEDIUM DUTY DUMP	1999	03/25/99	\$73,731		\$ 78,599			
H16	123WFD	30	PUBLIC WORKS	MDD-DPW	MEDIUM DUTY DUMP	2002	11/09/01	\$90,377					\$111,523
H17	86WFD	30	PUBLIC WORKS	MDD-DPW	MEDIUM DUTY DUMP	2003	04/16/03	\$73,982					
H18	125WFD	30	PUBLIC WORKS	MDD-DPW	MEDIUM DUTY DUMP	2010	06/17/09	\$99,117					
H19	78WFD	30	PUBLIC WORKS	PCK-DPW	PICK UP	1997	12/09/97	\$19,829					
H24	56WFD	30	PUBLIC WORKS	SWP-DPW	SWEEPER	2003	10/18/02	\$118,670					
H25	57WFD	30	PUBLIC WORKS	9999	PELICAN SWEEPER	1994	08/20/94	\$117,071					
H26	62WFD	30	PUBLIC WORKS	9999	TRACTOR-MOWER	1959	01/01/59	N/A					
H27	63WFD	30	PUBLIC WORKS	TRC-DPW	TRACTOR	2008	05/20/08	\$82,026					
H30	87WFD	30	PUBLIC WORKS	5DMP-DPW	5 TON DUMP TRUCK	2000	06/01/00	\$124,378					
H31	88WFD	30	PUBLIC WORKS	5DMP-DPW	5 TON DUMP TRUCK	2000	05/23/00	\$86,950					
H32	95WFD	30	PUBLIC WORKS	5DMP-DPW	5 TON DUMP TRUCK	2000	05/31/00	\$86,950					
H34	94WFD	30	PUBLIC WORKS	5DMP-DPW	5 TON DUMP TRUCK	2004	03/19/04	\$91,116					
H35	92WFD	30	PUBLIC WORKS	5DMP-DPW	5 TON DUMP TRUCK	1990	12/01/89	\$61,468					

FLEET MANAGEMENT BASELINE REPORT FOR FISCAL YEARS 2013-2017

Asset ID Number	License Plate No.	Agency Code	Agency Name	Class Code	Class Description	Asset Model Year	In-Service Date	Gross Vehicle Cost	2013	2014	2015	2016	2017
H36	124WFD	30	PUBLIC WORKS	5DMP-DPW	5 TON DUMP TRUCK	1996	04/01/96	\$47,505				\$ 139,511	
H37	131WFD	30	PUBLIC WORKS	5DMP-DPW	5 TON DUMP TRUCK	1997	12/01/96	\$50,524					\$141,603
H38	130WFD	30	PUBLIC WORKS	5DMP-DPW	5 TON DUMP TRUCK	1996	06/01/96	\$47,505				\$ 139,511	
H39	96WFD	30	PUBLIC WORKS	5DMP-DPW	5 TON DUMP TRUCK	2009	07/23/08	\$115,800					
H40	97WFD	30	PUBLIC WORKS	5DMP-DPW	5 TON DUMP TRUCK	2003	06/13/03	\$91,116					
H42	99WFD	30	PUBLIC WORKS	216SPC-DPW	18-YEAR SPECIALITY UNIT	2010	12/31/09	\$104,350					
H43DT	161WFD	30	PUBLIC WORKS	TRL-DPW	TRAILER	2001	09/14/00	\$26,375					
H43LB	165WFD	30	PUBLIC WORKS	9999	TRAILER	1954	01/02/54	\$1,295					
H43RO	151WFD	30	PUBLIC WORKS	180SPC-DPW	15-YEAR SPECIALITY UNIT	2000	11/10/00	\$14,925			\$ 56,396		
H44	158WFD	30	PUBLIC WORKS	CHP-DPW	CHIPPER	2005	11/18/04	\$24,000					
H45	60WFD	30	PUBLIC WORKS	BKH-DPW	BACKHOE	2009	03/15/10	\$89,180					
H47	58WFD	30	PUBLIC WORKS	LDR-DPW	LOADER	2008	09/09/08	\$39,900					
H-33	135WFD	30	PUBLIC WORKS	GRN-DPW	STUMP GRINDER	2004	11/12/04	\$23,651					
H48	N/A	30	PUBLIC WORKS	RLL-DPW	ROLLER	1991	07/07/91	\$12,950					
H49	N/A	30	PUBLIC WORKS	9999	5-7 TON ROLLER	1970	01/02/70	\$36,000					
H50	101WFD	30	PUBLIC WORKS	VAC-CC	Vac-All Cab and Chassis	2009	04/30/08	\$63,890					
H-50V	N/A	30	PUBLIC WORKS	VAC-Body	Vac-All Body	2004	04/22/04	\$109,898		\$ 123,691			
H51	90WFD	30	PUBLIC WORKS	216SPC-DPW	18-YEAR SPECIALITY UNIT	2010	06/04/10	\$180,550					
H52	162WFD	30	PUBLIC WORKS	CMP-DPW	COMPRESSOR	2004	12/16/04	\$14,420					
H53	66WFD	30	PUBLIC WORKS	PLW-DPW	SIDEWALK SNOW PLOW	2003	12/12/03	\$81,897					
H54	67WFD	30	PUBLIC WORKS	PLW-DPW	SIDEWALK SNOW PLOW	1986	11/01/86	\$32,977					\$72,692
H55	127WFD	30	PUBLIC WORKS	CHP-DPW	CHIPPER	1998	04/21/98	\$17,045					
H56	N/A	30	PUBLIC WORKS	9999	PAVER	1977	01/01/77	\$2,770					
H58	146WFD	30	PUBLIC WORKS	CMP-DPW	COMPRESSOR	2008	10/14/08	\$16,985					
H6	108WFD	30	PUBLIC WORKS	PCK-DPW	PICK UP	2005	01/24/05	\$30,175					
H60	64WFD	30	PUBLIC WORKS	9999	GRADER	1970	01/02/70	\$34,775					
H64	61WFD	30	PUBLIC WORKS	LDR-DPW	LOADER	1997	01/10/97	\$110,238					\$157,682
H65	59WFD	30	PUBLIC WORKS	LDR-DPW2	LOADER	2004	09/07/04	\$115,135					
H77	168WFD	30	PUBLIC WORKS	BOX-DPW	BOX TRAILER	1993	06/14/93	\$3,282	\$ 5,500				
H9	77WFD	30	PUBLIC WORKS	180SPC-DPW	15-YEAR SPECIALITY UNIT	2011	11/24/10	\$76,014					
H43T	164WFD	30	PUBLIC WORKS	UTL-F	UTILITY UNIT (FIRE)	2006	10/05/05	\$37,398					
S19	70WFD	30	PUBLIC WORKS	9999	SIDE LOAD PACKER	2001	04/09/01	\$141,824					
S20	71WFD	30	PUBLIC WORKS	SLP-DPW	SIDE LOAD PACKER	2007	01/16/07	\$194,875			\$ 240,425		
S21	93WFD	30	PUBLIC WORKS	SLP-DPW	SIDE LOAD PACKER	2007	01/16/07	\$191,749			\$ 240,425		
S22	72WFD	30	PUBLIC WORKS	SLP-DPW	SIDE LOAD PACKER	2011	03/15/11	\$218,336					\$255,067
S23	73WFD	30	PUBLIC WORKS	RLP-DPW	REAR LOAD PACKER	2009	09/17/08	\$131,557					\$133,413
S24	74WFD	30	PUBLIC WORKS	RLP-DPW	REAR LOAD PACKER	2009	09/19/08	\$131,557					\$133,413
S25	75WFD	30	PUBLIC WORKS	RCY-DPW	RECYCLING TRUCK	2011	03/03/11	\$218,336					
S26	76WFD	30	PUBLIC WORKS	RCY-DPW	RECYCLING TRUCK	2011	03/03/11	\$218,336					
S27	68WFD	30	PUBLIC WORKS	RCY-DPW	RECYCLING TRUCK	2007	08/13/07	\$153,944					\$193,816
S28	N/A	30	PUBLIC WORKS	9999	CRAWLER/DOZER	1993	10/14/93	\$114,650					
S29	69WFD	30	PUBLIC WORKS	FLP-DPW	FRONT LOAD PACKER	2001	05/27/01	\$149,165				\$ 131,392	
S30	65WFD	30	PUBLIC WORKS	B&E-DPW	BULLDOZER/EXCAVATOR	2001	12/28/01	\$159,000					\$264,703
H78	171WFD	30	PUBLIC WORKS	9999	TRAILER	1974	01/01/74	Homemade					
UT2	163WFD	30	PUBLIC WORKS	TRL-DPW	TRAILER	2009	07/27/09	\$11,857					
A20	104WFD	30	PUBLIC WORKS	9999	SUBURBAN	1992	07/01/92	\$26,946					
A7	107WFD	PZ/BH		9999	CROWN VICTORIA	2003	07/01/02	\$22,153					
A9	111WFD	PZ/BH		9999	CROWN VICTORIA	2003	01/20/03	\$21,759					
A10	112WFD	PZ/BH		SUV-DPW	SUV	2011	11/19/10	\$18,433					
A3	103WFD	PZ/BH		9999	CROWN VICTORIA	2012	10/13/11	\$26,928					
A4	128WFD	PZ/BH		9999	CROWN VICTORIA	2012	10/13/11	\$26,928					
W-200	590	QH	Quaker Hill Fire Company	9999	AMBULANCE	2004	09/28/04	\$144,000					

FLEET MANAGEMENT BASELINE FOR FISCAL YEARS 2013-2017

Asset ID Number	License Plate No.	Agency Code	Agency Name	Class Code	Class Description	Asset Model Year	In-Service Date	Gross Vehicle Cost	2013	2014	2015	2016	2017
W-21	N/A	QH	Quaker Hill Fire Company	1500PMP-F	1500 GPM PUMPER (FIRE)	2006	10/02/06	\$396,447					
W-23	1025	QH	Quaker Hill Fire Company	BRS-F	BRUSH TRUCK (FIRE)	2006	07/01/06	\$125,245					
W-25	N/A	QH	Quaker Hill Fire Company	1500PMP-F	1500 GPM PUMPER (FIRE)	1993	11/01/93	\$372,000		\$ 699,700			
W-26	1285	QH	Quaker Hill Fire Company	UTL-F	UTILITY UNIT (FIRE)	2006	02/01/06	\$44,076					
P74	N/A	37	REC/PARKS	TRP RAKE	TRAP RAKE	2005	07/11/05	\$11,505				\$ 14,318	
P50	42WFD	37	REC/PARKS	9999	TRACTOR	1980							
P51	119WFD	37	REC/PARKS	TRC-DPW	TRACTOR	2003	09/30/03	\$31,317					
P52	N/A	37	REC/PARKS	9999	TRACTOR	1987							
P53	N/A	37	REC/PARKS	MOW LG	MOWER, LARGE	2006	09/22/06	\$44,168					\$52,427
P54	N/A	37	REC/PARKS	MOW-DPW	MOWER	2003	08/25/03	\$12,919					
P55	N/A	37	REC/PARKS	MOW LG	MOWER, LARGE	2005	05/13/05	\$42,511			\$ 49,421		
P60	N/A	37	REC/PARKS	MOW LG	MOWER, LARGE	2002	07/12/02	\$37,344	\$ 46,300				
P61	43WFD	37	REC/PARKS	MOW2-DPW	TRACTOR/LOADER	1997	02/07/97	\$21,690	\$ 44,559				
P62	N/A	37	REC/PARKS	MOW2-DPW	MOWER	2000	07/19/00	\$19,341					\$ 24,548
P63	N/A	37	REC/PARKS	MOW2-DPW	MOWER	2000	04/24/00	\$19,341			\$ 22,916		
P64	N/A	37	REC/PARKS	SED-DPW	SEEDER	2006	07/12/06	\$10,628					\$15,280
R1	32WFD	37	REC/PARKS	9999	CROWN VICTORIA	2003	01/20/03	\$21,759					
R10	36WFD	37	REC/PARKS	PCK-DPW	PICK UP	2001	03/17/00	\$20,662		\$ 40,620			
R11	37WFD	37	REC/PARKS	PCK-DPW	PICK UP	2004	02/06/04	\$27,037					
R14	38WFD	37	REC/PARKS	MDD-PKS	MEDIUM DUTY DUMP	2009	03/13/09	\$43,799					
R15	100WFD	37	REC/PARKS	PCK-DPW	PICK UP	2008	01/29/08	\$35,358					
R16	44WFD	37	REC/PARKS	PCK-PKS	PICK UP	2008	01/11/08	\$35,358					
R2	33WFD	37	REC/PARKS	SUV-DPW	SUV	2009	05/27/09	\$19,932					\$25,249
R6	41WFD	37	REC/PARKS	9999	VAN	1999	01/02/99	\$27,530					
R8	34WFD	37	REC/PARKS	SUV-DPW	SUV	2004	08/20/04	\$18,000	\$ 38,763				
R9	35WFD	37	REC/PARKS	MDD-DPW	MEDIUM DUTY DUMP	2006	03/08/06	\$35,175					
R30	31934	35	SRCTZ	VAN-DPW	VAN	2008	10/21/08	\$46,912					
R4	26820	35	SRCTZ	VAN-DPW	VAN	2009	09/16/09	\$51,975					
R5	23979	35	SRCTZ	VAN-DPW	VAN	2006	10/31/06	\$43,960			\$ 56,979		
C1	53WFD	31	WUC	SUV-DPW	SUV	2000	01/02/00	\$22,670					
C2	116WFD	31	WUC	VAN-DPW	VAN	211	03/29/11	\$26,617					
C3	51WFD	31	WUC	240SPC-DPW	20-YEAR SPECIALITY UNIT	1991	01/02/91	\$114,000					
T5	46WFD	31	WUC	PCK-DPW	PICK UP	2006	06/20/06	\$20,939					
PG1	144WFD	31	WUC	GEN-DPW	GENERATOR	1982	01/02/82	\$19,125	\$ 25,000				
PG2	179WFD	31	WUC	GEN-DPW	GENERATOR	1991	01/02/91	\$20,000	\$ 25,000				
PP1	145WFD	31	WUC	PMP-DPW	PUMP	1985	01/02/85	\$21,250	\$ 26,200				
PP2	148WFD	31	WUC	PMP-DPW	PUMP	1990	01/02/90	\$21,250	\$ 26,200				
T1	48WFD	31	WUC	PCK-DPW	PICK UP	2006	06/20/06	\$35,665					
T2	49WFD	31	WUC	PCK-DPW	PICK UP	1996	03/01/96	\$27,420					
T3	82WFD	31	WUC	PCK-DPW	PICK UP	2004	01/31/04	\$28,597					
T4	52WFD	31	WUC	PCK-DPW	PICK UP	2003	10/23/02	\$30,768					\$45,761

**FLEET MANAGEMENT FUNDING ANALYSIS
BASED UPON THE FIRST SELECTMAN'S RECOMMENDED PLAN FOR
FY2013-FY2017**

FUND CASH BALANCE AS OF 12/31/11	\$1,341,131.00
ESTIMATED INCOME THROUGH 06/30/12	\$25,000.00
ESTIMATED GRANTS TO OFFSET POLICE BOAT AND FORD FUSIONS	\$472,158.00
TOTAL FUNDING AVAILABLE THROUGH FYE 06/30/12	\$1,838,289.00
ENCUMBRANCES TO DATE (INCURRED BUT NOT EXPENDED)	\$649,914.27
PRIOR YEARS' VEHICLES/EQUIPMENT NOT YET REPLACED:	\$0.00
FY12 REPLACEMENTS PENDING	\$74,867.00
COMMITMENTS THROUGH 06/30/12:	\$724,781.27
UNCOMMITTED BALANCE FY12:	\$1,113,507.73
LEVEL OF FUNDING FOR FY13:	\$1,095,000.00
ESTIMATED REVENUE FOR FY13	\$75,000.00
AVAILABLE THROUGH 06/30/13:	\$2,283,507.73
TOTAL FY13 REPLACEMENTS AS RECOMMENDED BY THE FIRST SELECTMAN	\$1,085,260.00
UNCOMMITTED BALANCE THROUGH 06/30/13:	\$1,198,247.73
ASSUMED LEVEL OF FUNDING FOR FY14:	\$1,095,000.00
ESTIMATED REVENUE FOR FY14	\$75,000.00
AVAILABLE THROUGH 06/30/14:	\$2,368,247.73
TOTAL FY14 REPLACEMENTS AS RECOMMENDED BY THE PLAN	\$1,913,761.00
UNCOMMITTED BALANCE THROUGH 06/30/14:	\$454,486.73
ASSUMED LEVEL OF FUNDING FOR FY15:	\$1,095,000.00
ESTIMATED REVENUE FOR FY15	\$75,000.00
AVAILABLE THROUGH 06/30/15:	\$1,624,486.73
TOTAL FY15 REPLACEMENTS AS RECOMMENDED BY THE PLAN	\$934,906.00
UNCOMMITTED BALANCE THROUGH 06/30/15:	\$689,580.73
ASSUMED LEVEL OF FUNDING FOR FY16:	\$1,095,000.00
ESTIMATED REVENUE FOR FY16	\$75,000.00
AVAILABLE THROUGH 06/30/16:	\$1,859,580.73
TOTAL FY16 REPLACEMENTS AS RECOMMENDED BY THE PLAN	\$1,895,395.00
UNCOMMITTED BALANCE THROUGH 06/30/16:	(\$35,814.27)
ASSUMED LEVEL OF FUNDING FOR FY17:	\$1,095,000.00
ESTIMATED REVENUE FOR FY17	\$75,000.00
AVAILABLE THROUGH 06/30/17:	\$1,134,185.73
TOTAL FY17 REPLACEMENTS AS RECOMMENDED BY THE PLAN	\$2,573,304.00
UNCOMMITTED BALANCE THROUGH 06/30/17:	(\$1,439,118.27)

Waterford Public Schools
Purchase Order Edit List/Register

Report # 20815

PO #: 4957

PO #	Location Code / Description	Ordered For	Terms	Amount
PO Date	Vendor Code / Description	Reference	Deliver To - Purch. Address	Freight
Posting Date	PO Status - Posting Status	External PO#	Ship Via - FOB	Total
Internal Handling Code / Description				

Submitter ID: poneill

2011/4957	BLDG&GR BUILDING & GROU	maint	No Terms	300.00
11/15/2010	723 WHALING CITY FORD	maint	DfltPA1 - BLDG&GR	0.00
11/10/2010	COMP - Posted			\$300.00

Line #	Item # / Description	Qty	Unit Price	Unit	Amount	Freight	Line Total
	Distribution Account # / Description				Dist. Amount	Dist. Freight	Dist. Total
10	repair veh #26	1.00	300.00	EA	300.00	0.00	\$300.00
	100-08120-430-2600-10-12-200-11-5 EQUIP REP MAINT VEH				300.00	0.00	300.00

Total Posted \$ 300.00

Total Unposted \$ 0.00

1 Purchase Order Listed.

Expense Distribution Summary

Account Number / Description	Amount
100-08120-430-2600-10-12-200-11-5 EQUIP REP MAINT VEH	300.00
Total Expenses:	\$300.00

Waterford Public Schools
Purchase Order Edit List/Register

Report # 20816

PO #: 5556

PO #	Location Code / Description	Ordered For	Terms	Amount
PO Date	Vendor Code / Description	Reference	Deliver To - Purch. Address	Freight
Posting Date	PO Status - Posting Status	External PO#	Ship Via - FOB	Total
Internal Handling Code / Description				

Submitter ID: poneill

2011/5556	BLDG&GR BUILDING & GROU	maint	No Terms	250.00
04/01/2011	723 WHALING CITY FORD	maint	DfltPA1 - BLDG&GR	0.00
04/01/2011	COMP - Posted			\$250.00

Line#	Item #/ Description	Qty	Unit Price/ Unit	Amount	Freight	Line Total
	Distribution Account #/ Description			Dist. Amount	Dist. Freight	Dist. Total
10	replace contact on 2003 Ford F250 100-08240-430-2600-10-12-200-11-5 REPAIR GENERAL	1.00	250.00 EA	250.00	0.00	\$250.00

Total Posted \$ 250.00

Total Unposted \$ 0.00

1 Purchase Order Listed.

Expense Distribution Summary

Account Number / Description	Amount
100-08240-430-2600-10-12-200-11-5 REPAIR GENERAL	250.00
Total Expenses:	\$250.00

Waterford Public Schools
Purchase Order Edit List/Register

Report # 20817

PO #: 5271

PO #	Location Code / Description	Ordered For	Terms	Amount
PO Date	Vendor Code / Description	Reference	Deliver To - Purch. Address	Freight
Posting Date	PO Status - Posting Status	External PO#	Ship Via - FOB	Total
	Internal Handling Code / Description			

Submitter ID: poneill

2011/5271	BLDG&GR BUILDING & GROU maint	No Terms	750.00
01/26/2011	4866 JMS AUTOMOTIVE CENTE veh	DfltPA1 - BLDG&GR	0.00
01/25/2011	COMP - Posted		\$750.00

Line#	Item #/Description	Qty	Unit Price/Unit	Amount	Freight	Line Total
	Distribution Account #/Description			Dist. Amount	Dist. Freight	Dist. Total
10	repair brakes ford F250	750.00	1.00 EA	750.00	0.00	\$750.00
	100-08120-430-2600-10-12-200-11-5 EQUIP REP MAINT VEH			750.00	0.00	750.00

Total Posted \$ 750.00

Total Unposted \$ 0.00

1 Purchase Order Listed.

Expense Distribution Summary

Account Number / Description	Amount
100-08120-430-2600-10-12-200-11-5 EQUIP REP MAINT VEH	750.00
Total Expenses:	\$750.00

Waterford Public Schools
Purchase Order Edit List/Register

Report # 20813

PO #: 5996

PO #	Location Code / Description	Ordered For	Terms	Amount
PO Date	Vendor Code / Description	Reference	Deliver To - Purch. Address	Freight
Posting Date	PO Status - Posting Status	External PO#	Ship Via - FOB	Total
Internal Handling Code / Description				

Submitter ID: poneill

2012/5996	BLDG&GR BUILDING & GROU	maint	No Terms	275.00
10/26/2011	723 WHALING CITY FORD	maint	DfltPA1 - BLDG&GR	0.00
10/25/2011	COMP - Posted			\$275.00

Line #	Item #/ Description	Qty	Unit Price	Unit	Amount	Freight	Line Total
	Distribution Account #/ Description				Dist. Amount	Dist. Freight	Dist. Total
10	dome light on F250 repair	1.00	200.00	EA	200.00	0.00	\$200.00
	100-08120-430-2600-10-12-200-11-5 EQUIP REP MAINT VEH				200.00	0.00	200.00
20	hinge assemblies for Veh #117	1.00	75.00	EA	75.00	0.00	\$75.00
	100-14000-613-2600-10-12-200-11-5 SUPPLIES VEHICLES				75.00	0.00	75.00

Total Posted \$ 275.00

Total Unposted \$ 0.00

1 Purchase Order Listed.

Expense Distribution Summary

Account Number / Description	Amount
100-08120-430-2600-10-12-200-11-5 EQUIP REP MAINT VEH	200.00
100-14000-613-2600-10-12-200-11-5 SUPPLIES VEHICLES	75.00
Total Expenses:	\$275.00

Waterford Public Schools
Purchase Order Edit List/Register

Report # 20812

PO #: 6078

PO #	Location Code / Description	Ordered For	Terms	Amount
PO Date	Vendor Code / Description	Reference	Deliver To - Purch. Address	Freight
Posting Date	PO Status - Posting Status	External PO#	Ship Via - FOB	Total
Internal Handling Code / Description				

Submitter ID: poneill

2012/6078	BLDG&GR BUILDING & GROU	maint	No Terms	440.77
11/09/2011	723 WHALING CITY FORD	maint	DfltPA1 - BLDG&GR	0.00
11/08/2011	COMP - Posted			\$440.77

Line #	Item # / Description	Qty	Unit Price / Unit	Amount	Freight	Line Total
	Distribution Account # / Description			Dist. Amount	Dist. Freight	Dist. Total
10	veh #26 trans service and oil change	1.00	240.77 EA	240.77	0.00	\$240.77
	100-08120-430-2600-10-12-200-11-5 EQUIP REP MAINT VEH			240.77	0.00	240.77
20	repair dip stick tube on veh #30	1.00	200.00 EA	200.00	0.00	\$200.00
	100-08120-430-2600-10-12-200-11-5 EQUIP REP MAINT VEH			200.00	0.00	200.00

Total Posted \$ 440.77

Total Unposted \$ 0.00

1 Purchase Order Listed.

Expense Distribution Summary

Account Number / Description	Amount
100-08120-430-2600-10-12-200-11-5 EQUIP REP MAINT VEH	440.77
Total Expenses:	\$440.77

Waterford Public Schools
Purchase Order Edit List/Register

Report # 20814

PO #: 5746

PO #	Location Code / Description	Ordered For	Terms	Amount
PO Date	Vendor Code / Description	Reference	Deliver To - Purch. Address	Freight
Posting Date	PO Status - Posting Status	External PO#	Ship Via - FOB	Total
	Internal Handling Code / Description			

Submitter ID: poneill

2012/5746	BLDG&GR	BUILDING & GROU	maint	No Terms	250.00
09/23/2011	723	WHALING CITY FORD	maint	DfltPA1 - BLDG&GR	0.00
09/22/2011	OPEN - Posted				\$250.00

Line #	Item # / Description	Qty	Unit Price / Unit	Amount	Freight	Line Total
	Distribution Account # / Description			Dist. Amount	Dist. Freight	Dist. Total
10	repair 2003 Ford F250	1.00	250.00 EA	250.00	0.00	\$250.00
	100-08120-430-2600-10-12-200-11-5 EQUIP REP MAINT VEH			250.00	0.00	250.00

Total Posted \$ 250.00

Total Unposted \$ 0.00

1 Purchase Order Listed.

Expense Distribution Summary

Account Number / Description	Amount
100-08120-430-2600-10-12-200-11-5 EQUIP REP MAINT VEH	250.00
Total Expenses:	\$250.00



Each Shop Individually Owned and Operated®
www.meineke.com

MEINEKE CAR CARE CENTER #178
526 COLMAN STREET
NEW LONDON, CT 06320
(860)443-0322

PAGE 1

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O

Customer ID: 0178103468
Name: WATERFORD PUBLIC SCHOOLS
Address: 15 ROPEFIARY RD
Address 2:
City, State, Zip/Postal Code: WATERFORD, CT, 06385
Home Phone: (860) 444-5855
Work Phone: () -
Other Phone: (860) 625-5534
Tax Exempt #:

Year: 03
Make: FORD
Model: PICKUP F250 SUP
Lic No: 26WFD
VIN: 1FTNF21L73EA71972
Color:
Engine: V8-330ci 5.4L F
Mileage In: 57993
Mileage Out: 57993
Date/Time: 03/10/11 16:30:15
Estimate #: 66195
Invoice #: 56555
Key Tag:
PO Number:
Fleet/Wholesale: N
Email Address:

Service comments:

TECH STATES THE THE RR HUB CAP WAS GLUED TO THE LUG NUTS WHEN THE TRUCK CAME IN FOR INSPECTION.

Salesperson: Y. BELLANGER

Qty.	Part #	RFR	Loc	Description	Parts	Labor	Total
BRAKE SERVICE							
1	BFF		*	BRAKE BLEED FLUSH & FI OVERALL	29.95 89.90	59.95	89.90
1	ATD824C	S1	* F	AT BRAKE PAD	137.48	69.95	207.43
1	*ATD824C	S1	F	Discount On ATD824C	(68.74)	0.00	(68.74)
1	FRC10960		* RF	CALIPER	122.09	37.47	159.56
1	FRC10959		* LF	CALIPER	122.09	37.47	159.56
				TOTAL FRONT BRAKES:	457.81		
1	ATD757M	RB	* B	AT BRAKE PAD	113.36	69.95	183.31
1	*ATD757M	RB	B	Discount On ATD757M	(56.68)	0.00	(56.68)
2	SSB		* B	CALIPER SLIDE SEVICE	19.95	0.00	39.90
2	66823		* B	DISC BRAKE ROTOR	195.06	22.00	434.12
				TOTAL REAR BRAKES:	600.65		
				TOTAL BRAKE SERVICE:	1,148.36		
SHOCKS&STRUTS							
2	*911195		F	SHOCK FRONT	159.42	37.47	393.78
2	*911196		B	SHOCK REAR,	150.63	37.47	376.20
				TOTAL SHOCKS&STRUTS:	769.98		
MISCELLANEOUS							
1	SSS		*	STEERING STABILZER	115.77	37.47	153.24
				TOTAL MISCELLANEOUS:	153.24		

*** Customer Wishes To Discard Old Parts ***

RB Separating from backing

S1 Worn close to, but above, minimum specification

PAY HOUSE	AMOUNT 2,021.02	EST \$ APPROVED PLEASE REINSPECT BRAKES EVERY 7,500-10,000 MILES TO AVOID COSTLY DAMAGE THAT IS BEYOND OUR CONTROLL!!!!!!!!!!!!!!	SHOP SUPPLY SUB TOTAL DISCOUNT SALES TAX GRAND TOTAL 2,096.58 (75.56) 0.00 2,021.02
TECH: 901789-0.00	B. THIBAULT		

666598

INVOICE MEINEKE CAR CARE CENTER #178 INVOICE

X

RECEIVED BY
Form #46268-L3MKEY

WARRANTY INFORMATION ON BACK OF RECEIPT.

Service fax:(860)437-4451

service@mywhalingcity.com

Parts Tel:(860)443-8929

parts@mywhalingcity.com

Body Shop fax:(860)701-6075

michael.canova@mywhalingcity.com



LINCOLN



MERCURY



475 Broad Street (at Colman)

New London, CT 06320

Telephone (860) 443-8361

www.mywhalingcity.com

Cashier: 001642 09:50-2

Date Out: 10/26/2010

Mon-Fri: 8-6 Thurs: 8-8 Sat: 8:30-1

Fri: 8:30- Sat: 8:30-1

RO: 230340

Cashier: 001642 09:50-2

Date Out: 10/26/2010

Mon-Fri: 8-6 Thurs: 8-8 Sat: 8:30-1

Fri: 8:30- Sat: 8:30-1

OPEN

TAG: IN:0731OUT:0943

VIN:1FTNF21L73EA71972

2003 FORD F-250

Est. Mileage: 54902

Delivered: 07/09/2003

In Service: 07/09/2003

Customer: 715 Stock #:3588L

WATERFORD SCHOOLS

15 ROPE FERRY RD

WATERFORD CT 06385

Home: 860-444-5857

Work: 000-625-5534

Cellular: 000-444-5870

Advisor: 001642-LAURA PHILLIPS

Hat: WTFD

Date In: 10/26/2010

OP	Acct	Tech	Hours	Complaint/Cause/Correction	Per Unit	Extended Price
[CUSTOMER PAY]						
A	FOCS	004751	04AXLE		Labor Total:	133.50
				CUSTOMER STATES: SOMETHING KNOCKING UNDERNEATH OVER BUMPY		
				ROADS, SOUNDS LIKE FROM LEFT FRONT?		
				DIAG FOUND FRONT SWAY BAR BUSHING AND LINKS WORN . REPLACED		
				BOTH FRONT LINKS AND BUSHINGS.		
	Parts:	1	1C3Z5K483BA	LINK	78.32	78.32
		1	1C3Z5K483BB	LINK	72.50	72.50
		2	2C3Z5484AA	BUSH	18.47	36.94
				Total Parts:	187.76	
				Operation Total:	321.26	

Customer Pay Labor: 133.50
Customer Pay Parts: 187.76

Customer Pay Subtotal: 321.26
Customer Pay Sales Tax: 19.28

Customer Total Due: 340.54

To our valued customer. You may be receiving a survey based on your experience at Whaling City Motors. Your satisfaction is very important to us. If for any reason you feel that you cannot give us a PERFECT SCORE, please speak to management.

OUR GOAL IS:

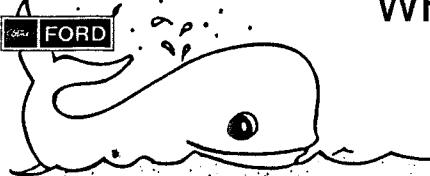
*****COMPLETE SATISFACTION*****

THANK YOU FOR YOUR BUSINESS. WE APPRECIATE YOUR PATRONAGE AND WELCOME YOUR QUESTIONS AND SUGGESTIONS.

SIGNATURE _____

Signature not on file

WHALING CITY FORD, Inc.



475 Broad St. (At Colman) New London, CT 06320

We Treat You Well
Every Department
Every Time

Fax No. (860) 443-0597 Tel. (860) 443-8361
www.whalingcityford.com
e-mail: service@whalingcityford.com

CUSTOMER NO.	ADVISOR	TAG NO.		INVOICE DATE	INVOICE NO.
		LABOR RATE	LICENSE NO.		
715	JEFFREY D. JANSSEN 5932	1972		03/21/07	FOCS146478
WATERFORD PUBLIC SCHOOLS 15 ROPE FERRY RD WATERFORD, CT 06385		29,507		OXFORD WHIT	3588L
	YEAR / MAKE / MODEL			DELIVERY DATE	DELIVERY MILES
	03/FORD TRUCK/F-250/4X4 STYLESIDE			07/09/03	137
	VEHICLE I.D. NO.			SELLING DEALER NO.	PRODUCTION DATE
	1 F T N F 2 1 L 7 3 E A 7 1 9 7 2				
	F.T.E. NO.		P.O. NO.	R. O. DATE	
	0000000			03/21/07	REPRINT# 1

RESIDENCE PHONE 860-444-5857 BUSINESS PHONE 625-5534

COMMENTS
E# 99L

JOB# 1 CHARGES

LABOR # 1 03FOZ STEERING/SUSPENSTON HOURS 2.10 TECH(S) 04087 WARRANTY
CUSTOMER STATES THE TOWN GARAGE SAID EXCESSIVE PLAY IN BALL JOINTS
PERFROM INSPECTION FOUND
UPPER AND LOWER BALL JOINTS WITH EXCESSIVE PLAY
REPLACED UPPER AND LOWER BALL JOINTS LEFT AND RIGHT SIDE,
REMOVE AND REPLACE SIDE STEPS TO LIFT,ROAD TEST.

PARTS	QTY	FP NUMBER	DESCRIPTION	UNIT PRICE	WARRANTY
2	5C3Z-3050-AB	JOINT A 084110			WARRANTY
2	4C3Z-3049-DB	JOINT A 297703			0.00

TOTAL - PARTS

1 TOTALS

JOB# 1 JOURNAL PREFIX	FOCS	JOB# 1 TOTAL	0.00
-----------------------	------	--------------	------

JOB# 2 CHARGES

LABOR # 2 35FOZ001 RECALL HOURS 0.20 TECH(S) 04087 WARRANTY
PREFORM RECALL AS OUTLINED.
05S28
INSPECT AND INSTALL JUMPER HARNESS AS PER RECALL 05S28.

PARTS	QTY	FP NUMBER	DESCRIPTION	UNIT PRICE	WARRANTY
1	4W1Z-14A411-BC	WIRE AS 226006			WARRANTY

TOTAL - PARTS

JOB# 2 TOTALS

JOB# 2 JOURNAL PREFIX	FOCS	JOB# 2 TOTAL	0.00
-----------------------	------	--------------	------

JOB# 3 CHARGES

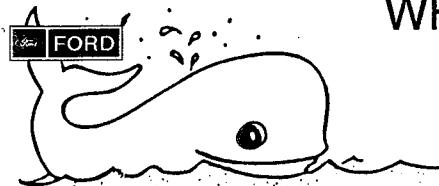
LABOR # 3 49FOZ99P REPORT CARD HOURS 0.00 TECH(S) 54722 WARRANTY
REPORT CARD COMPLETED

JOB# 3 JOURNAL PREFIX	FOCS	JOB# 3 TOTAL	0.00
-----------------------	------	--------------	------

Thank you
for your business.
We appreciate your
patronage and
welcome your
questions, comments
and suggestions.

Front End Alignment
You do not
do not
or
Replace
TIE RODS
do not
need to
26 Wharf St.

WHALING CITY FORD, Inc.



475 Broad St. (At Colman) New London, CT 06320

We Treat You Well

Every Department

Every Time

Fax No. (860) 443-0597 Tel. (860) 443-8361

www.whalingcityford.com

e-mail: service@whalingcityford.com

<p>CUSTOMER NO. 715 WATERFORD PUBLIC SCHOOLS 15 ROPE FERRY RD WATERFORD, CT 06385</p>	ADVISOR JEFFREY D. JANSSEN		TAG NO. 5932 1972	INVOICE DATE 03/21/07	INVOICE NO. FOCS146478
	LABOR RATE	LICENSE NO.	MILEAGE 29,507	COLOR OXFORD WHIT	STOCK NO. 3588L
	YEAR / MAKE / MODEL 03/FORD TRUCK/F-250/4X4 STYLESIDE			DELIVERY DATE 07/09/03	DELIVERY MILES 137
	VEHICLE I.D. NO. 1 F T N F 2 1 L 7 3 E A 7 1 9 7 2			SELLING DEALER NO.	PRODUCTION DATE
	F.T.E. NO. 0000000		P.O. NO.	R.O. DATE 03/21/07	REPRINT# 1
	RESIDENCE PHONE 860-444-5857	BUSINESS PHONE 625-5534	COMMENTS E# 99L		

TOTALS.....

YOU CAN NOW MAKE A SERVICE APOINTMENT AT OUR WEBSITE AT:
www.whalingcityford.com

PAID BY: CASH CHECK VISA MASTERCARD WCF ACCOUNT

INVOICED BY RE JJ DL TM VD JM OTHER
DELIVERED BY RE JJ DL TM VD JM OTHER

To our valued customer. You may be receiving a survey based
on your experience at Whaling City Ford. Your satisfaction is
very important to us. If for any reason you feel that you
can not give us a PERFECT SCORE please speak to management.
Our goal is:
***** COMPLETE SATISFACTION *****

TOTAL LABOR.... 0.00
TOTAL PARTS.... 0.00
TOTAL SUBLET... 0.00
TOTAL G.O.G.... 0.00
TOTAL MISC CHG. 0.00
TOTAL MISC DISC 0.00
TOTAL TAX..... 0.00

TOTAL INVOICE \$ 0.00

Thank you
for your business.
We appreciate your
patronage and
welcome your
questions, comments
and suggestions.

CUSTOMER SIGNATURE



GENUINE PARTS & SERVICE



Diesel Multi-Point Inspection Report Card As Recommended by Ford Motor Company

CUSTOMER NAME: Waterford Public Schools
RO/TAG: 1972 MILEAGE: 29,507

YEAR/MODEL: 03 F-350DATE: 6/21/07

(80# 146478)

CHECKED AND OKAY AT THIS TIME

MAY REQUIRE FUTURE ATTENTION

REQUIRES IMMEDIATE ATTENTION

Check Fluid Levels and Fill

OK	ADD
<input checked="" type="checkbox"/>	<input type="checkbox"/> Engine Oil
<input checked="" type="checkbox"/>	<input type="checkbox"/> Window washer
<input checked="" type="checkbox"/>	<input type="checkbox"/> Transmission fluid level and color
<input checked="" type="checkbox"/>	<input type="checkbox"/> Brake fluid level and color
<input checked="" type="checkbox"/>	<input type="checkbox"/> Power steering fluid level
<input checked="" type="checkbox"/>	<input type="checkbox"/> Coolant recovery reservoir fluid level
<input checked="" type="checkbox"/>	<input type="checkbox"/> 4 x 4 transfer case, front drive axle, clutch reservoir fluid

Comments _____

Check Following Systems / Components

<input type="checkbox"/>	Operation of horn, interior lights, exterior lamps, turn signals, hazard and brake lights
<input type="checkbox"/>	<input checked="" type="checkbox"/> Windshield washer spray, wiper operation and wiper blades
<input checked="" type="checkbox"/>	Windshield for cracks, chips and pitting
<input type="checkbox"/>	Radiator, heater, and air-conditioning hoses for leaks and damage
<input type="checkbox"/>	Air filter restriction gauge and inspect air cleaner housing for damage
<input type="checkbox"/>	"Water in fuel" dash light for illumination
<input checked="" type="checkbox"/>	Accessory drive belt(s)
<input checked="" type="checkbox"/>	Battery terminals (clean, if necessary)
<input type="checkbox"/>	Clutch operation (if equipped)
<input type="checkbox"/>	Oil and/or fuel leaks including turbocharger
<input type="checkbox"/>	Exhaust system (leaks, damage, loose parts)
<input type="checkbox"/>	Drive shaft transmission, u-joint and shift linkage (if equipped) and lubricate (as needed)
<input type="checkbox"/>	Steering and steering linkages
<input type="checkbox"/>	Suspension (shocks for bounce/leaks/damage)
<input type="checkbox"/>	Brake system (including lines, hoses, and parking brake) and wheel end for end-play and bearing noise
<input type="checkbox"/>	Engine cooling, charge air cooling piping, and air intake system (hoses, tubes, and clamps)

State Inspection Due (If Applicable) _____

MO / DAY / YEAR

Comments Waterford Public Schools

Left front tire slice in side, Right front tire
Crack in tire, Top left brake bulb out on
Service spreader, Moisture in Right Reverse
Light housing, Wiper blades,

This Courtesy Inspection Completed by Your Dealership Team!

Service Advisor:

Techician: Carl Clark 03/21/07

D2110205 REV 02/06 ©2004, Ford Motor Company, All Rights Reserved

Customer Signature _____

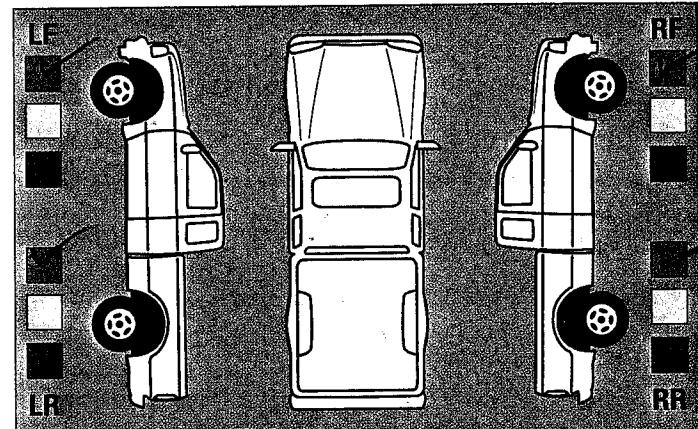
Customer Copy

Tire Pressure Set to
Factory RecommendationsFRONT 55 REAR 70 PS

Check Battery Performance

BATTERY #1		BATTERY #2	
GOOD	FACTORY SPEC COLD CRANKING AMPS	<input type="checkbox"/>	<input type="checkbox"/>
RECHARGE		<input type="checkbox"/>	<input type="checkbox"/>
BAD	ACTUAL COLD CRANKING AMPS	<input type="checkbox"/>	<input type="checkbox"/>

CHECK BRAKES: Measure Front/Rear Brake Linings



Over 5 mm or 7/32" (Disc) or Over 2 mm or 3/32" (Drum)

3 to 5 mm or 4/32" to 7/32" (Disc) or 1.01 to 2 mm or 2/32" to 3/32" (Drum)

Less than 3 mm or 4/32" (Disc) or 1 mm or 2/32" or less (Drum)

Brake Measurement Not Taken This Service Visit

Comments _____

Check Tires

LF	TREAD DEPTH			RF
<input checked="" type="checkbox"/> 7/32 or Greater	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/> 7/32 or Greater
<input type="checkbox"/> 4/32 to 6/32	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/> 4/32 to 6/32
<input type="checkbox"/> 3/32 or less	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/> 3/32 or less
DUAL WHEELS	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	DUAL WHEELS
<input checked="" type="checkbox"/> 7/32 or Greater	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/> 7/32 or Greater
<input type="checkbox"/> 4/32 to 6/32	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/> 4/32 to 6/32
<input type="checkbox"/> 3/32 or less	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/> 3/32 or less
LOR LIR	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	RIR ROR



WEAR PATTERN / DAMAGE

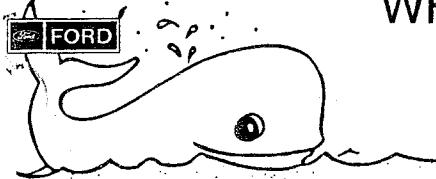
Tire Wear Indicators

Alignment Check Needed Wheel Balance Needed

Comments _____

Tire Pressure Set to
Factory RecommendationsFRONT 55 REAR 70 PS

70- 3166898



WHALING CITY FORD, Inc.

We Treat You Well
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Every Time

475 Broad St. (At Colman) New London, CT 06320

Fax No. (860) 443-0597 Tel. (860) 443-8361
www.whalingcityford.com
e-mail: service@whalingcityford.com

1. Introduction

20073664

CUSTOMER NO. 715 WATERFORD PUBLIC SCHOOLS 15 ROPE FERRY RD WATERFORD, CT 06385	ADVISOR JEFFREY D. JANSSEN	TAG NO. 5932 1972	INVOICE DATE 03/15/07	INVOICE NO. FOCS146341	
	LABOR RATE	LICENSE NO.	MILEAGE	COLOR OXFORD WHIT	STOCK NO. 3588L
	YEAR / MAKE / MODEL 03/FORD TRUCK/F-250/4X4 STYLESIDE		DELIVERY DATE 07/09/03	DELIVERY MILES 137	
	VEHICLE I.D. NO. 1 F T N F 2 1 L 7 3 E A 7 1 9 7 2		SELLING DEALER NO.	PRODUCTION DATE	
	F.T.E. NO. 0000000	P.O. NO.	R. O. DATE 03/15/07	REPRINT# 1	
	RESIDENCE PHONE 860-444-5857	BUSINESS PHONE 625-5534	COMMENTS E# 99L		

JOB# 1 CHARGES

LABOR

STEERING SUSPENSION HORN 20 TECHS 77
CUSTOMER STATES HAS A KLUNKING NOISE IN FRT DRIVING

OVER BUMPS

SOUNDS LIKE THE PASSENGER SIDE
NOISE DIAGNOSIS COMPLETED.

REPLACED TWO FRONT STABILIZER BAR BUSHINGS, RETEST, ROADTEST

PARTS-----QTY-----FP. NUMBER-----DESCRIPTION-----UNIT PRICE-----
2 2C3Z-5484-AA BUSH 789888 14.32

JOB# 1 TOTALS

LABOR 100.80

JOB# 2 CHARGES

LABOR

REPORT CARD
REPORT CARD COMPLETED

REPORT CARD COMPLETED

JOB# 2 TOTALS

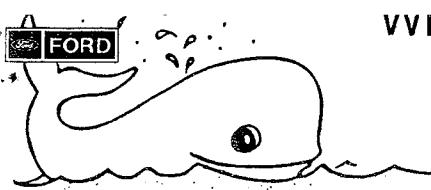
JOB# 2 JOURNAL PREFIX FOCS JOB# 2 TOTAL

0.00

MISC-----CODE-----DESCRIPTION-----CONTROL NO-----
JOB # A ENVSERV ENVIRONMENTAL COMPLIANCE TOTAL MISC

ESTIMATE -
CUSTOMER HEREBY ACKNOWLEDGES RECEIVING
ORIGINAL ESTIMATE OF \$0.00 (+TAX)

Thank you
for your business.
We appreciate your
patronage and
welcome your
questions, comments
and suggestions.



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Every Time

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e-mail: service@whalingcityford.com

CUSTOMER NO.	ADVISOR	TAG NO.	INVOICE DATE	INVOICE NO.
715	JEFFREY D. JANSSEN	5932 1972	03/15/07	FOCS146341
WATERFORD PUBLIC SCHOOLS 15 ROPE FERRY RD WATERFORD, CT 06385		29,394	OXFORD WHIT	3588L
	YEAR / MAKE / MODEL		DELIVERY DATE	DELIVERY MILES
	03/FORD TRUCK/F-250/4X4 STYLESIDE		07/09/03	137
	VEHICLE I.D. NO.		SELLING DEALER NO.	PRODUCTION DATE
	1 F T N F 2 1 L 7 3 E A 7 1 9 7 2			
	F.T.E.NO.	P.O. NO.	R.O. DATE	
	0000000		03/15/07	REPRINT# 1
RESIDENCE PHONE 860-444-5857	BUSINESS PHONE 625-5534	COMMENTS E# 99L		

TOTALS-----

YOU CAN NOW MAKE A SERVICE APOINTMENT AT OUR WEBSITE AT:
www.whalingcityford.com

PAID BY: CASH CHECK VISA MASTERCARD WCF ACCOUNT

INVOICED BY RE JJ DL TM VD JM OTHER
DELIVERED BY RE JJ DL TM VD JM OTHER

TOTAL LABOR....	100.80
TOTAL PARTS....	28.64
TOTAL SUBLET...	0.00
TOTAL G.O.G....	0.00
TOTAL MISC CHG.	2.27
TOTAL MISC DISC	0.00
TOTAL TAX.....	0.00

TOTAL INVOICE \$ 131.71

To our valued customer. You may be receiving a survey based
on your experience at Whaling City Ford. Your satisfaction is
very important to us. If for any reason you feel that you
can not give us a PERFECT SCORE please speak to management.

Our goal is:

***** COMPLETE SATISFACTION *****

Thank you
for your business.
We appreciate your
patronage and
welcome your
questions, comments
and suggestions.

CUSTOMER SIGNATURE

JMS AUTOMOTIVE CENTER LLC

(860) 444-2180

JMS AUTOMOTIVE CENTER LLC

360 ROPE FERRY ROAD
WATERFORD, CONNECTICUT 06385

(860) 444-2180 03/07/07 INVOICE 7577

TOWN OF WATERFORD BOE
15 ROPE FERRY RD
WATERFORD CT 06385

MAKE: FORD MODEL: F250 YEAR: 03
MILES: 29170 LICENSE: A71972 BY: LSW

-- SHOP LABOR DESCRIPTION -- TECH -- AMOUNT --
COMPLETE LOF SERVICES JSQ 15.00

CHANGE ENGINE OIL AND FILTER--CHECK
ALL LIGHTS, FLUID LEVELS, TIRE
PRESSURES, GREASE VEHICLE WHERE
APPLICABLE INCLUDING GENERAL CHECK
OVER OF VEHICLE.

ALL WORK IS
WARANTEED
FOR A PERIOD
OF NINETY DAYS

-- PARTS --

QTY	PART NUMBER	NAME OF PART	PRICE
1	PF1250	OIL FILTER	9.39
7	5W20	OIL PER QT	18.55
2	S/M	ENVIR./SHOP MAT. CHARGE	2.00

Any warranties on parts and accessories sold hereby are made by the manufacturer. The above shop disclaims all warranties, including implied warranties of merchantability or fitness for the particular purpose, and does not authorize any person to assume for it any liability.

WE APPRECIATE YOUR BUSINESS AND THANK YOU FOR
FOR SEEING US TODAY

X

LABOR TOTAL	15.00
OTHER CHARGES	.00
PARTS TOTAL	29.94
SALES TAX	.00
PD IN ADVANCE	.00
CHARGED	44.94



Line-X of Southern New England



66 COLMAN ST. NEW LONDON, CT 06320 • (860) 437-8588 • 437-8534 Fax

INVOICE • WORK ORDER

No. 2788

Important Notice & Authorization

The Line-X liner is a permanent accessory. It is specifically stated that the product cannot be removed from the vehicle once sprayed on. The Line-X application process requires sanding and masking of the truck bed. Purchaser warrants the paint on his vehicle against damage due to application or removal of tape and Line-X hereby disclaims liability for damage caused. I hereby authorize the work described on work order to be done on my vehicle. You and your employees may operate my vehicle for the purpose of product fitting, cleaning, inspection, collection, and delivery. Line-X will not be held responsible for loss or damage to the vehicle or articles left in the vehicle in case of fire, theft, accident or any other cause beyond our control. I have read and acknowledge receipt of the estimate for this work. An express mechanics lien is acknowledged on vehicle to secure the amount of work performed.

OWNER'S INITIALS**Damage Release Waiver**

Line-X will not be held liable for damage to vehicles resulting from the Line-X application process. In addition, and without limiting the foregoing, purchaser expressly acknowledges the existence of the pre-existing damage to vehicle described and releases Line-X from any liability or loss in connection with such damage including and exacerbation of the described damage.

OWNER'S INITIALSDate 9/21/06 Contact *John W. Morris*Name *John Morris SNTA*Address *1512 Main St. New London, CT*City *New London* State/Zip *06320*Phone (H) *444-5855* (W) *207-525-5534*

Make _____ Model _____

Year _____ License No. *2005*

VIN No. _____

Color _____ Miles *2000*Payment MasterCard Visa Discover Cash Check (No) *18* DL No. *1822* P.O. No. *R/O Stock No. X-126***Damage Description** _____**Description of Work** _____*8 foot utility body over top* *750.00**8 foot Ford pick up* *499.00*Color Liner *BLACK* Lip Under Edge Over

Bug Shield # _____ Tailgate Cap # _____

Bed Rails # _____ Other _____

Materials _____

Labor _____

Other Charges/Accessories *Discount* *-50.00*

Allowances _____

Sub Total *1201.00*Tax *Town tax* *1201.00*TOTAL *1201.00*

Salesperson _____ Work Performed By _____

Quality Control Inspection: (INSPECTORS INITIALS REQUIRED AT 6 INSPECTION CATEGORIES)

(#1 thru #3 Electronic Thickness Verification • 7 point check • two points each side wall, three points on truck bed)

1. LEFT SIDE • 80+ MILL: _____ 2. RIGHT SIDE • 80+ MILL: _____ 3. BED 125+ MILL: _____

4. SURFACE TEXTURE: _____ 5. EDGE UNIFORMITY: _____ 6. OVERSPRAY REMOVED: _____

Customer: I have inspected my vehicle & the LINE-X bedliner and everything is in satisfactory condition...

Customer Signature: *John Morris* Date: _____

Refrain from placing heavy objects on your new bedliner for the first 24 hours. If possible, leave tailgate down for first 24 hours.

Keep Invoice/Work Order as proof of purchase. Warranty is void without proof of purchase.

GUY'S OIL SERVICE STATION, INC.

87 WEST MAIN STREET P.O. BOX 81
NIANTIC, CONNECTICUT 06357

GUY'S OIL SERVICE STATION, INC.

24 HOUR SERVICE

VISA MASTERCARD DISCOVER AMEX MOBIL

(860) 739-8700 10/25/04 INVOICE 27588

TOWN OF WATERFORD #470

15 ROPE FERRY ROAD

WATERFORD

CT 06385

TOM

MAKE: FORD

MODEL: F-250

YEAR: 02

MILES: 16742

LICENSE: D33919

BY: JPS

--SHOP LABOR DESCRIPTION-- MECH --- AMOUNT --

***COMPLETE FRONT BRAKE SERVICE: CBM 130.00

EXAMINE CALIPERS, SLIDES, HARDWARE
AND BEARING, REMOVE AND INSTALL NEW
FRONT BRAKE PADS, ALSO CHECKED THE
REAR BRAKE WHICH ARE IN GOOD
CONDITION AT THIS TIME.

GUY'S OIL SERVICE
WARRANTIES ALL PARTS
AND LABOR FOR 90 DAYS

THANK YOU

WE APPRECIATE YOUR
BUSINESS

!!!! THANK YOU !!!!

!!!! HAVE A WONDERFUL DAY !!!!

:) :) :) :) :)

PARTS

QTY	PART NUMBER	NAME OF PART	PRICE
2	SHOP	SHOP MATERIALS	2.00
1	ZX756	FRONT BRAKE PADS	60.06
1	PARTS CLEANER	ALL PURPOSE CLEANER	5.19
2	60-020-1	WIPER BLADE	26.76

Any warranties on parts and accessories sold hereby are made by the manufacturer. The above shop disclaims all warranties, including implied warranties of merchantability or fitness for the particular purpose, and does not authorize any person to assume for it any liability.

I HAVE AUTHORIZED THE ABOVE REPAIR WORK TO BE
PERFORMED ON THIS VEHICLE. I HAVE READ THE COMP-
LETE INVOICE AND UNDERSTAND THE SERVICES PERFORM-
ED. I FURTHER ACKNOWLEDGE AN EXPRESSED MECHANICS'
LIEN AND AGREE TO PAY THE TOTAL AMOUNT LISTED.
SIGNED X

LABOR TOTAL	130.00
OTHER CHARGES	.00
PARTS TOTAL	94.01
SALES TAX	.00
PD IN ADVANCE	.00
CHARGED	224.01

Maintenance Cost Report

Filter: Date from "1/9/2007"; Date to "1/9/2012"; Object ID (from "Q105" to "Q105");
Grouped By: Equipment ID

Dates : 1/9/2007 and 1/9/2012

<u>Equipment ID</u>		<u>Mileage</u>	<u>Hour</u>	<u>Labor Costs</u>	<u>Part Costs</u>	<u>Contract Costs</u>	<u>Total Costs</u>
Equipment ID Q105	CAR						
	Total for Equipment ID	36710	0	\$929.66	\$759.66	\$0.00	\$1,689.32
Grand Totals:				\$929.66	\$759.66	\$0.00	\$1,689.32

Maintenance Cost Report

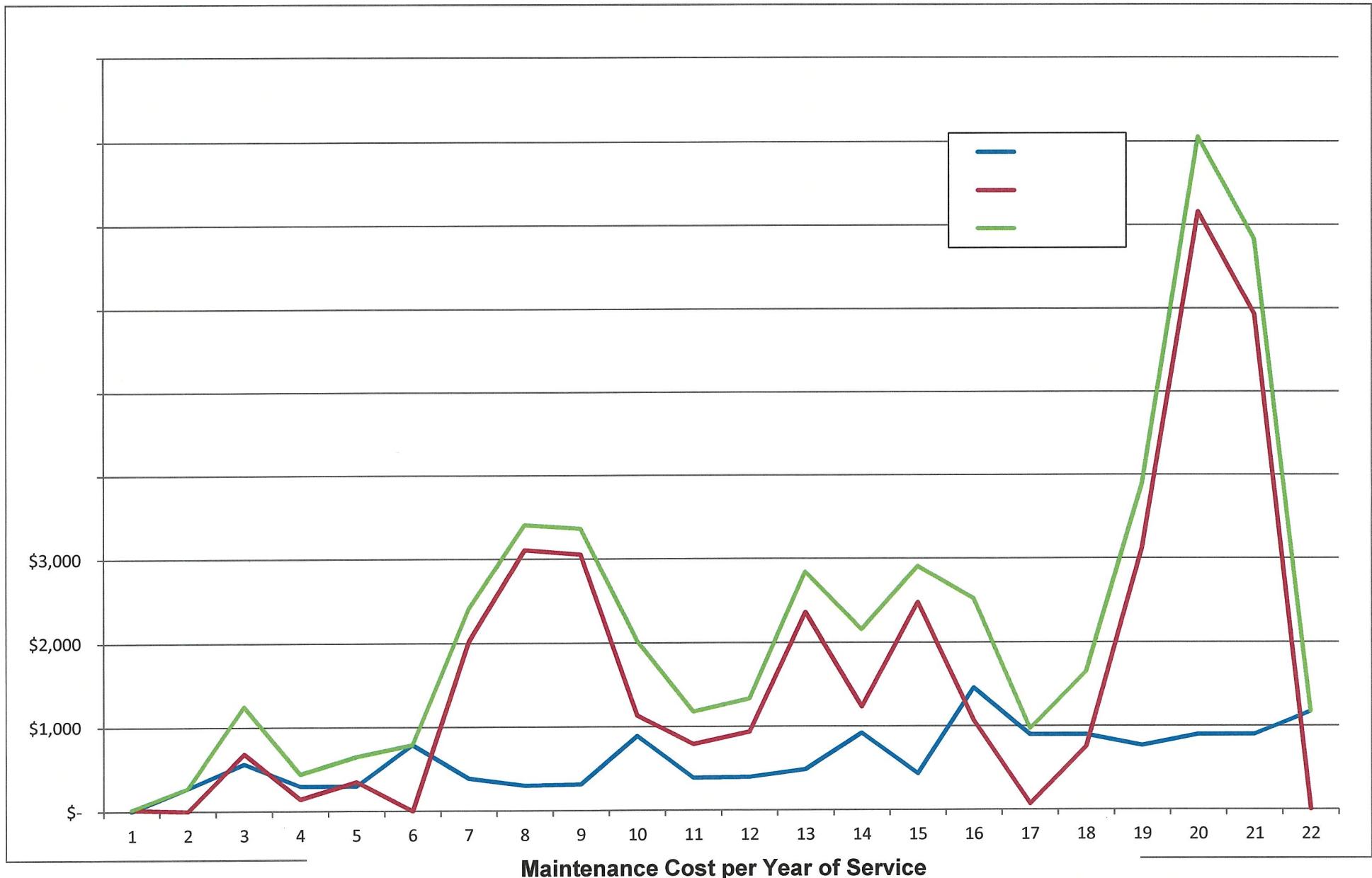
Filter: Date from "1/9/2007"; Date to "1/9/2012"; Object ID (from "Q55" to "Q85");

Grouped By: Equipment ID

Dates : 1/9/2007 and 1/9/2012

<u>Equipment ID</u>		<u>Mileage</u>	<u>Hour</u>	<u>Labor Costs</u>	<u>Part Costs</u>	<u>Contract Costs</u>	<u>Total Costs</u>
Equipment ID Q55	SUV						
	Total for Equipment ID	88958	0	\$3,282.91	\$3,091.84	\$0.00	\$6,374.75
Equipment ID Q85	CAR						
	Total for Equipment ID	32884	0	\$476.53	\$764.02	\$0.00	\$1,240.55
Grand Totals:				\$3,759.44	\$3,855.86	\$0.00	\$7,615.30

W-31 Maintenance Cost History
In-Service Date - March 14, 1991



Maintenance Type	1991	1992	1993	1994	1995	1996	1997	1998	1999	2000	2001	2002
Planned	\$ -	\$ 275	\$ 565	\$ 300	\$ 385	\$ 788	\$ 312	\$ 889	\$ 387	\$ 387	\$ 397	
Corrective	\$ 20	\$ -	\$ 685	\$ 144	\$ 349	\$ -	\$ 2,018	\$ 3,109	\$ 3,054	\$ 1,133	\$ 791	\$ 936
Total	\$ 20	\$ 275	\$ 565	\$ 300	\$ 385	\$ 788	\$ 312	\$ 889	\$ 387	\$ 387	\$ 397	

Maintenance Type	1991	1992	1993	1994	1995	1996	1997	1998	1999	2000	2001	2002
Planned	\$ -	\$ 275	\$ 565	\$ 300	\$ 385	\$ 788	\$ 312	\$ 889	\$ 387	\$ 387	\$ 397	
Corrective	\$ 20	\$ -	\$ 685	\$ 144	\$ 349	\$ -	\$ 2,018	\$ 3,109	\$ 3,054	\$ 1,133	\$ 791	\$ 936
Total	\$ 20	\$ 275	\$ 565	\$ 300	\$ 385	\$ 788	\$ 312	\$ 889	\$ 387	\$ 387	\$ 397	

Transaction Report FY10-11

7/1/10 Through 6/30/11

Page 1

Date	Num	Description	Memo	Category	Amount
10/11/10	3472	... National Auto Parts Service, Inc.		App. Maint.:W-94	-52.53
12/13/10	3504	Solar Fabricators, LLC	Inv #800390	App. Maint.:W-94	-386.00
12/13/10	3517	... Shipman's Fire Equipment Co., Inc.	Inv #158791	App. Maint.:W-94	-273.54
1/10/11	3530	National Auto Parts Service, Inc.	Acc #2365	App. Maint.:W-94	-48.70
2/14/11	3546	... Shipman's Fire Equipment Co., Inc.	Inv #161723	App. Maint.:W-94	-795.00
5/9/11	3614	... Courville's Garage, Inc.	Inv #133325 annual insp	App. Maint.:W-94	-1,178.04
6/13/11	3636	Andy's Trim Shop	Inv. #0176	App. Maint.:W-94	-500.00
TOTAL 7/1/10 - 6/30/11					-3,233.81
TOTAL INFLOWS					0.00
TOTAL OUTFLOWS					-3,233.81
NET TOTAL					-3,233.81

Transaction Report FY09/10

7/1/09 Through 6/30/10

Page 1

Date	Num	Description	Memo	Category	Amount
8/10/09	3154	Eastford Fire & Rescue	Inv #7626	App. Maint.:W-94	-4,900.35
8/10/09	3161	S Shipman's Fire Equipment Co., Inc.	Inv#137964	App. Maint.:W-94	-600.40
			Inv #137933	App. Maint.:W-94	-481.00
2/8/10	3288	Rexel CLS	Acc#22765	App. Maint.:W-94	-39.57
4/12/10	3332	Courville's Garage, Inc.	Acc. #1705 Inv #68766	App. Maint.:W-94	-142.75
4/12/10	3346	S Shipman's Fire Equipment Co., Inc.	Inv #146569 Belts &batt	App. Maint.:W-94	-2,108.00
5/10/10	3362	S Shipman's Fire Equipment Co., Inc.	Inv #149478	App. Maint.:W-94	-190.00
6/14/10	3375	Courville's Garage, Inc.	Acc. #1705 Inv #130299	App. Maint.:W-94	-1,752.65
TOTAL 7/1/09 - 6/30/10					-10,214.72
TOTAL INFLOWS					0.00
TOTAL OUTFLOWS					-10,214.72
NET TOTAL					-10,214.72

Maintenance Cost Report

Filter: Date from "1/9/2007"; Date to "1/9/2012"; Object ID (from "CAR22" to "CAR22");

Grouped By: Equipment ID

Dates : 1/9/2007 and 1/9/2012

<u>Equipment ID</u>		<u>Mileage</u>	<u>Hour</u>	<u>Labor Costs</u>	<u>Part Costs</u>	<u>Contract Costs</u>	<u>Total Costs</u>
Equipment ID CAR22	SUV						
	Total for Equipment ID	80000	0	\$3,983.99	\$2,731.01	\$0.00	\$6,715.00
Grand Totals:				\$3,983.99	\$2,731.01	\$0.00	\$6,715.00

Maintenance Cost Report

Filter: Date from "1/9/2007"; Date to "1/9/2012"; Object ID (from "CAR29" to "CAR29");
Grouped By: Equipment ID

Dates : 1/9/2007 and 1/9/2012

<u>Equipment ID</u>		<u>Mileage</u>	<u>Hour</u>	<u>Labor Costs</u>	<u>Part Costs</u>	<u>Contract Costs</u>	<u>Total Costs</u>
Equipment ID CAR29	CHIEF						
	Total for Equipment ID	102353	0	\$1,569.20	\$2,363.16	\$0.00	\$3,932.36
Grand Totals:				\$1,569.20	\$2,363.16	\$0.00	\$3,932.36

Maintenance Cost Report

Filter: Date from "1/9/2007"; Date to "1/9/2012"; Object ID (from "CAR6" to "CAR6");

Grouped By: Equipment ID

Dates : 1/9/2007 and 1/9/2012

<u>Equipment ID</u>		<u>Mileage</u>	<u>Hour</u>	<u>Labor Costs</u>	<u>Part Costs</u>	<u>Contract Costs</u>	<u>Total Costs</u>
Equipment ID CAR6	CRUISER						
	Total for Equipment ID	101234	0	\$4,034.27	\$4,966.83	\$0.00	\$9,001.10
Grand Totals:				\$4,034.27	\$4,966.83	\$0.00	\$9,001.10

Maintenance Cost Report

Filter: Date from "1/9/2007"; Date to "1/9/2012"; Object ID (from "CAR9" to "CAR9");

Grouped By: Equipment ID

Dates : 1/9/2007 and 1/9/2012

<u>Equipment ID</u>		<u>Mileage</u>	<u>Hour</u>	<u>Labor Costs</u>	<u>Part Costs</u>	<u>Contract Costs</u>	<u>Total Costs</u>
Equipment ID CAR9	CRUISER						
	Total for Equipment ID	104779	0	\$2,970.15	\$4,264.51	\$48.00	\$7,282.66
Grand Totals:				\$2,970.15	\$4,264.51	\$48.00	\$7,282.66

Maintenance Cost Report

Filter: Date from "1/9/2007"; Date to "1/9/2012"; Object ID (from "H77" to "H77");

Grouped By: Equipment ID

Dates : 1/9/2007 and 1/9/2012

<u>Equipment ID</u>	<u>Mileage</u>	<u>Hour</u>	<u>Labor Costs</u>	<u>Part Costs</u>	<u>Contract Costs</u>	<u>Total Costs</u>
Equipment ID H77	TRAILER					
	Total for Equipment ID		\$521.13	\$61.22	\$0.00	\$582.35
Grand Totals:			\$521.13	\$61.22	\$0.00	\$582.35

Maintenance Cost Report

Filter: Date from "1/9/2007"; Date to "1/9/2012"; Object ID (from "A3" to "A6");

Grouped By: Equipment ID

Dates : 1/9/2007 and 1/9/2012

<u>Equipment ID</u>		<u>Mileage</u>	<u>Hour</u>	<u>Labor Costs</u>	<u>Part Costs</u>	<u>Contract Costs</u>	<u>Total Costs</u>
Equipment ID A3	CAR						
	Total for Equipment ID	103867	0	\$1,442.41	\$2,478.53	\$0.00	\$3,920.94
Equipment ID A6	CAR						
	Total for Equipment ID	93471	86286	\$1,349.09	\$1,756.02	\$0.00	\$3,105.11
Grand Totals:				\$2,791.50	\$4,234.55	\$0.00	\$7,026.05

Maintenance Cost Report

Filter: Date from "1/9/2007"; Date to "1/9/2012"; Object ID (from "P60" to "P62");

Grouped By: Equipment ID

Dates : 1/9/2007 and 1/9/2012

<u>Equipment ID</u>		<u>Mileage</u>	<u>Hour</u>	<u>Labor Costs</u>	<u>Part Costs</u>	<u>Contract Costs</u>	<u>Total Costs</u>
Equipment ID P60	MOWER						
	Total for Equipment ID	0	2026	\$1,666.20	\$3,316.70	\$0.00	\$4,982.90
Equipment ID P61	LOADER						
	Total for Equipment ID	0	1822	\$2,624.37	\$2,685.92	\$0.00	\$5,310.29
Equipment ID P62							
	Total for Equipment ID	0	1262	\$1,988.44	\$958.36	\$0.00	\$2,946.80
Grand Totals:				\$6,279.01	\$6,960.98	\$0.00	\$13,239.99

Maintenance Cost Report

Filter: Date from "1/9/2007"; Date to "1/9/2012"; Object ID (from "R8" to "R8");

Grouped By: Equipment ID

Dates : 1/9/2007 and 1/9/2012

<u>Equipment ID</u>		<u>Mileage</u>	<u>Hour</u>	<u>Labor Costs</u>	<u>Part Costs</u>	<u>Contract Costs</u>	<u>Total Costs</u>
Equipment ID R8	PICKUP						
	Total for Equipment ID	76234	0	\$2,044.70	\$2,042.83	\$0.00	\$4,087.53
Grand Totals:				\$2,044.70	\$2,042.83	\$0.00	\$4,087.53

**FLEET MANAGEMENT FUNDING ANALYSIS
BASED UPON THE FIRST SELECTMAN'S RECOMMENDED PLAN FOR
FY2013-FY2017**

FUND CASH BALANCE AS OF 12/31/11	\$1,341,131.00
ESTIMATED INCOME THROUGH 06/30/12	\$25,000.00
ESTIMATED GRANTS TO OFFSET POLICE BOAT AND FORD FUSIONS	\$472,158.00
TOTAL FUNDING AVAILABLE THROUGH FYE 06/30/12	\$1,838,289.00
ENCUMBRANCES TO DATE (INCURRED BUT NOT EXPENDED)	\$649,914.27
PRIOR YEARS' VEHICLES/EQUIPMENT NOT YET REPLACED:	\$0.00
FY12 REPLACEMENTS PENDING	\$74,867.00
COMMITMENTS THROUGH 06/30/12:	\$724,781.27
UNCOMMITTED BALANCE FY12:	\$1,113,507.73
LEVEL OF FUNDING FOR FY13:	\$1,095,000.00
ESTIMATED REVENUE FOR FY13	\$75,000.00
AVAILABLE THROUGH 06/30/13:	\$2,283,507.73
TOTAL FY13 REPLACEMENTS AS RECOMMENDED BY THE FIRST SELECTMAN	\$633,931.00
UNCOMMITTED BALANCE THROUGH 06/30/13:	\$1,649,576.73
ASSUMED LEVEL OF FUNDING FOR FY14:	\$1,095,000.00
ESTIMATED REVENUE FOR FY14	\$75,000.00
AVAILABLE THROUGH 06/30/14:	\$2,819,576.73
TOTAL FY14 REPLACEMENTS AS RECOMMENDED BY THE PLAN	\$1,913,761.00
UNCOMMITTED BALANCE THROUGH 06/30/14:	\$905,815.73
ASSUMED LEVEL OF FUNDING FOR FY15:	\$1,095,000.00
ESTIMATED REVENUE FOR FY15	\$75,000.00
AVAILABLE THROUGH 06/30/15:	\$2,075,815.73
TOTAL FY15 REPLACEMENTS AS RECOMMENDED BY THE PLAN	\$934,906.00
UNCOMMITTED BALANCE THROUGH 06/30/15:	\$1,140,909.73
ASSUMED LEVEL OF FUNDING FOR FY16:	\$1,095,000.00
ESTIMATED REVENUE FOR FY16	\$75,000.00
AVAILABLE THROUGH 06/30/16:	\$2,310,909.73
TOTAL FY16 REPLACEMENTS AS RECOMMENDED BY THE PLAN	\$1,895,395.00
UNCOMMITTED BALANCE THROUGH 06/30/16:	\$415,514.73
ASSUMED LEVEL OF FUNDING FOR FY17:	\$1,095,000.00
ESTIMATED REVENUE FOR FY17	\$75,000.00
AVAILABLE THROUGH 06/30/17:	\$1,585,514.73
TOTAL FY17 REPLACEMENTS AS RECOMMENDED BY THE PLAN	\$2,573,304.00
UNCOMMITTED BALANCE THROUGH 06/30/17:	(\$987,789.27)

TOWN OF WATERFORD
STATEMENT OF REVENUES, EXPENDITURES,
AND CHANGES IN FUND BALANCE
FLEET MANAGEMENT FUND
AS OF DECEMBER 31, 2011

Revenues:

Investment Income	1,417
Vehicle Rentals	15,379
Sale of Vehicles	19,805
Insurance Settlement	2,838
Total Revenues	<u>39,438</u>

Expenditures:

Equipment Replacement	453,878
Vehicle Replacement	767,863
Total Expenditures	<u>1,221,741</u>
Excess (Deficiency) of Revenues Over Expenditures	<u>(1,182,303)</u>

Other Financing Sources (Uses):

Transfers from other funds	1,095,000
Total Other Financing Sources (Uses)	<u>1,095,000</u>

Net Change in Fund Balances	(87,303)
Fund Balances - Beginning	1,428,435
Fund Balances - Ending	<u>1,341,131</u>

FLEET MANAGEMENT PLAN FOR FY13 CAPITAL PLAN - PRIOR TO BOARD OF SELECTMEN ACTION

Asset ID Number	License Plate No.	Agency Code	Agency Name	Class Code	Class Description	Asset Model Year	In-Service Date	Gross Vehicle Cost	Manufacturer	Model	Serial #	Mileage as of 10/15/11	19	9	12	20	20
													2013	2014	2015	2016	2017
													\$ 1.05	\$ 1.63	\$ 0.90	\$ 1.92	\$ 2.57
A4	114WFD		Assessor	9999	Do Not Replace	2003	06/23/03	\$ 22,153	FORD	CROWN VICTORIA	2FAHP71W83X209380	97,559					
BOE1	23WFD	60	Board of Ed	PCK-F	PICK UP AND JEEP (FIRE)	2011	08/15/11	\$ 38,465	Ford	F250	1FDBF2B64BEC82407	1,266					
BOE2	24WFD	60	Board of Ed	VAN-DPW	VAN	2005	04/21/05	\$ 29,056	FORD	ECONOLINE	1FDW35L75HA78392	24,266					\$ 56,979
BOE3	25WFD	60	Board of Ed	PCK-F	PICK UP AND JEEP (FIRE)	2001	11/01/00	\$ 25,817	Ford	F350	1FDW36X1EA36569	96,737					
BOE4	26WFD	60	Board of Ed	PCK-F	PICK UP AND JEEP (FIRE)	2003	06/01/03	\$ 23,697	Ford	F250	1FTNF2L73EA71972	62,100	\$ 39,730				
BOE5	27WFD	60	Board of Ed	VAN-DPW	VAN	2011	01/20/11	\$ 18,425	Ford	E250 Van	1FTNE2EWXBD4A5393	6,185					
BOE6	28WFD	60	Board of Ed	PCK-F	PICK UP AND JEEP (FIRE)	2008	10/26/07	\$ 31,800	Ford	F250	1FDFN21508EB26217	31,466					
BOE7	29WFD	60	Board of Ed	PCK-F	PICK UP AND JEEP (FIRE)	2005	01/10/05	\$ 25,679	FORD	F350	1FTWX31545EB72091	43,772					\$ 41,828
BOE8	30WFD	60	Board of Ed	MDD-DPW	MEDIUM DUTY DUMP	2000	02/01/00	\$ 25,336	Ford	Dump Truck	1FDWF3653YEC20505	57,700					\$ 69,217
BOE9	117WFD	60	Board of Ed	PCK-F	PICK UP AND JEEP (FIRE)	2005	01/11/05	\$ 24,913	FORD	F250	1FDFN21595EB830178	59,727					\$ 41,828
BOE10	N/A	60	Board of Ed	SKD STR	SKID STEER LOADER	2007	05/11/07	\$ 22,000	New Holland	L170 Skid Star Bobcat	N7M455552	303.6/Hrs.					\$ 29,064
BOE11	N/A	60	Board of Ed	UTL-DPW	UTILITY CART	2002	09/20/02	\$ 13,649	Kawasaki	MLE 4x4 Diesel	KAF590A3	2,859	\$ 18,900				
W-500	1360	COH	Cohanzie Fire Company	9999	Do Not Replace	2009	05/20/09	\$ 192,168	Ford	Ambulance	1FDAF56R595A45556	19,355					
W-52 aka W-51	1028	COH	Cohanzie Fire Company	1500PMP-F	1500 GPM PUMPER (FIRE)	1996	07/12/96	\$ 280,055	Pierce	Dash Pumper	4P1CT02E4TA000232	46,873					\$ 485,979
W-53	817	COH	Cohanzie Fire Company	BRS-F	BRUSH TRUCK (FIRE)	2009	05/13/09	\$ 95,854	Ford	Super Duty F-550	1FDAW5R729EA35364	3,002					
W-55	2655	COH	Cohanzie Fire Company	T&A-F	TOWER LADDER/AERIAL (FIF)	2004	12/29/04	\$ 667,929	SUTPHEN	Ladder Truck	1S9A7LLD442003078	16,078					
W-56	42WFD	COH	Cohanzie Fire Company	UTL-F	UTILITY UNIT (FIRE)	2011	10/12/11	\$ 47,426	Ford	F350	1FT8X3BT6BEA81633	3,136					
W-57	595	COH	Cohanzie Fire Company	SPC-F	SPECIALITY UNIT (FIRE)	2010	10/05/10	\$ 51,983	Ferrara Igniter Custom	Rescue Truck	1F9505328AH140755	16,078					
Car 105 (Q105)	105WFD	23/FM	FIRE MARSHAL	CAR-F2	CAR (FIRE)	2005	07/15/05	\$ 19,886	FORD	CROWN VICTORIA	2FAHP71W15X106188	36,521	\$ 33,190				
Car 115 (Q115)	115WFD	23/FM	FIRE MARSHAL	CAR-F2	CAR (FIRE)	2006	01/17/06	\$ 20,696	FORD	CROWN VICTORIA	2FAHP71W86X122289	31,570					\$ 33,682
Car 55 (Q55)	55WFD	23/FM	FIRE MARSHAL	CAR-F	CAR (FIRE)	2005	09/15/04	\$ 26,464	FORD	EXPEDITION	1FMPU16565LA13705	88,888	\$ 33,353				
Car 85 (Q85)	85 WFD	23/FM	FIRE MARSHAL	CAR-F	CAR (FIRE)	2005	10/25/04	\$ 22,747	FORD	CROWN VICTORIA	2FAFP74W55X114846	33,546	\$ 33,353				
A1	1WFD	1	FIRST SELECTMAN	CAR-DPW	CAR	2010	10/06/10	\$ 19,658	FORD	Fusion	3FAHPOJGXBR129270	13,583					\$ 24,177
Avon	N/A	GOS	Goshen Fire Company	9999	Do Not Replace	1991				Avon	AVB10669J091	10 hours					
Boat Trailer	1957	GOS	Goshen Fire Company	TRL-F	TRAILER (FIRE)	2008	11/02/07	\$ 4,900	EZ Loader	Boat Trailer	1ZEAAMXG58A008657	N/A					
Utility Trailer	N/A	GOS	Goshen Fire Company	TRL-F	TRAILER (FIRE)	1999	11/01/89		Parker	Utility Trailer	13Z5A1014X1000005	N/A					
W-300	1340	GOS	Goshen Fire Company	9999	Do Not Replace	2002	09/27/02	\$ 140,000	Ford	Ambulance	1FDXE45F92HA21112	33,959					
W-31	1303	GOS	Goshen Fire Company	1500PMP-F	1500 GPM PUMPER (FIRE)	1990	03/12/91	\$ 250,150	Simon	Duplex Pumper	1D91P11E1L1008202	41,493					
W-32	371	GOS	Goshen Fire Company	1500PMP-F	1500 GPM PUMPER (FIRE)	1998	02/23/99	\$ 307,935	Spartan	1,500 GPM Pumper	4S7AT419XWC026143	15,298					\$ 524,519
W-33	2654	GOS	Goshen Fire Company	UTL-F	UTILITY UNIT (FIRE)	On Order		\$ 58,114	FORD	F450							
W-36	1494	GOS	Goshen Fire Company	UTL-F	UTILITY UNIT (FIRE)	2007	08/29/06	\$ 36,973	FORD	F350 PICK-UP	1FTWW31P17EA67953	19,963					
W-93	N/A	GOS	Goshen Fire Company	BOT-F	BOAT (FIRE)	2007	06/29/07	\$ 103,334	Lake Assault	24' Landing Craft	LKGKF936707	371 Hours					
W-100	600	JOR	Jordan Fire Company	9999	Do Not Replace	2004	09/28/04	\$ 144,000	Ford	E450 Ambulance	1FDXE45P9484B29712	1,901					
W-11	1451	JOR	Jordan Fire Company	1500PMP-F	1500 GPM PUMPER (FIRE)	1993	01/02/94	\$ 245,093	Simon	Duplex - 1,500 GPM Pumper	1D91P16E13P3008643	53,436					
W-15	1454	JOR	Jordan Fire Company	T&A-F	TOWER LADDER/AERIAL (FIF)	1996	01/01/96	\$ 499,354	Pierce	Lance Ladder Truck	4P1CT02G4TA000233	35,177					\$ 964,795
W-16	1484	JOR	Jordan Fire Company	UTL-F	UTILITY UNIT (FIRE)	2005	04/22/05	\$ 43,594	Ford	F350	1FTWW31P15EC55708	45,322					\$ 50,552
W-400	589	OSW	Oswegatchie Fire Company	9999	Do Not Replace	2008	01/08/08	\$ 192,378	Ford	F-550 4x4 Ambulance	1FDAF57R18EC1919	31,597					\$ 524,519
W-41	1103	OSW	Oswegatchie Fire Company	1500PMP-F	1500 GPM PUMPER (FIRE)	1998	02/16/99	\$ 279,030	Spartan	1,500 GPM Pumper	4S7AT2294WC026142	21,581					
W-42	1349	OSW	Oswegatchie Fire Company	1500PMP-F	1500 GPM PUMPER (FIRE)	2007	05/14/08	\$ 416,972	Seagrave	1,500 Marauder Pumper	1F9E528T97CS12165	10,393					
W-43	1026	OSW	Oswegatchie Fire Company	BRS-F	BRUSH TRUCK (FIRE)	2006	06/06/06	\$ 125,245	Ford	F550 Brush Truck	1FDAX57P66ED51762	5,327					
W-46	1348	OSW	Oswegatchie Fire Company	UTL-F	UTILITY UNIT (FIRE)	2006	01/23/06	\$ 44,606	Ford	F350	1FDWW31P96EB60911	32,568					
W-94	1347	OSW	Oswegatchie Fire Company	SPC-F	SPECIALITY UNIT (FIRE)	1988	09/15/89	\$ 275,093	Simon	Duplex	1D91P11J3J1008891	26,176	\$ 512,000				
CAR 1	377MHN	29	POLICE	CAR-DPW	CAR	2009	10/20/08	\$ 22,991	FORD	CROWN VICTORIA	2FAHP71V49X113715	62,203					\$ 32,151
CAR 10	10WFD	29	POLICE	CRU-DPW	CRUISER	2011	09/09/10	\$ 23,068	FORD	CROWN VICTORIA	2FABP7BV8BX101156	30,410					\$ 35,733
CAR 11	11WFD	29	POLICE	CRU-DPW	CRUISER	2011	07/19/11	\$ 21,631	FORD	CROWN VICTORIA	2FABP7BV7BX173224	8,599					\$ 25,076
CAR 12	12WFD	29	POLICE	CRU-DPW	CRUISER	2011	09/09/10	\$ 23,068	FORD	CROWN VICTORIA	2FABP7BV0BX101152	29,100					\$ 35,733
CAR 13	13WFD	29	POLICE	CRU-DPW	CRUISER	2011	07/19/11	\$ 21,631	FORD	CROWN VICTORIA	2FABP7BV9BX173223	4,131					\$ 25,076
CAR 14	14WFD	29	POLICE	CRU-DPW	CRUISER	2009	02/27/09	\$ 22,770	FORD	CROWN VICTORIA	2FAHP71V59X128028	77,516					\$ 33,272
CAR 15	15WFD	29	POLICE	CAR-DPW	CAR	2008	12/18/07	\$ 23,412	FORD	EXPLORER	1FMEU73E68UA17301	18,933					\$ 32,151
CAR 16	16WFD	29	POLICE	CRU-DPW	CRUISER	2008	08/13/08	\$ 20,744	FORD	CROWN VICTORIA	2FAHP71V18X178603	53,034					\$ 33,682
CAR 17	17WFD	29	POLICE	9999	Do Not Replace	1981	01/02/81	\$ 16,901	CHEVROLET	CORVETTE (Drug Forfeiture)	161AY8764B5410774						
CAR 18	18WFD	29	POLICE	9999	Do Not Replace	2008	11/15/10	\$ 19,000	CHEVROLET	SILVERADO 1500 PICK-UP	3GCEK13328G306359	88,292					
CAR 19	19WFD	29	POLICE	CAR-DPW	CAR	2011	09/09/10	\$ 23,068	FORD	CROWN VICTORIA	2FABP7BV2BX101153	19,926					\$ 35,733
CAR 2	2WFD	29	POLICE	CAR-DPW	CAR	2011	07/19/11	\$ 21,631	FORD	CROWN VICTORIA	2FABP7BV1BX173221	2,010					\$ 25,076
CAR 20	20WFD	29	POLICE	CRU-DPW	CRUISER	2011	09/09/10	\$ 23,068	FORD	CROWN VICTORIA	2FABP7BV7BX101150	3,151					\$ 35,733
CAR 21	21WFD	29	POLICE	SUV-DPW	SUV	2011	11/14/11	\$ 28,552	FORD	EXPEDITION	1FMUJ1G55B5E53860	18					

FLEET MANAGEMENT PLAN FOR FY13 CAPITAL PLAN - PRIOR TO BOARD OF SELECTMEN ACTION

Asset ID Number	License Plate No.	Agency Code	Agency Name	Class Code	Class Description	Asset Model Year	In-Service Date	Gross Vehicle Cost	Manufacturer	Model	Serial #	Mileage as of 10/15/11	2013	2014	2015	2016	2017
CAR 22	22WFD	29	POLICE	SUV-DPW	SUV	2005	03/02/05	\$ 29,288	FORD	EXPEDITION XLS	1FMPU16595LA65040	80,418	\$ 37,726				
CAR 23	932PHE	29	POLICE	SUV-DPW	SUV	2003	07/01/02	\$ 22,153	FORD	CROWN VICTORIA	2FAFP71W83X209380	9,915					
CAR 24	931PHE	29	POLICE	CAR-DPW*	CAR	2003	07/01/02	\$ 21,025	FORD	CROWN VICTORIA	2FAFP71W73X105891	7,681					
CAR 25	583UTH	29	POLICE	CAR-DPW*	CAR	2007	09/20/06	\$ 19,686	CHEVROLET	IMPALA	2G1WC58R479148677	74,993		\$ 34,692			
CAR 26	584UTH	29	POLICE	CAR-DPW	CAR	2007	09/20/06	\$ 19,686	CHEVROLET	IMPALA	2G1WC58R379143504	105,802		\$ 34,692			
CAR 27	873PK	29	POLICE	CAR-DPW	CAR	2009	02/11/09	\$ 19,090	FORD	ESCAPE	1FMCU92779KB46666	33,884				\$ 33,035	
CAR 28	402UUX	29	POLICE	CAR-DPW	CAR	2007	09/20/06	\$ 19,686	CHEVROLET	IMPALA	2G1WC58R079139717	64,028		\$ 34,692			
CAR 29	935GHH	29	POLICE	CAR-DPW	CAR	2005	01/27/05	\$ 24,718	FORD	CROWN VICTORIA	2FAFP74W65X129341	104,263	\$ 29,423				
CAR 3	3WFD	29	POLICE	CAR-DPW	CAR	2011	07/13/11	\$ 21,631	FORD	CROWN VICTORIA	2FABP7BV5BX173223	3,985					
CAR 30	156MCM	29	POLICE	CAR-DPW	CAR	2010	11/30/09	\$ 19,701	FORD	TAURUS	1FAHP2DW1AG112586	20,812					
CAR 31	539WNX	29	POLICE	CAR-DPW	CAR	2008	11/05/07	\$ 24,079	FORD	CROWN VICTORIA	2FAFP74V28X122849	64,824		\$ 35,733			
CAR 4	4WFD	29	POLICE	CRU-DPW	CRUISER	2011	07/13/11	\$ 21,631	FORD	CROWN VICTORIA	2FABP7BV3BX173222	3,760		\$ 25,076			
CAR 5	5WFD	29	POLICE	CRU-DPW	CRUISER	2011	09/09/10	\$ 23,068	FORD	CROWN VICTORIA	2FABP7BV6BX101155	29,115					
CAR 6	6WFD	29	POLICE	CRU-DPW	CRUISER	2008	10/30/07	\$ 20,841	FORD	CROWN VICTORIA	2FAHP71V68X118798	100,907	\$ 33,272				
CAR 7	7WFD	29	POLICE	CAR-DPW	CAR	2007	12/15/06	\$ 20,455	FORD	CROWN VICTORIA	2FAHP71W17X100992	63,382		\$ 34,692			
CAR 8	8WFD	29	POLICE	CRU-DPW	CRUISER	2011	09/09/10	\$ 23,068	FORD	CROWN VICTORIA	2FABP7BV4BX101154	39,116		\$ 36,624			
CAR 9	9WFD	29	POLICE	CRU-DPW	CRUISER	2008	10/30/07	\$ 20,841	FORD	CROWN VICTORIA	2FAHP71V48X118797	104,892	\$ 33,272				
CAR 33	236DJW	29	POLICE	9999	Do Not Replace	1993	02/27/08	\$ 18,751	Toyota - Drug Forfeiture	CAMRY	JT2SK12EXP0134941	145,211					
PD1	152WFD	29	POLICE	BOX-DPW	BOX TRAILER	1993	01/02/93	\$ 3,282	CROSS	59 TRAILER	1C9FS0917P1431062	N/A	\$ 3,500				
CV1	118WFD	29	POLICE	9999	Do Not Replace	1998	08/23/98	\$ 128,999	Ford	E450 Command Vehicle	1FDXE40F2WHB92670	50,429					
A18	45WFD	30	PUBLIC WORKS	9999	Do Not Replace	2002	04/30/02	\$ 23,656	FORD	CROWN VICTORIA	2FAFP74W62X159256	94,326					
A2	106WFD	30	PUBLIC WORKS	9999	Do Not Replace	2001	12/06/03	\$ 22,523	JEEP	CHEROKEE	1J4FF485S1L565909	130,067					
A8	110WFD	30	PUBLIC WORKS	9999	Do Not Replace	2003	01/20/03	\$ 21,759	FORD	CROWN VICTORIA	2FAFP71W03X140532	76,851					
H1	N/A	30	PUBLIC WORKS	FRK-DPW	FORK LIFT	1967	01/20/99	\$ 17,062	HYSTER - Purchased Use	S20A Fork Lift	A10D5889L	1,833/Hrs.					
H11	81WFD	30	PUBLIC WORKS	PCK-DPW	PICK UP	2007	09/27/06	\$ 29,837	FORD	F250 4X4	1FTSF21P87EA84225	53,166					
H12	80WFD	30	PUBLIC WORKS	PCK-DPW	PICK UP	2003	01/02/03	\$ 36,000	FORD	F450 4x2	1FDXF46F63EB17892	30,237				\$ 45,268	
H13	79WFD	30	PUBLIC WORKS	PCK-DPW	PICK UP	2002	11/09/01	\$ 25,000	DODGE	RAM 2500	387KF26Z62M228318	126,034			\$ 44,166		
H14	129WFD	30	PUBLIC WORKS	MDD-DPW	MEDIUM DUTY DUMP	1999	03/25/99	\$ 73,731	IH	4X2 - 4,700 GVW DUMP	1HTSAA15XH668941	75,108	\$ 78,942				
H147	147WFD	30	PUBLIC WORKS	RLL-DPW	ROLLER	1991	06/07/91	\$ 2,194	ROSCO	TRAILER	1YB411327M1B1T617	N/A					
H15	121WFD	30	PUBLIC WORKS	MDD-DPW	MEDIUM DUTY DUMP	1999	03/25/99	\$ 73,731	IH	4X2 - 4,700 GVW DUMP	1HTSAA13XH668940	88,969	\$ 78,599				
H16	123WFD	30	PUBLIC WORKS	MDD-DPW	MEDIUM DUTY DUMP	2002	11/09/01	\$ 90,377	IH	4X2 - 4,900 GVW DUMP	1HTSDAA1X2H529376	65,543				\$ 111,523	
H17	83WFD	30	PUBLIC WORKS	MDD-DPW	MEDIUM DUTY DUMP	2003	04/16/03	\$ 73,982	STERLING	L 7500	2FZAARBS03AL76553	51,900					
H18	125WFD	30	PUBLIC WORKS	MDD-DPW	MEDIUM DUTY DUMP	2010	06/17/09	\$ 99,117	IH	4300 SBA Dump	1HTMMMAAL8A188776	11,000					
H19	78WFD	30	PUBLIC WORKS	PCK-DPW	PICK UP	1997	12/09/97	\$ 19,829	FORD	F-250 4X4	1FTHF26H7VEC72176	128,644					
H24	56WFD	30	PUBLIC WORKS	SWP-DPW	SWEEPER	2003	10/18/02	\$ 118,670	ELGIN	PELICAN P SWEEPER	P-3928-S	10,551					
H25	57WFD	30	PUBLIC WORKS	9999	Do Not Replace	1994	08/20/94	\$ 117,071	ELGIN	PELICAN SWEEPER	S-8054-S	16,878					
H26	62WFD	30	PUBLIC WORKS	9999	Do Not Replace	1959	01/01/59	N/A	FERGUSON	T0-35 TRACTOR MOWER	SGM 193199	4,929/Hrs.					
H27	63WFD	30	PUBLIC WORKS	TRC-DPW	TRACTOR	2008	05/20/08	\$ 82,026	JOHN DEERE	MODEL 5603 TRACTOR	LV5603R268632	1,327/Hrs.					
H30	87WFD	30	PUBLIC WORKS	SDMP-DPW	5 TON DUMP TRUCK	2000	06/01/00	\$ 124,378	FREIGHTLINER	5-TON DUMP - FL80	1FV6JBB2YH1B17532	59,672					
H31	88WFD	30	PUBLIC WORKS	SDMP-DPW	5 TON DUMP TRUCK	2000	05/23/00	\$ 86,950	IH	4X2 - 4,900 GVW DUMP	1HTSDAA1R2YH288328	69,570					
H32	95WFD	30	PUBLIC WORKS	SDMP-DPW	5 TON DUMP TRUCK	2000	05/31/00	\$ 86,950	IH	4X2 - 4,900 GVW DUMP	1HTSDAA1R4YH288329	70,951					
H34	94WFD	30	PUBLIC WORKS	SDMP-DPW	5 TON DUMP TRUCK	2004	03/19/04	\$ 91,116	STERLING	L7500	2FZAATDC84AN04719	25,142					
H35	92WFD	30	PUBLIC WORKS	SDMP-DPW	5 TON DUMP TRUCK	1990	12/01/89	\$ 61,468	IH	5-TON DUMP	1HTGBDR3LH255911	133,857					
H36	124WFD	30	PUBLIC WORKS	SDMP-DPW	5 TON DUMP TRUCK	1996	04/01/96	\$ 47,505	IH	4X2 - 4,900 GVW DUMP	1HTSDAA1R2TH409125	56,918			\$ 139,511		
H37	131WFD	30	PUBLIC WORKS	SDMP-DPW	5 TON DUMP TRUCK	1997	12/01/96	\$ 50,524	IH	4X2 - 4,900 GVW DUMP	1HTSDAA1R1VH461302	58,293				\$ 141,603	
H38	130WFD	30	PUBLIC WORKS	SDMP-DPW	5 TON DUMP TRUCK	1996	06/01/96	\$ 47,505	IH	4X2 - 4,900 GVW DUMP	1HTSDAA1R4TH409126	52,179				\$ 139,511	
H39	96WFD	30	PUBLIC WORKS	SDMP-DPW	5 TON DUMP TRUCK	2009	07/23/08	\$ 115,800	INTERNATIONAL	7400 SFA	1HTWDAAR29J07349	11,524					
H40	97WFD	30	PUBLIC WORKS	SDMP-DPW	5 TON DUMP TRUCK	2003	06/13/03	\$ 91,116	STERLING	L 7500	2FZAATAK93AL96265	34,943					
H42	99WFD	30	PUBLIC WORKS	216SPC-DPW	18-YEAR SPECIALITY UNIT	2010	12/31/09	\$ 104,350	INTERNATIONAL	7600 SFA 6x4	1HWSXSTJ8A1J73491	15,892					
H43DT	161WFD	30	PUBLIC WORKS	TRL-DPW	TRAILER	2001	09/14/00	\$ 26,375	FRUEHAUF	DUMP TRAILER	1JU26ZF615737748	N/A					
H43LB	165WFD	30	PUBLIC WORKS	9999	Do Not Replace	1954	01/02/54	\$ 1,295	FRUEHAUF	LOW BED TRAILER	FW96419	N/A					
H43RO	151WFD	30	PUBLIC WORKS	180SPC-DPW	15-YEAR SPECIALITY UNIT	2000	11/10/00	\$ 14,925	BENLEE	TRAILER TA60TC34	1B9A13420YB183504	8,422/Hrs.			\$ 56,396		
H44	158WFD	30	PUBLIC WORKS	CHP-DPW	CHIPPER	2005	11/18/04	\$ 24,000	VERMEER	BC1000XL- CHIPPER	1VRY1119X51005548	837/Hrs.					
H45	60WFD	30	PUBLIC WORKS	BKH-DPW	BACKHOE	2009	03/15/10	\$ 89,180	VOLVO	BL70 BACKHOE/LOADER	VCEOBL70A0020080	794/Hrs.					
H47	58WFD	30	PUBLIC WORKS	LDR-DPW	TRACTOR/MOWER	2008	09/09/08	\$ 39,900	JOHN DEERE	5603 LOADER/TRACTOR	LV5603R270211	771/Hrs.					
H-33	135WFD	30	PUBLIC WORKS	GRN-DPW	STUMP GRINDER	2004	11/12/04	\$ 23,651	CARLTON	7500 - Stump Grinder	1J9G42122C1167474	96/Hrs.					
H48	N/A	30	PUBLIC WORKS	RLL-DPW	ROLLER	1991	07/07/91	\$ 12,950	ROSCO	5-7 TON ROLLER	\$ 34,578	805/Hrs.					
H49	N/A	30	PUBLIC WORKS	9999	Do Not Replace	1970	01/02/70	\$ 36,000	INGRAM	5-7 TON ROLLER	401073F27	1543/Hrs.					
H50	101WFD	30	PUBLIC WORKS	VAC-CC	Vac-All Cab and Chassis	2009	04/30/08	\$ 63,890	IH	7400 SBA 4x2	1HTWCAAR79J077348	3,078					

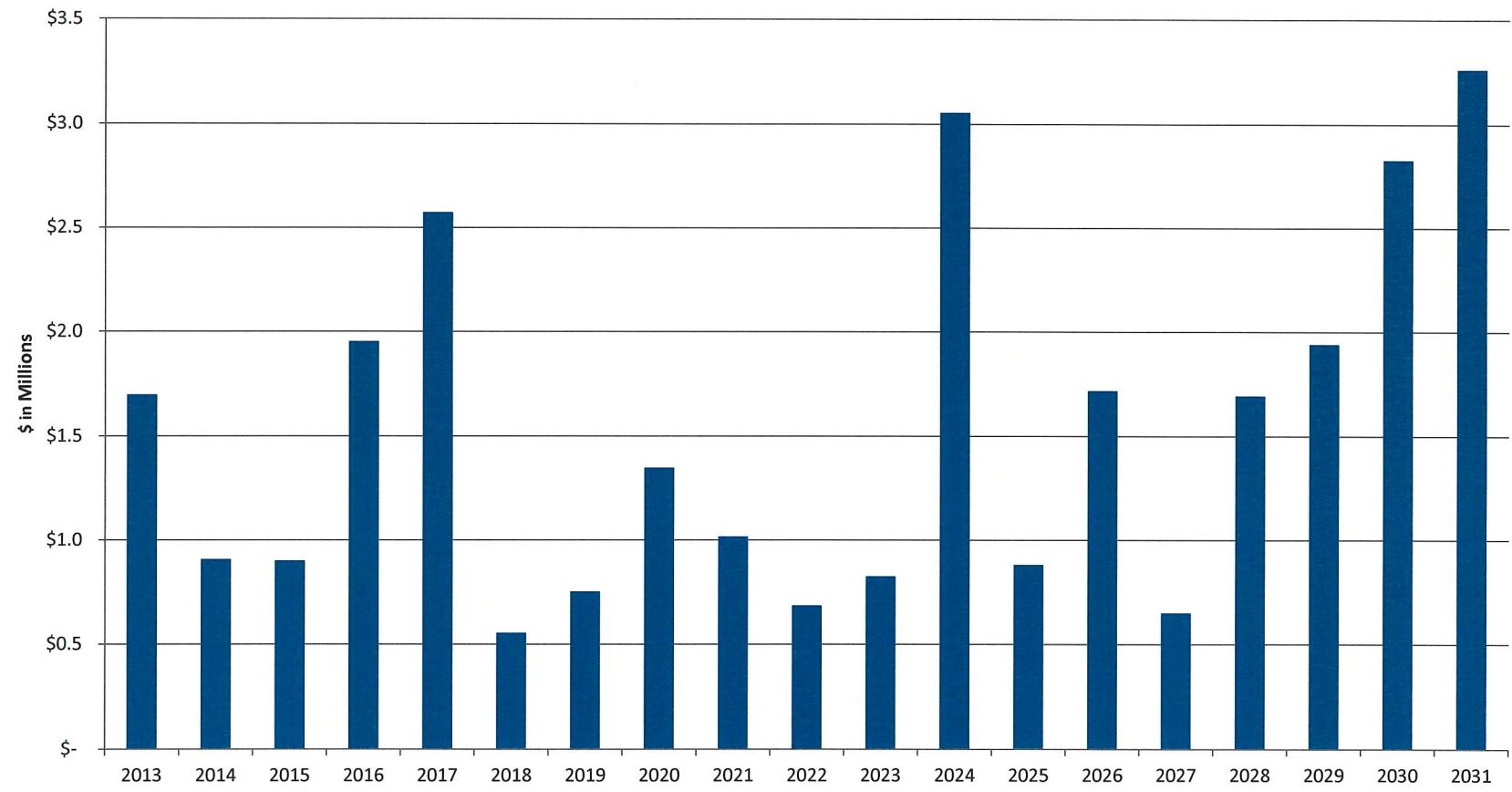
FLEET MANAGEMENT PLAN FOR FY13 CAPITAL PLAN - PRIOR TO BOARD OF SELECTMEN ACTION

Asset ID Number	License Plate No.	Agency Code	Agency Name	Class Code	Class Description	Asset Model Year	In-Service Date	Gross Vehicle Cost	Manufacturer	Model	Serial #	Mileage as of 10/15/11	2013	2014	2015	2016	2017
H-50V	N/A	30	PUBLIC WORKS	VAC-Body	Vac-All Body	2004	04/22/04	\$109,898	Vac-All Body for H-50	LV10C	N/A	1,643/Hrs.	\$ 123,691				
H51	90WFD	30	PUBLIC WORKS	216SPC-DPW	18-YEAR SPECIALITY UNIT	2010	06/04/10	\$180,550	IH	Model AL60 Aerial Lift	1HTMMAAR3AH247768	3,288					
H52	162WFD	30	PUBLIC WORKS	CMP-DPW	COMPRESSOR	2004	12/16/04	\$ 14,420	INGERSOLL RAND	P185WJD COMPRESSOR	350036UJ0221	407/Hrs.					
H53	66WFD	30	PUBLIC WORKS	PLW-DPW	SIDEWALK SNOW PLOW	2003	12/12/03	\$ 81,897	TRACKLESS	MT5TD SIDEWALK PLOW	MT5TD-2330	1,121/Hrs.					
H54	67WFD	30	PUBLIC WORKS	PLW-DPW	SIDEWALK SNOW PLOW	1986	11/01/86	\$ 32,977	BOMBARDIER	SW-48FA	\$ 1,860,984	978/Hrs.					\$ 72,692
H55	127WFD	30	PUBLIC WORKS	CHP-DPW	CHIPPER	1998	04/21/98	\$ 17,045	VERMEER CHIPPER	BC1230	1VRK15152W1001604	860/Hrs.					
H56	N/A	30	PUBLIC WORKS	9999	Do Not Replace	1977	01/01/77	\$ 2,770	LAYTON	D.550B SUPER PAVER	D7621-K-6	No Hour Meter					
H58	146WFD	30	PUBLIC WORKS	CMP-DPW	COMPRESSOR	2008	10/14/08	\$ 16,985	Ingersoll Rand	XP185WJD Port. Compressor	402988UGS222	63/Hrs.					
H6	108WFD	30	PUBLIC WORKS	PCK-DPW	PICK UP	2005	01/24/05	\$ 30,175	FORD	F250 4X4	1FTSF21P45EB72105	101,400					
H60	64WFD	30	PUBLIC WORKS	9999	Do Not Replace	1970	01/02/70	\$ 34,775	CLEVELAND	D-560 GRADER	70-560B-138-3696	1,984/Hrs.					
H64	61WFD	30	PUBLIC WORKS	LDR-DPW	LOADER	1997	01/10/97	\$110,238	VOLVO	L90C LOADER WHEEL	TD63KBE12630	5,931/Hrs.					\$ 58,888
H65	59WFD	30	PUBLIC WORKS	LDR-DPW2	LOADER	2004	09/07/04	\$115,135	VOLVO	L90E LOADER	D6D1AE2969011	3,608/Hrs.					
H77	168WFD	30	PUBLIC WORKS	BOX-DPW	BOX TRAILER	1993	06/14/93	\$ 3,282	CROSS COUNTRY	59 PAPER TRAILER	1C9FS0919P1431063	N/A	\$ 5,500				
H9	77WFD	30	PUBLIC WORKS	180SPC-DPW	15-YEAR SPECIALITY UNIT	2011	11/24/10	\$ 76,014	IH	4300 SBA 4x2 w/Platform Body	1HTMMAAL9BH388342	5,512					
H43T	164WFD	30	PUBLIC WORKS	UTL-F	UTILITY UNIT (FIRE)	2006	10/05/05	\$ 37,398	TOWMASTER	T-70DTG DUMP TRAILER	4KNT22276L160354	N/A					
S19	72WFD	30	PUBLIC WORKS	9999	Do Not Replace	2001	04/09/01	\$141,824	VOLVO	WX64 PACKER	4V2DC6UEX1N321692	123,790					
S20	71WFD	30	PUBLIC WORKS	SLP-DPW	SIDE LOAD PACKER	2007	01/16/07	\$194,875	VOLVO AUTOCAR	WX-64 PACKER	5VCDC6MF67H204567	66,570					\$ 240,425
S21	93WFD	30	PUBLIC WORKS	SLP-DPW	SIDE LOAD PACKER	2007	01/16/07	\$191,749	VOLVO AUTOCAR	WX-64 PACKER	5VCDC6MF57H203734	64,318					\$ 240,425
S22	70WFD	30	PUBLIC WORKS	SLP-DPW	SIDE LOAD PACKER	2011	03/15/11	\$218,336	PETERBUILT	MCNEILUS SIDE LOADER	3BPZL50X9BF129187	6,931					\$ 255,067
S23	73WFD	30	PUBLIC WORKS	RLP-DPW	REAR LOAD PACKER	2009	09/17/08	\$131,557	IH	REFUSE PACKER	1HTWGAAT19J093030	10,741					\$ 133,413
S24	74WFD	30	PUBLIC WORKS	RLP-DPW	REAR LOAD PACKER	2009	09/19/08	\$131,557	IH	REFUSE PACKER	1HTWGAAT39J093031	13,116					\$ 133,413
S25	75WFD	30	PUBLIC WORKS	RCY-DPW	RECYCLING TRUCK	2011	03/03/11	\$218,336	PETERBUILT	MCNEILUS SIDE LOADER	3BPZL50X7BF129186	2,696					
S26	76WFD	30	PUBLIC WORKS	RCY-DPW	RECYCLING TRUCK	2011	03/03/11	\$218,336	PETERBUILT	MCNEILUS SIDE LOADER	3BPZL50X0BF129188	5,005					
S28	N/A	30	PUBLIC WORKS	9999	Do Not Replace	1993	10/14/93	\$114,650	DRESSER	TD15E CRAWLER/DOZER	4450009P030608	12,125/Hrs.					
S29	69WFD	30	PUBLIC WORKS	FLP-DPW	FRONT LOAD PACKER	2001	05/27/01	\$149,165	VOLVO	WX64 FRONT LOADER	4V2DC6UE11N321693	96,222					\$ 131,392
S30	65WFD	30	PUBLIC WORKS	B&E-DPW	BULLDOZER/EXCAVATOR	2001	12/28/01	\$159,000	VOLVO	EW170 EXCAVATOR	\$ 26,289,408	80,048/Hrs.					\$ 264,703
H78	171WFD	30	PUBLIC WORKS	9999	Do Not Replace	1974	01/01/74	Homemade	COX	7214 SMBT	W-23	533/Hrs.					
UT2	163WFD	30	PUBLIC WORKS	TRL-DPW	TRAILER	2009	07/27/09	\$ 11,857	CAM	8CAM824DOTT - Utility Trailer	5JPBU292X9P024572	N/A					
A20	104WFD	30	PUBLIC WORKS	9999	Do Not Replace	1992	07/01/92	\$ 26,946	GMC	Suburban	1GKG26K9N7J21324	120,320					
A7	107WFD	PZ/BH		CAR-DPW	CAR	2012	10/13/11	\$ 26,928	FORD	FUSION HYBRID	3FADPOL3XCR187566	1					
A9	111WFD	PZ/BH		CAR-DPW	CAR	2012	10/13/11	\$ 26,928	FORD	FUSION HYBRID	3FADPOL31CR187567	95					
A10	112WFD	PZ/BH		SUV-DPW	SUV	2011	11/18/10	\$ 18,433	FORD	ESCAPE	1FMCU9C7XBKA75810	3,000					
A3	103WFD	PZ/BH		9999	Do Not Replace	2003	11/30/03	\$ 22,153	FORD	CROWN VICTORIA	2FAHP71W83X209377	106,258					
A6	128WFD	PZ/BH		9999	Do Not Replace	2012	10/13/11	\$ 26,928	FORD	CROWN VICTORIA	2FAHP71W13X209379	98,279					
W-200	590	QH	Quaker Hill Fire Company	9999	Do Not Replace	2004	09/28/04	\$144,000	Ford	Ambulance - E450	1FDXE45PO48H29713	30,813					
W-21	N/A	QH	Quaker Hill Fire Company	1500PMP-F	1500 GPM PUMPER (FIRE)	2006	10/02/06	\$396,447	FERRARA	Igniter Custom Pumper	1F94047266H140373	11,705					
W-23	1025	QH	Quaker Hill Fire Company	BRS-F	BRUSH TRUCK (FIRE)	2006	07/01/06	\$125,245	Ford	F550 Brush Truck	1FDAX57P86ED51763	2,934					
W-25	N/A	QH	Quaker Hill Fire Company	1500PMP-F	1500 GPM PUMPER (FIRE)	1993	11/01/93	\$372,000	Pierce	1,500 GPM Pumper/Aerial	4P1CT02VXPA000631	27,199					\$ 720,690
W-26	1285	QH	Quaker Hill Fire Company	UTL-F	UTILITY UNIT (FIRE)	2006	02/01/06	\$ 44,076	Ford	F350	1FDWW31P76EB60910	21,162					
P74	N/A	37	REC/PARKS	TRP RAKE	TRAP RAKE	2005	07/11/05	\$ 11,505	Toro	Sand Pro 3020	\$ 250,000,494	546/Hrs.					\$ 14,318
P50	42WFD	37	REC/PARKS	9999	Do Not Replace	1980			JOHN DEERE	1050 TRACTOR	1D50S003477	4,308/Hrs.					
P51	119WFD	37	REC/PARKS	TRC-DPW	TRACTOR	2003	09/30/03	\$ 31,317	JOHN DEERE	4710 TRACTOR	CWV1JD4510AS	1,238/Hrs.					
P52	N/A	37	REC/PARKS	9999	Do Not Replace	1987			JOHN DEERE	332 TRACTOR	M003C424601	1,179/Hrs.					
P53	N/A	37	REC/PARKS	MOW LG	MOWER, LARGE	2006	09/22/06	\$ 44,168	TORO	GROUNDMASTER 4100	\$ 260,000,727	1,938/Hrs.					\$ 52,427
P54	N/A	37	REC/PARKS	MOW-DPW	MOWER	2003	08/25/03	\$ 12,919	FERRIS	IS 5000 72" Mower	\$ 682	1,227/Hrs.					
P55	N/A	37	REC/PARKS	MOW LG	MOWER, LARGE	2005	05/13/05	\$ 42,511	TORO	4000D Mower	3841024000562	2,017/Hrs.					\$ 49,421
P60	N/A	37	REC/PARKS	MOW LG	MOWER, LARGE	2002	07/12/02	\$ 37,344	TORO	455D Mower	30455220000247	2,023/Hrs.					\$ 46,300
P61	43WFD	37	REC/PARKS	MOW2-DPW	MOWER	1997	02/07/97	\$ 21,690	KABOTA	L4200 Mower	3022829540	1,866/Hrs.					\$ 44,559
P62	N/A	37	REC/PARKS	MOW2-DPW	MOWER	2000	07/19/00	\$ 19,341	TORO	325D Mower	2000000278	13,351/Hrs.					\$ 24,548
P63	N/A	37	REC/PARKS	MOW2-DPW	MOWER	2000	04/24/00	\$ 19,341	TORO	325D Mower	200000310	13,631/Hrs.					\$ 22,916
P64	N/A	37	REC/PARKS	SED-DPW	SEEDER	2006	07/12/06	\$ 10,628	SEEDA-VATOR	SE-60E	483	504/Hrs.					\$ 15,280
R1	32WFD	37	REC/PARKS	9999	Do Not Replace	2003	01/20/03	\$ 21,759	FORD	CROWN VICTORIA	2FAFP71W03X140529	87,156					
R10	36WFD	37	REC/PARKS	PCK-DPW	PICK UP	2001	03/17/00	\$ 20,662	DODGE	RAM 2500 4X4	3B7KF26Z21M256325	103,198					\$ 40,620
R11	37WFD	37	REC/PARKS	PCK-DPW	PICK UP	2004	02/06/04	\$ 27,037	CHEVROLET	SMALL DUMP	1GBJK34224E259100	59,850					
R14	38WFD	37	REC/PARKS	MDD-PKS	MEDIUM DUTY DUMP	2009	03/13/09	\$ 43,799	FORD	F450 DUMP	1FDAF47R94EA08998	18,929					
R15	100WFD	37	REC/PARKS	PCK-DPW	PICK UP	2008	01/29/08	\$ 35,358	FORD	F250	1FTSF21RX8EC87491	42,652					
R16	44WFD	37	REC/PARKS	PCK-PKS	PICK UP	2008	01/11/08	\$ 35,358	FORD	F250 PICK-UP	1FTSF21R88EC87490	31,884					
R2	33WFD	37	REC/PARKS	SUV-DPW	SUV	2009	05/27/09	\$ 19,932	FORD	ESCAPE - Utility Body	1FMCU93G79KC38309	11,943					\$ 25,249
R6	41WFD	37	REC/PARKS	9999	Do Not Replace	1999	01/02/99	\$ 27,530	DODGE	3,500 VAN	2BSWB35Z3XK581749	73,849					

FLEET MANAGEMENT PLAN FOR FY13 CAPITAL PLAN - PRIOR TO BOARD OF SELECTMEN ACTION

Asset ID Number	License Plate No.	Agency Code	Agency Name	Class Code	Class Description	Asset Model Year	In-Service Date	Gross Vehicle Cost	Manufacturer	Model	Serial #	Mileage as of 10/15/11	2013	2014	2015	2016	2017
R8	34WFD	37	REC/PARKS	SUV-DPW	SUV	2004	08/20/04	\$ 18,000	CHEVROLET	Silverado	1GCEK14TO4Z260345	77,938	\$ 38,763				
R9	35WFD	37	REC/PARKS	MDD-DPW	MEDIUM DUTY DUMP	2006	03/08/06	\$ 35,175	GMC	SIERRA	16DJK34D66E163756	49,092					
R30	31934	35	SRCTZ	VAN-DPW	VAN	2009	10/21/08	\$ 46,912	FORD/SUPREME	CANDIDATE BUS	1FD3E35L38DB23570	43,449					
R4	26820	35	SRCTZ	VAN-DPW	VAN	2009	09/16/09	\$ 51,975	FORD/SUPREME	SENATOR BUS WITH LIFT	1FDEE35S49DA64658	28,275					
R5	23979	35	SRCTZ	VAN-DPW	VAN	2006	10/31/06	\$ 43,960	FORD	E350 CUTAWAY VAN	1FDWE35S16DB03982	75,772					\$ 56,979
C1	53WFD	31	WUC	SUV-DPW	SUV	2000	01/02/00	\$ 22,670	FORD	EXPLORER	1FMZU71X0YUA60411	52,790					
C2	116WFD	31	WUC	SUV-DPW	SUV	2011	03/29/11	\$ 26,617	FORD	EXPEDITION XLT 4x4	1FMJU1651BEF25120	3,215					
C3	51WFD	31	WUC	240SPC-DPW	20-YEAR SPECIALITY UNIT	2012	On Order	\$244,676	INTERNATIONAL	7,500 SBA With Hi-Vac Attach							
T5	46WFD	31	WUC	PCK-DPW	PICK UP	2006	06/20/06	\$ 20,939	FORD	F-150	1FTPX14546NB57564	35,207					
PG1	144WFD	31	WUC	GEN-DPW	GENERATOR	1982	01/02/82	\$ 19,125	ONAN	TRAILER	1013131	N/A	\$ 25,000				
PG2	179WFD	31	WUC	GEN-DPW	GENERATOR	1991	01/02/91	\$ 20,000	ONAN	TRAILER	16MG1067LD017031	N/A	\$ 25,000				
PP1	145WFD	31	WUC	PMP-DPW	PUMP	1985	01/02/85	\$ 21,250	GORMAN RUPP	PUMP TRAILER	774243	N/A	\$ 26,200				
PP2	148WFD	31	WUC	PMP-DPW	PUMP	1990	01/02/90	\$ 21,250	GORMAN RUPP	PUMP TRAILER	968976N	N/A	\$ 26,200				
T1	48WFD	31	WUC	PCK-DPW	PICK UP	2006	06/20/06	\$ 35,665	CHEVROLET	SILVERADO PICK-UP 2500	16BHK24DX6E251727	38,856					
T2	49WFD	31	WUC	PCK-DPW	PICK UP	2011	07/27/11	\$ 55,594	GMC	K-3500 SIERRA 4wd	16D322CL8BF243396	1,700					
T3	82WFD	31	WUC	PCK-DPW	PICK UP	2004	01/31/04	\$ 28,597	CHEVROLET	2500HD PICK-UP	1GBHK24UX4E178792	98,892					
T4	52WFD	31	WUC	PCK-DPW	PICK UP	2003	10/23/02	\$ 30,768	CHEVROLET	2500HD PICK-UP	1GBHK24U63E122220	119,141					\$ 45,761

Waterford Replacement Plan
Summary of Gross Replacement Costs



Asset Class Code	Asset Class Description	Replacement Cycle in Months	Replacement Cycle in Years
1500PMP-F	1500 GPM PUMPER (FIRE)	216	18
180SPC-DPW	15-YEAR SPECIALITY UNIT	180	15
216SPC-DPW	18-YEAR SPECIALITY UNIT	216	18
240SPC-DPW	20-YEAR SPECIALITY UNIT	240	20
VAC-CC	Vac-All Cab and Chassis	240	20
VAC-Body	Vac-All Body	120	10
5DMP-DPW	5 TON DUMP TRUCK	240	20
AMB-F	AMBULANCE (FIRE)	240	20
B&E-DPW	BULLDOZER/EXCAVATOR	180	15
BKH-DPW	BACKHOE	300	25
BOT-F	BOAT (FIRE)	360	30
BOX-DPW	BOX TRAILER	240	20
BRS-F	BRUSH TRUCK (FIRE)	240	20
CAR-DPW	CAR	96	8
CAR-F	CAR (FIRE)	96	8
CAR-F2	CAR (FIRE)	96	8
CHP-DPW	CHIPPER	240	20
CMP-DPW	COMPRESSOR	180	15
CRU-DPW	CRUISER	60	5
FLP-DPW	FRONT LOAD PACKER	180	15
FRK-DPW	FORK LIFT	300	25
GEN-DPW	GENERATOR	180	15
GRD-DPW	GRADER	300	25
GRN-DPW	STUMP GRINDER	180	15
LDR-DPW	LOADER	240	20
LDR-DPW2	LOADER	240	20
MDD-DPW	MEDIUM DUTY DUMP	180	15
MDD-PKS	MEDIUM DUTY DUMP	144	12
MOW LG	MOWER, LARGE	120	10
MOW2-DPW	MOWER	180	15
MOW-DPW	MOWER	180	15
PCK-DPW	PICK UP	168	14
PCK-F	PICK UP AND JEEP (FIRE)	120	10
PCK-PKS	PICK UP	144	12
PLW-DPW	SIDEWALK SNOW PLOW	360	30
PMP-DPW	PUMP	180	15
PVR-DPW	SUPER PAVER	300	25
RCY-DPW	RECYCLING TRUCK	108	9

RLL-DPW	ROLLER	240	20
RLP-DPW	REAR LOAD PACKER	96	8
SED-DPW	SEEDER	120	10
SKD STR	SKID STEER LOADER	120	10
SLP-DPW	SIDE LOAD PACKER	84	7
SPC-F	SPECIALITY UNIT (FIRE)	240	20
SUV-DPW	SUV	96	8
SWP-DPW	SWEeper	216	18
T&A-F	TOWER LADDER/AERIAL (FIRE)	240	20
TRC-DPW	TRACTOR	180	15
TRL-DPW	TRAILER	240	20
TRL-F	TRAILER (FIRE)	240	20
TRP RAKE	TRAP RAKE	120	10
UTL-F	UTILITY UNIT (FIRE)	144	12
VAN-DPW	VAN	120	10
SPC-F1	SPECIALITY UNIT (FIRE)	240	20
UTL-DPW	UTILITY CART	120	10
SPC P1	SPECIALITY UNIT (POLICE)	240	20
9999	Do Not Replace		

RTM APPROVED FLEET MANAGEMENT PLAN FOR FISCAL YEARS 2012-2016

										14	19	10	12	16
										\$ 1.35	\$ 1.05	\$ 1.63	\$ 0.90	\$ 1.80
										\$ 1,353,788	\$ 1,045,241	\$ 1,633,300	\$ 901,224	\$ 1,799,284
Asset ID Number	License Plate No.	Agency Code	Agency Name	Class Code	Class Description	Asset Model Year	In-Service Date	Gross Vehicle Cost	2012	2013	2014	2015	2016	
A6	114WFD	4	ASSESSOR	9999	POLICE CAR	2003	07/01/02	\$21,025						
BOE1	23WFD	60	Board of Ed	PCK-F	PICK UP AND JEEP (FIRE)	2011	07/21/11	\$38,465						
BOE2	24WFD	60	Board of Ed	VAN-DPW	VAN	2005	04/21/05	\$29,056						
BOE3	25WFD	60	Board of Ed	PCK-F	PICK UP AND JEEP (FIRE)	2001	11/01/00	\$25,817						
BOE4	26WFD	60	Board of Ed	PCK-F	PICK UP AND JEEP (FIRE)	2003	06/01/03	\$23,697		\$ 39,730				
BOE5	27WFD	60	Board of Ed	VAN-DPW	VAN	2011	01/20/11	\$18,425						
BOE6	28WFD	60	Board of Ed	PCK-F	PICK UP AND JEEP (FIRE)	2008	10/26/07	\$31,800						
BOE7	29WFD	60	Board of Ed	PCK-F	PICK UP AND JEEP (FIRE)	2005	01/10/05	\$25,679				\$ 41,828		
BOE8	30WFD	60	Board of Ed	MDD-DPW	MEDIUM DUTY DUMP	2000	02/01/00	\$25,336				\$ 69,217		
BOE9	117WFD	60	Board of Ed	PCK-F	PICK UP AND JEEP (FIRE)	2005	01/11/05	\$24,913				\$ 41,828		
BOE10	N/A	60	Board of Ed	SKD STR	SKID STEER LOADER	2007	05/11/07	\$22,000						
BOE11	N/A	60	Board of Ed	UTL-DPW	UTILITY CART	2002	09/20/02	\$13,649		\$ 18,900				
W-500	1360	COH	Cohanzie Fire Company	9999		2009	05/20/09	\$192,168						
W-52 aka W-51	1028	COH	Cohanzie Fire Company	1500PMP-F	1500 GPM PUMPER (FIRE)	1996	07/12/96	\$280,055				\$ 485,979		
W-53	817	COH	Cohanzie Fire Company	BRS-F	BRUSH TRUCK (FIRE)	2009	05/13/09	\$95,854						
W-55	2655	COH	Cohanzie Fire Company	T&A-F	TOWER LADDER/AERIAL (FIRE)	2004	12/29/04	\$667,929						
W-56	42WFD	COH	Cohanzie Fire Company	UTL-F	UTILITY UNIT (FIRE)	2011	10/12/11	\$47,426						
W-57	595	COH	Cohanzie Fire Company	SPC-F	SPECIALITY UNIT (FIRE)	2010	10/05/10	\$511,983						
Car 105 (Q105)	105WFD	23/FM	FIRE MARSHAL	CAR-F2	CAR (FIRE)	2005	07/15/05	\$19,886		\$ 33,190				
Car 115 (Q115)	115WFD	23/FM	FIRE MARSHAL	CAR-F2	CAR (FIRE)	2006	01/17/06	\$20,696			\$ 33,682			
Car 55 (Q55)	55WFD	23/FM	FIRE MARSHAL	CAR-F	CAR (FIRE)	2005	09/15/04	\$26,464		\$ 33,353				
Car 85 (Q85)	85 WFD	23/FM	FIRE MARSHAL	CAR-F	CAR (FIRE)	2005	10/25/04	\$22,747		\$ 33,353				
A1	1WFD	1	FIRST SELECTMAN	CAR-DPW	CAR	2011	10/06/10	\$19,658						
Avon	N/A	GOS	Goshen Fire Company	9999		1991								
Boat Trailer	1957	GOS	Goshen Fire Company	TRL-F	TRAILER (FIRE)	2008	11/02/07	\$4,900						
Utility Trailer	N/A	GOS	Goshen Fire Company	TRL-F	TRAILER (FIRE)	1999	11/01/89							
W-300	1340	GOS	Goshen Fire Company	9999		2002	09/27/02	\$140,000						
W-31	1303	GOS	Goshen Fire Company	1500PMP-F	1500 GPM PUMPER (FIRE)	1990	03/12/91	\$250,150	\$427,543					
W-32	371	GOS	Goshen Fire Company	1500PMP-F	1500 GPM PUMPER (FIRE)	1998	02/23/99	\$307,935						
W-33	2654	GOS	Goshen Fire Company	SPC-F1	SPECIALITY UNIT (FIRE)	1989	08/12/89	\$146,282	\$ 54,000					
W-36	1494	GOS	Goshen Fire Company	UTL-F	UTILITY UNIT (FIRE)	2007	08/29/06	\$36,973						
W-93	N/A	GOS	Goshen Fire Company	BOT-F	BOAT (FIRE)	2007	06/29/07	\$103,334						
W-100	600	JOR	Jordan Fire Company	9999		2004	09/28/04	\$144,000						
W-11	1451	JOR	Jordan Fire Company	1500PMP-F	1500 GPM PUMPER (FIRE)	1993	01/02/94	\$245,093	\$ 451,329					
W-12	1452	JOR	Jordan Fire Company	9999		1984	08/01/84	\$160,000						\$ 964,795
W-15	1454	JOR	Jordan Fire Company	T&A-F	TOWER LADDER/AERIAL (FIRE)	1996		\$499,354						
W-16	1484	JOR	Jordan Fire Company	UTL-F	UTILITY UNIT (FIRE)	2005	04/22/05	\$43,594						
W-400	589	OSW	Oswegatchie Fire Company	9999		2008	01/08/08	\$192,378						
W-41	1103	OSW	Oswegatchie Fire Company	1500PMP-F	1500 GPM PUMPER (FIRE)	1998	02/16/99	\$279,030						
W-42	1349	OSW	Oswegatchie Fire Company	1500PMP-F	1500 GPM PUMPER (FIRE)	2007	05/14/08	\$416,972						
W-43	1026	OSW	Oswegatchie Fire Company	BRS-F	BRUSH TRUCK (FIRE)	2006	06/06/06	\$125,245						
W-46	1348	OSW	Oswegatchie Fire Company	UTL-F	UTILITY UNIT (FIRE)	2006	01/23/06	\$44,606						
W-94	1347	OSW	Oswegatchie Fire Company	SPC-F	SPECIALITY UNIT (FIRE)	1988	09/15/89	\$275,093	\$ 512,000					
CAR 1	377MHN	29	POLICE	CAR-DPW	CAR	2009	10/20/08	\$22,991						
CAR 10	10WFD	29	POLICE	CRU-DPW	CRUISER	2011	09/09/10	\$23,068				\$ 35,733		
CAR 11	11WFD	29	POLICE	CRU-DPW	CRUISER	2006	01/02/06	\$20,455	\$ 21,803				\$ 35,733	
CAR 12	12WFD	29	POLICE	CRU-DPW	CRUISER	2011	09/09/10	\$23,068						
CAR 13	13WFD	29	POLICE	CRU-DPW	CRUISER	2007	01/05/07	\$20,455	\$ 21,803					

RTM APPROVED FLEET MANAGEMENT PLAN FOR FISCAL YEARS 2012-2016

Asset ID Number	License Plate No.	Agency Code	Agency Name	Class Code	Class Description	Asset Model Year	In-Service Date	Gross Vehicle Cost	2012	2013	2014	2015	2016
CAR 14	14WFD	29	POLICE	CRU-DPW	CRUISER	2009	02/27/09	\$22,770			\$ 33,272		
CAR 15	15WFD	29	POLICE	CAR-DPW	CAR	2008	12/18/07	\$23,412					\$ 32,151
CAR 16	16WFD	29	POLICE	CRU-DPW	CRUISER	2008	08/13/08	\$20,744			\$ 33,682		
CAR 17	17WFD	29	POLICE	9999	CORVETTE (ASSET FORFEITURE)	1981	01/02/81	\$16,901					
CAR 18	18WFD	29	POLICE	9999	PICK UP (Used Vehicle)	2008	11/15/10	\$19,000					
CAR 19	19WFD	29	POLICE	CAR-DPW	CAR	2011	09/09/10	\$23,068					\$ 35,733
CAR 2	2WFD	29	POLICE	CAR-DPW*	CAR	2005	12/06/04	\$20,415	\$ 21,803				
CAR 20	20WFD	29	POLICE	CRU-DPW	CRUISER	2011	09/09/10	\$23,068					\$ 35,733
CAR 21	21WFD	29	POLICE	SUV-DPW	SUV	2003	09/01/02	\$27,879		\$ 37,726			
CAR 22	22WFD	29	POLICE	SUV-DPW	SUV	2005	03/02/05	\$29,288	\$ 28,552				
CAR 23	932PHE	29	POLICE	CAR-DPW*	CAR	2011	12/23/10	\$23,976					
CAR 24	931PHE	29	POLICE	CAR-DPW*	CAR	2011	12/23/10	\$23,976					
CAR 25	583UTH	29	POLICE	CAR-DPW	CAR	2007	09/20/06	\$19,686					\$ 34,692
CAR 26	584UTH	29	POLICE	CAR-DPW	CAR	2007	09/20/06	\$19,686					\$ 34,692
CAR 27	873PJX	29	POLICE	CAR-DPW	CAR	2009	02/11/09	\$19,090					
CAR 28	402UUX	29	POLICE	CAR-DPW	CAR	2007	09/20/06	\$19,686					\$ 34,692
CAR 29	935GHH	29	POLICE	CAR-DPW	CAR	2005	01/27/05	\$24,718		\$ 29,423			
CAR 3	3WFD	29	POLICE	CRU-DPW	CRUISER	2007	12/01/06	\$20,455	\$ 21,803				
CAR 30	156MCM	29	POLICE	CAR-DPW	CAR	2010	11/30/09	\$19,701					
CAR 31	539WNX	29	POLICE	CAR-DPW	CAR	2008	11/05/07	\$24,079					\$ 35,733
CAR 4	4WFD	29	POLICE	CRU-DPW	CRUISER	2006	01/02/06	\$20,455	\$ 21,803				
CAR 5	5WFD	29	POLICE	CRU-DPW	CRUISER	2011	09/09/10	\$23,068					\$ 36,624
CAR 6	6WFD	29	POLICE	CRU-DPW	CRUISER	2008	10/30/07	\$20,841		\$ 33,272			
CAR 7	7WFD	29	POLICE	CAR-DPW	CAR	2007	12/15/06	\$20,455					\$ 34,692
CAR 8	8WFD	29	POLICE	CRU-DPW	CRUISER	2011	09/09/10	\$23,068					\$ 36,624
CAR 9	9WFD	29	POLICE	CRU-DPW	CRUISER	2008	10/30/07	\$20,841		\$ 33,272			
CAR 33	236DJW	29	POLICE	9999	TOYOTA (ASSET FORFEITURE)	1993	02/27/08	\$18,751					
PD1	152WFD	29	POLICE	BOX-DPW	BOX TRAILER	1993	01/02/93	\$3,282		\$ 3,500			
CV1	118WFD	29	POLICE	9999	COMMAND VEH. FORMER AMBULANCE	1998	08/23/98	\$128,999					
A18	45WFD	30	PUBLIC WORKS	9999		2002	04/30/02	\$23,656					
A2	106WFD	30	PUBLIC WORKS	9999		2001	12/06/00	\$22,523					
A8	110WFD	30	PUBLIC WORKS	9999		2003	01/20/03	\$21,759					
H1	N/A	30	PUBLIC WORKS	FRK-DPW	FORK LIFT	1967	01/20/99	\$17,062					
H10	54WFD	30	PUBLIC WORKS	9999		2003	07/19/02	\$21,314					
H11	81WFD	30	PUBLIC WORKS	PCK-DPW	PICK UP	2007	09/27/06	\$29,837					
H12	80WFD	30	PUBLIC WORKS	PCK-DPW	PICK UP	2003	01/02/03	\$36,000					
H13	79WFD	30	PUBLIC WORKS	PCK-DPW	PICK UP	2002	11/09/01	\$25,000					\$ 44,166
H14	129WFD	30	PUBLIC WORKS	MDD-DPW	MEDIUM DUTY DUMP	1999	03/25/99	\$73,731			\$ 78,942		
H147	147WFD	30	PUBLIC WORKS	RLL-DPW	ROLLER TRAILER	1991	06/07/91	\$2,194			\$ 25,133		
H15	121WFD	30	PUBLIC WORKS	MDD-DPW	MEDIUM DUTY DUMP	1999	03/25/99	\$73,731			\$ 78,599		
H16	123WFD	30	PUBLIC WORKS	MDD-DPW	MEDIUM DUTY DUMP	2002	11/09/01	\$90,377					
H17	86WFD	30	PUBLIC WORKS	MDD-DPW	MEDIUM DUTY DUMP	2003	04/16/03	\$73,982					
H18	125WFD	30	PUBLIC WORKS	MDD-DPW	MEDIUM DUTY DUMP	2010	06/17/09	\$99,117					
H19	78WFD	30	PUBLIC WORKS	PCK-DPW	PICK UP	1997	12/09/97	\$19,829	\$ 39,288				
H24	56WFD	30	PUBLIC WORKS	SWP-DPW	SWEEPER	2003	10/18/02	\$118,670					
H25	57WFD	30	PUBLIC WORKS	9999	PELICAN SWEEPER	1994	08/20/94	\$117,071					
H26	62WFD	30	PUBLIC WORKS	9999	TRACTOR-MOWER	1959	01/01/59	N/A					
H27	63WFD	30	PUBLIC WORKS	TRC-DPW	TRACTOR	2008	05/20/08	\$82,026					
H30	87WFD	30	PUBLIC WORKS	5DMP-DPW	5 TON DUMP TRUCK	2000	06/01/00	\$124,378					
H31	88WFD	30	PUBLIC WORKS	5DMP-DPW	5 TON DUMP TRUCK	2000	05/23/00	\$86,950					
H32	95WFD	30	PUBLIC WORKS	5DMP-DPW	5 TON DUMP TRUCK	2000	05/31/00	\$86,950					

RTM APPROVED FLEET MANAGEMENT PLAN FOR FISCAL YEARS 2012-2016

Asset ID Number	License Plate No.	Agency Code	Agency Name	Class Code	Class Description	Asset Model Year	In-Service Date	Gross Vehicle Cost	2012	2013	2014	2015	2016
H34	94WFD	30	PUBLIC WORKS	5DMP-DPW	5 TON DUMP TRUCK	2004	03/19/04	\$91,116					
H35	92WFD	30	PUBLIC WORKS	5DMP-DPW	5 TON DUMP TRUCK	1990	12/01/89	\$61,468					
H36	124WFD	30	PUBLIC WORKS	5DMP-DPW	5 TON DUMP TRUCK	1996	04/01/96	\$47,505					\$ 139,511
H37	131WFD	30	PUBLIC WORKS	5DMP-DPW	5 TON DUMP TRUCK	1997	12/01/96	\$50,524					
H38	130WFD	30	PUBLIC WORKS	5DMP-DPW	5 TON DUMP TRUCK	1996	06/01/96	\$47,505					\$ 139,511
H39	96WFD	30	PUBLIC WORKS	5DMP-DPW	5 TON DUMP TRUCK	2009	07/23/08	\$115,800					
H40	97WFD	30	PUBLIC WORKS	5DMP-DPW	5 TON DUMP TRUCK	2003	06/13/03	\$91,116					
H42	99WFD	30	PUBLIC WORKS	216SPC-DPW	18-YEAR SPECIALITY UNIT	2010	12/31/09	\$104,350					
H43DT	161WFD	30	PUBLIC WORKS	TRL-DPW	TRAILER	2001	09/14/00	\$26,375					
H43LB	165WFD	30	PUBLIC WORKS	9999	TRAILER	1954	01/02/54	\$1,295					
H43RO	151WFD	30	PUBLIC WORKS	180SPC-DPW	15-YEAR SPECIALITY UNIT	2000	11/10/00	\$14,925					\$ 56,396
H44	158WFD	30	PUBLIC WORKS	CHP-DPW	CHIPPER	2005	11/18/04	\$24,000					
H45	60WFD	30	PUBLIC WORKS	BKH-DPW	BACKHOE	2009	03/15/10	\$89,180					
H47	58WFD	30	PUBLIC WORKS	LDR-DPW	TRACTOR/MOWER	2008	09/09/08	\$39,900					
H-33	135WFD	30	PUBLIC WORKS	GRN-DPW	STUMP GRINDER	2004	11/12/04	\$23,651					
H48	N/A	30	PUBLIC WORKS	RLL-DPW	ROLLER	1991	07/07/91	\$12,950	\$ 35,579				
H49	N/A	30	PUBLIC WORKS	9999	5-7 TON ROLLER	1970	01/02/70	\$36,000					
H50	101WFD	30	PUBLIC WORKS	VAC-CC	Vac-All Cab and Chassis	2009	04/30/08	\$63,890					
H-50V	N/A	30	PUBLIC WORKS	VAC-Body	Vac-All Body	2004	04/22/04	\$109,898					\$ 123,691
H51	90WFD	30	PUBLIC WORKS	216SPC-DPW	18-YEAR SPECIALITY UNIT	2010	06/04/10	\$180,550					
H52	162WFD	30	PUBLIC WORKS	CMP-DPW	COMPRESSOR	2004	12/16/04	\$14,420					
H53	66WFD	30	PUBLIC WORKS	PLW-DPW	SIDEWALK SNOW PLOW	2003	12/12/03	\$81,897					
H54	67WFD	30	PUBLIC WORKS	PLW-DPW	SIDEWALK SNOW PLOW	1986	11/01/86	\$32,977					
H55	127WFD	30	PUBLIC WORKS	CHP-DPW	CHIPPER	1998	04/21/98	\$17,045					
H56	N/A	30	PUBLIC WORKS	9999	PAVER	1977	01/01/77	\$2,770					
H58	146WFD	30	PUBLIC WORKS	CMP-DPW	COMPRESSOR	2008	10/14/08	\$16,985					
H6	108WFD	30	PUBLIC WORKS	PCK-DPW	PICK UP	2005	01/24/05	\$30,175					
H60	64WFD	30	PUBLIC WORKS	9999	GRADER	1970	01/02/70	\$34,775					
H64	61WFD	30	PUBLIC WORKS	LDR-DPW	LOADER	1997	01/10/97	\$110,238					
H65	59WFD	30	PUBLIC WORKS	LDR-DPW2	LOADER	2004	09/07/04	\$115,135					
H77	168WFD	30	PUBLIC WORKS	BOX-DPW	BOX TRAILER	1993	06/14/93	\$3,282	\$ 5,500				
H9	77WFD	30	PUBLIC WORKS	180SPC-DPW	15-YEAR SPECIALITY UNIT	2011	11/24/10	\$76,014					
H43T	164WFD	30	PUBLIC WORKS	UTL-F	UTILITY UNIT (FIRE)	2006	10/05/05	\$37,398					
S19	72WFD	30	PUBLIC WORKS	9999	SIDE LOAD PACKER	2001	04/09/01	\$141,824					
S20	71WFD	30	PUBLIC WORKS	SLP-DPW	SIDE LOAD PACKER	2007	01/16/07	\$194,875					\$ 240,425
S21	93WFD	30	PUBLIC WORKS	SLP-DPW	SIDE LOAD PACKER	2007	01/16/07	\$191,749					\$ 240,425
S22	70WFD	30	PUBLIC WORKS	SLP-DPW	SIDE LOAD PACKER	2011	03/15/11	\$218,336					
S23	73WFD	30	PUBLIC WORKS	RLP-DPW	REAR LOAD PACKER	2009	09/17/08	\$131,557					
S24	74WFD	30	PUBLIC WORKS	RLP-DPW	REAR LOAD PACKER	2009	09/19/08	\$131,557					
S25	75WFD	30	PUBLIC WORKS	RCY-DPW	RECYCLING TRUCK	2011	03/03/11	\$218,336					
S26	76WFD	30	PUBLIC WORKS	RCY-DPW	RECYCLING TRUCK	2011	03/03/11	\$218,336					
S27	68WFD	30	PUBLIC WORKS	RCY-DPW	RECYCLING TRUCK	2007	08/13/07	\$153,944					
S28	N/A	30	PUBLIC WORKS	9999	CRAWLER/DOZER	1993	10/14/93	\$114,650					
S29	69WFD	30	PUBLIC WORKS	FLP-DPW	FRONT LOAD PACKER	2001	05/27/01	\$149,165					\$ 131,392
S30	65WFD	30	PUBLIC WORKS	B&E-DPW	BULLDOZER/EXCAVATOR	2001	12/28/01	\$159,000					
H78	171WFD	30	PUBLIC WORKS	9999	TRAILER	1974	01/01/74	Homemade					
UT2	163WFD	30	PUBLIC WORKS	TRL-DPW	TRAILER	2009	07/27/09	\$11,857					
A20	104WFD	30	PUBLIC WORKS	9999	SUBURBAN	1992	07/01/92	\$26,946					
A7	107WFD		PZ/BH	9999	CAR	2012	10/13/11	\$26,928	\$ 14,241				
A9	111WFD		PZ/BH	9999	CAR	2012	10/13/11	\$26,928	\$ 14,241				
A10	112WFD		PZ/BH	SUV-DPW	SUV	2011	11/19/10	\$18,433					

RTM APPROVED FLEET MANAGEMENT PLAN FOR FISCAL YEARS 2012-2016

Asset ID Number	License Plate No.	Agency Code	Agency Name	Class Code	Class Description	Asset Model Year	In-Service Date	Gross Vehicle Cost	2012	2013	2014	2015	2016
A3	103WFD		PZ/BH	9999	CROWN VICTORIA	2003	11/30/03	\$22,153					
A4	114WFD		PZ/BH	9999	CROWN VICTORIA	2003	06/23/03	\$22,153					
W-200	590	QH	Quaker Hill Fire Company	9999	AMBULANCE	2004	09/28/04	\$144,000					
W-21	N/A	QH	Quaker Hill Fire Company	1500PMP-F	1500 GPM PUMPER (FIRE)	2006	10/02/06	\$396,447					
W-23	1025	QH	Quaker Hill Fire Company	BRS-F	BRUSH TRUCK (FIRE)	2006	07/01/06	\$125,245					
W-25	N/A	QH	Quaker Hill Fire Company	1500PMP-F	1500 GPM PUMPER (FIRE)	1993	11/01/93	\$372,000			\$ 699,700		
W-26	1285	QH	Quaker Hill Fire Company	UTL-F	UTILITY UNIT (FIRE)	2006	02/01/06	\$44,076					
P74	N/A	37	REC/PARKS	TRP RAKE	TRAP RAKE	2005	07/11/05	\$11,505					\$ 14,318
P50	42WFD	37	REC/PARKS	9999	TRACTOR	1980							
P51	119WFD	37	REC/PARKS	TRC-DPW	TRACTOR	2003	09/30/03	\$31,317					
P52	N/A	37	REC/PARKS	9999	TRACTOR	1987							
P53	N/A	37	REC/PARKS	MOW LG	MOWER, LARGE	2006	09/22/06	\$44,168					
P54	N/A	37	REC/PARKS	MOW-DPW	MOWER	2003	08/25/03	\$12,919					
P55	N/A	37	REC/PARKS	MOW LG	MOWER, LARGE	2005	05/13/05	\$42,511					\$ 49,421
P60	N/A	37	REC/PARKS	MOW LG	MOWER, LARGE	2002	07/12/02	\$37,344		\$ 46,300			
P61	43WFD	37	REC/PARKS	MOW2-DPW	TRACTOR/LOADER	1997	02/07/97	\$21,690		\$ 44,559			
P62	N/A	37	REC/PARKS	MOW2-DPW	MOWER	2000	07/19/00	\$19,341					\$ 24,548
P63	N/A	37	REC/PARKS	MOW2-DPW	MOWER	2000	04/24/00	\$19,341					\$ 22,916
P64	N/A	37	REC/PARKS	SED-DPW	SEEDER	2006	07/12/06	\$10,628					
R1	32WFD	37	REC/PARKS	9999	CAR	2003	01/20/03	\$21,759					
R10	36WFD	37	REC/PARKS	PCK-DPW	PICK UP	2001	03/17/00	\$20,662			\$ 40,620		
R11	37WFD	37	REC/PARKS	PCK-DPW	PICK UP	2004	02/06/04	\$27,037					
R14	38WFD	37	REC/PARKS	MDD-PKS	MEDIUM DUTY DUMP	2009	03/13/09	\$43,799					
R15	100WFD	37	REC/PARKS	PCK-DPW	PICK UP	2008	01/29/08	\$35,358					
R16	44WFD	37	REC/PARKS	PCK-PKS	PICK UP	2008	01/11/08	\$35,358					
R2	33WFD	37	REC/PARKS	SUV-DPW	SUV	2009	05/27/09	\$19,932					
R6	41WFD	37	REC/PARKS	9999	VAN	1999	01/02/99	\$27,530					
R8	34WFD	37	REC/PARKS	SUV-DPW	SUV	2004	08/20/04	\$18,000		\$ 38,763			
R9	35WFD	37	REC/PARKS	MDD-DPW	MEDIUM DUTY DUMP	2006	03/08/06	\$35,175					
R30	31934	35	SRCTZ	VAN-DPW	VAN	2009	10/21/08	\$46,912					
R4	26820	35	SRCTZ	VAN-DPW	VAN	2009	09/16/09	\$51,975					
R5	23979	35	SRCTZ	VAN-DPW	VAN	2006	10/31/06	\$43,960					\$ 56,979
C1	53WFD	31	WUC	SUV-DPW	SUV	2000	01/02/00	\$22,670					
C2	116WFD	31	WUC	VAN-DPW	VAN	2011	03/29/11	\$26,617					
C3	51WFD	31	WUC	240SPC-DPW	20-YEAR SPECIALITY UNIT	1991	01/02/91	\$114,000	\$ 180,000				
T5	46WFD	31	WUC	PCK-DPW	PICK UP	2006	06/20/06	\$20,939					
PG1	144WFD	31	WUC	GEN-DPW	GENERATOR	1982	01/02/82	\$19,125		\$ 25,000			
PG2	179WFD	31	WUC	GEN-DPW	GENERATOR	1991	01/02/91	\$20,000		\$ 25,000			
PP1	145WFD	31	WUC	PMP-DPW	PUMP	1985	01/02/85	\$21,250		\$ 26,200			
PP2	148WFD	31	WUC	PMP-DPW	PUMP	1990	01/02/90	\$21,250		\$ 26,200			
T1	48WFD	31	WUC	PCK-DPW	PICK UP	2006	06/20/06	\$35,665					
T2	49WFD	31	WUC	PCK-DPW	PICK UP	2011	07/27/11	\$55,594					
T3	82WFD	31	WUC	PCK-DPW	PICK UP	2004	01/31/04	\$28,597					
T4	52WFD	31	WUC	PCK-DPW	PICK UP	2003	10/23/02	\$30,768					

FLEET MANAGEMENT PLAN

Fire Services

The following vehicles have been designated for replacement in the Fleet management Plan:

Service/Command Vehicle (W-94)

This vehicle is a 1988 Salisbury custom rescue body vehicle that is housed at the Oswegatchie Fire station. This vehicle has out lived its life expectancy and currently has over 26,000 miles. The vehicle is showing rust and it is time to begin the replacement process for this vehicle. This unit carries bulky salvage equipment to aid all Waterford departments. This unit carries the mounted cascade system for the filling of SCBA units at incident scenes. This unit also serves as the mobile command post during large scale incidents and can facilitate the Firefighter rehab function at any scene. The unit will be replaced with a scaled down unit and the project funding will not exceed \$375,000.

Fire Marshal Vehicle (Car 55)

This vehicle is a 2005 Ford expedition that is utilized on a take home basis by the Fire Marshal. This vehicle has served its life expectancy and currently has over 85,000 miles. The vehicle is showing rust and it is time to begin the replacement process for this vehicle. This unit is a 4WD unit that carries a large amount of the fire investigation equipment that is utilized by the Bureau of Fire Prevention. This unit will be replaced with a similar type vehicle.

Fire Marshal Vehicle (Car 85)

This vehicle is a 2005 Ford Crown Victoria that is utilized on a work-day basis by the Director of Fire Services. This vehicle is has only 33,000 miles on it and does not require replacement at this time. When replacement is necessary (Possibly FY-2015), this unit will be replaced with a similar type vehicle.

Fire Marshal Vehicle (Car 105)

This vehicle is a 2005 Ford Crown Victoria that is utilized on a work-day basis by one of the Fire Inspectors. This vehicle is has over 38,000 miles on it and it is utilized as an emergency response type unit also. This vehicle is in good operational condition at this time and I would like to hold off for one year and replace this vehicle in conjunction with Car 115. Both units are utilized in a similar fashion and Car 115 is due for replacement in FY2014 as it is a 2006 unit.

Memo

To: Rudie Beers, Director of finance

From: Chief Murray J. Pendleton

CC: Files

Date: 1/9/2012

Re: Variance to Fleet Management Program

This memo is in regards to the police department requesting a variance to the fleet management program.

We are requesting that following marked police car be pushed out for replacement to the FYI 2014:

Car #22- low mileage and low repair costs as of this date. This vehicle is also a four wheel drive vehicle and is not used by the patrol division as a regular patrol vehicle. It is currently assigned to the Support Services Division (Traffic Officer).

Rudie Beers

From: Stephen Bellos
Sent: Monday, January 09, 2012 3:17 PM
To: Rudie Beers
Subject: RE: Car #22

Sounds good to me I still cant figure out what trailer it is. I believe it is the regional traffic trailer and it is in good condition.

From: Rudie Beers
Sent: January 09, 2012 14:30
To: Stephen Bellos
Subject: RE: Car #22

Thanks Steve – What about putting off replace of PD1 to FY18? - Rudie

From: Stephen Bellos
Sent: Monday, January 09, 2012 1:52 PM
To: Rudie Beers
Subject: Car #22

See Attached

Sergeant Stephen Bellos
Waterford Police Department
(860) 442-9451 ext. 312
sbellos@waterfordct.org

FIFTEEN ROPE FERRY ROAD



WATERFORD, CT 06385-2886

NOV - 2 2011

RECEIVED
CITY OF WATERFORD
FINANCE DEPT.

Waterford Recreation and Parks Commission

TO: Rudie Beers, Finance Director
FROM: Ryan McNamara, Assistant Director
DATE: November 1, 2011
RE: Fleet Management Variance

A review has been conducted of the most recently approved First Selectman's Fleet Management Plan for FY 2012 – 2016. In previous plans there was equipment identified for non-replacement in concert with the current usage and operations. As most are aware, the Maintenance Division of R&P saw five (5) employees take advantage of the Retirement Incentive Program in June of 2011. This resulted in the contracting of the BOE grounds as well as an overall decrease in the recommended staffing. These two variables has provided some changes in the scope of work being requested and the best practices to efficiently and effectively complete these requests.

The issue of decreased staffing will lead most of this request for a variance. The snow removal responsibilities did not decrease in comparison to the lessened landscaping requirements at the BOE grounds. The R&P Maintenance division is responsible for approximately 15,000 sf of sidewalks or three (3) miles. This figure does not include snow removal from stairwells and landings. Responsibilities include sidewalks and parking lots for:

- Town Hall
- Emergency Services
- Police Department
- Youth Services
- Community Center
- Friendship School
- Leary Park
- Jordan Parkhouse
- Library
- O'Neill Theater
- Civic Triangle Park
- Waterford Beach Park
- Mago Point Park
- Dedrick Park
- Other Town areas as assigned

The decrease in staffing is currently 2 – 3 individuals.

The equipment requested is a multi-use, multi-seasonal machine. The Bacher Corporation has a 21 HP Kubota Diesel engine that can provide numerous functions with the attachment availability. It can be modified in the winter to include a cab and v-blade for sidewalk snow removal. It also has the capability of holding a 60" rotary mowing deck for use in the other seasons. There are further attachments such as rototillers, stump grinders, core aerators, power blowers, front end loader, etc. that could be purchased in future years to help accomplish the variety of tasks each park and responsibility offers. The release of the responsibility to the BOE grounds has provided the department with opportunities to tend to many different aspects of the park systems, athletic field maintenance, and Town landscaping efforts.

Public Works has also seen a decrease in staffing and the availability to receive timely assistance is understandably difficult. The machines are not available for use by our employees and currently any work requested can be done on an overtime basis. Our employees are skilled to use these machines and competent enough to complete the work required. Only the proper equipment is required.

The Recreation and Parks Fleet Management Listing does not have any machines similar to this request. The work is currently done with snow shovels and snow blowers. We cannot perform sidewalk maintenance with any of the plows or the Kubota because the buckets are much larger than the width of the sidewalk and turf would be extremely damaged. As we move forward with less staffing and more work, I highly recommend the purchase of this machine to assist with lowering risk of injury and claims.

Although I am requesting this machine for immediate review and purchase as soon as possible for the upcoming winter, I would like to offer some solutions and/or suggestions.

In FY2012, Fleet Management tag P61 is again due for replacement after R&P deferred purchase last year. I am again requesting deferment as the machine is still in good working condition and any issues should be able to be resolved by the Town mechanics at less cost to the Town. In FY2012 this replacement cost is \$44,559.

I believe that the deferment for P61 can be repositioned to FY2015. The usage of the newly requested Kubota multi-purpose machine may indeed prove to be cost savings for attachments that could replace other existing equipment. This IS NOT a zero turn mower but one that could most likely handle large areas with a box turn as opposed to a simple spin with a zero turn. Use and recordings will take place to determine the future needs of scheduled equipment.

Again, the P61 tag for FY2012 is \$44,559. The multi-purpose machine request is \$31,290.55 which includes the base machine, snow blade, cab, light kit, and 60" rotary mower deck. Freight, delivery, and set-up may fluctuate. Bacher Corporation is also on the State Contract #10PSX0307 and the price above reflects a 5% discount.

In an effort to compare, rental prices in 2011 are \$3,500 per month which would incur costs of roughly \$10,500 - \$14,000 per year. Purchase of this machine would realize a savings immediately with its efficiency but also all usage after 3+ years.

In conclusion, I believe this piece of equipment is vital to the snow removal operations, will aid in overall operations with the attachment capability year round, as well as cut down the amount of hours needed for snow removal and staff required. Depending on the timing and amount of snow fall, this machine could also very well cut down on overtime shifts.

I have attached the quotes (rental and purchase) and catalogue for review as well. Please contact me with any questions regarding this request and I thank you for your time and consideration.

Proposal

BACHER CORPORATION

Turf Equipment and Vegetation Control Machinery
 P.O. Box 733, Thompson Road, East Windsor Industrial Park
 East Windsor, CT 06088
 860-627-5924 Fax 860-292-6393 800-724-0635

Proposal Submitted to:
 Town of Waterford
 Park & Rec Dept.
 Attn: Ryan McNamara
 15 Rope Ferry Road
 Waterford, CT 06385

Date
 October 19, 2011

We hereby submit specifications and estimates for:

	MSRP
One (1) Steiner, Model 430 with 21HP Kubota Diesel Engine with Field Trax Tires	\$17,950.00
Cab, Model CB430 w/wiper blade (requires ROPS roll bar)	2,385.00
ROPS Roll Bar with Seat Belt, Model RB430	702.00
Suspension Seat Kit	545.00
Rear Weight Bar w/ 4 weights, 3 pt hitch Model WB153	630.00
3 Point hitch, category "0"	885.15
Reverse Fan for Heat	200.00
High Output Alternator	780.00
Blade, V-Blade, BD358	<u>2,992.50</u>
5% Discount State of CT Contract #10PSX0307	\$27,069.65
*Incoming freight, setup and delivery	<u>-1,353.45</u>
Options	\$25,715.55
	<u>2,000.00 Net</u>
	\$27,715.55

60" Mower Deck, Hi capacity, flip up, Model MD460	2,975.00
Core Aerator (with 4 weights)	1,921.00
Power Blower with deflector	1,299.00
Front End Loader with 2 weights	4,189.00
48" Slip Scoop, Hydraulic	1,225.00
48" Snow Blower, hydraulic chute, Model SB448	3,840.00
Electric Chute Deflector for SB448	249.00
Stump Cutter	2,820.00
54" Rotary sweeper with gauge wheels standard, Model RS350	3,118.00
48" Rototiller, Model TL348	2,619.00
Light Kit	600.00

Take 5% off of the above options per CT State Contract #10PSX0307

* If all above items are purchased, if only part of this proposal is purchased please contact me for a revised freight price

We Propose hereby to furnish the above - complete in accordance with the above specifications, for the sum of: :

Payment to be made as follows: Net 30 days

All material is guaranteed to be as specified. All work is to be completed in a workman like manner according to standard practices. Any alteration or deviation from above specifications involving extra costs will be executed only on written orders, and will become an extra charge over and above the estimate. All agreements contingent upon strikes, accidents or delays beyond our control. Owner shall carry fire, ~~and other necessary insurance. Our workers are fully covered by Workmen's~~ compensation insurance. jd

Authorized
Signature _____

GARRY RIENDEAU

Acceptance of Proposal— The above prices, specifications and conditions are satisfactory and are hereby accepted. You are authorized to do the work as specified. Payment will be made as outlined above. Date of Acceptance

Signature _____
 Signature _____

BACHER CORPORATION

Industrial Grounds & Turf Equipment
7 Thompson Road P.O. Box 733
East Windsor Industrial Park East Windsor, CT 06088
860-627-5924 Fax: 860-292-6393

October 26, 2011

Town of Waterford
Park & Rec Dept.
Attn: Ryan McNamara
15 Rope Ferry Road
Waterford, CT 06385

Ryan,

Per our conversation I am submitting the cost to rent a Steiner Model 430 with Cab and Plow:

Price per week will be \$875.00, based on a 5 day week

Delivery of unit to your location and pick up of unit when rental period ends: \$200.00

The Town of Waterford must list the Bacher Corporation as an Additional Insured-Lessor on their General Liability Policy. A copy the certificate must be received by the Bacher Corp. prior to the equipment being delivered. Bacher's address is listed above. In the event of an accident, the Bacher Corp. must be notified immediately. The Bacher Corp. shall not be held liable for any loss of or damage to property when the equipment is in the Town of Waterford's possession.

The equipment will be delivered to the Town of Waterford in working condition with all known repairs and preventive maintenance completed. The Town of Waterford is responsible for keeping the equipment in good repair and operating condition as it was when delivered. Fluids and tires should be checked on a daily basis. In the course of the rental, if repair parts and service calls are required the customer should notify the Bacher Corp. immediately so the necessary work may be completed. The customer will be invoiced for repair parts and service calls.

The Town of Waterford shall maintain the equipment in good operating condition, repair, appearance and must protect the equipment from deterioration other than normal wear and tear. The equipment will be used in the manner it was intended for within normal operating capacity without abuse. Modifications, alterations or additions to the equipment without notifying Bacher will not be allowed. The equipment should not be removed from the area designated within the business/town.

If there is anything else I can help you with please let me know.

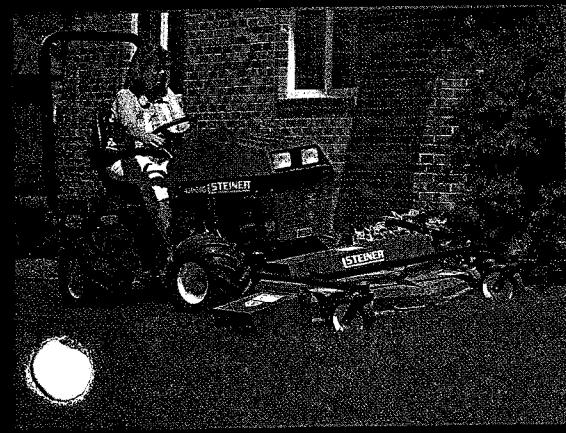
Sincerely,

Garry Riendeau
Outside Sales

GR/lp

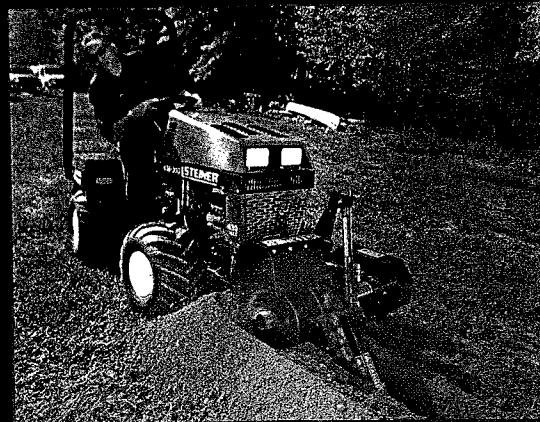
430 TRACTOR: THE GO ANYWHERE, DO ANYTHING TRACTOR FOR ALL SEASONS

No other tractor has the versatility of the Steiner 430 and its 28 attachments. Compact enough to fit into a pickup truck; strong enough for any task. The Steiner 430's unique combination of articulating steering, oscillating frame, four-wheel drive, and low center of gravity conquers the toughest conditions and makes short work of almost any chore you can throw at it.



Full-float action allows Steiner optional mowing decks to glide over ground contours for a smooth cut with less scalping. Standard rear roller provides a fairway-finished look to your lawn. Steiner rough cut mowers and tree farm mowers make short work of tall grass and woody plant material.

Trench, till gardens, load soil, sand, or gravel with our tough dirt and site preparation attachments.



1 TRACTION AND STABILITY

With full-time four-wheel drive, a low center of gravity, articulating steering, and oscillating frame, the 430 works up, down, and around slopes, maneuvers in tight spaces, and powers through deep snow or mud with confidence.

2 COMFORT AND EASY OPERATION

Comfort and convenience equals productivity with easy-to-reach, hand-controlled forward and reverse operation, hydrostatic transmission with infinite variable speeds, power steering, illuminated instrument panel, and deluxe, ergonomically designed seat.

3 28 AVAILABLE ATTACHMENTS

What do you want to get done today? Whatever it is, the 430 Tractor has the attachments to help you cross it off the list. The attachments include mowing decks, soil and site preparation attachments, snow blowers, sweepers, scoops, and blades. There are also specialty attachments for blowing, digging post holes, or chipping and shredding, making the 430 the most productive tractor available.

4 JUST HOOK UP AND GO

A few minutes, that's all it takes to switch attachments with our *Quick-Hitch™* System and hydraulic quick-couplers. Winter snow removal or summer yard work, with the 430 and our *Quick-Hitch* feature, changing with the seasons is fast and easy.

5 A CHOICE OF ENGINES

There are four power options to choose from: select either a Kubota® liquid-cooled, 21 horsepower diesel or Kubota liquid-cooled, 25 horsepower gas. The air-cooled, 30 horsepower Kohler® Command PRO and the Generac® air-cooled, 33 horsepower engines both have the power for any task.

6 CONVENIENT ACCESS

Service and maintenance is made simple with the 430 tractor. The hood opens up wide for easy access to hydraulic oil filters, dipsticks, belts, and hoses and makes keeping your 430 in peak running condition effortless.

7 ACCESSORIES TO SUIT YOUR NEEDS

As if it weren't versatile enough, the assortment of accessories for the 430 lets you fit it out for your particular needs. There's a handy utility box for hauling tools, material, or debris, dual wheels for extra stability, our traction boost system, ROPS with seatbelt, a weight bar kit, and canopies.

Specifications on page 13



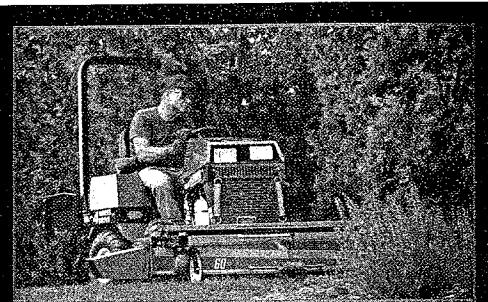
Remove snow from sidewalks, driveways, and other areas with the 430 tractor and a complete selection of snow removal attachments. The 430 tractor is just 44 inches wide making it perfect for clearing residential and commercial walks and paved surface areas of snow.

Productive specialty attachments like our turbine blower, power blower, vertical auger, and chipper/shredder let you get more work out of the 430 tractor for your money.



GET MORE DONE WITH STEINER SITE PREPARATION & TURF MAINTENANCE ATTACHMENTS

It's almost quicker to describe what you can't do with a Steiner tractor and attachments than what you can do. Mow lawns, take down tall weeds, till soil, aerate turf, move soil and sand; the list goes on and on. Steiner offers a broad range of site preparation and turf maintenance attachments.



◀ SIDE-DISCHARGE ROTARY MOWERS

Steiner three-blade side-discharge rotary mowers feature tapered roller-bearing spindle assemblies for long life. The decks are full-floating to reduce scalping. You adjust the height of cut easily with a single lever adjustment. High-velocity blade speeds and channelled discharge chute provides even distribution of clippings. For easy service, just flip the deck up over 90 degrees and get convenient access to the blades and underside of the deck.



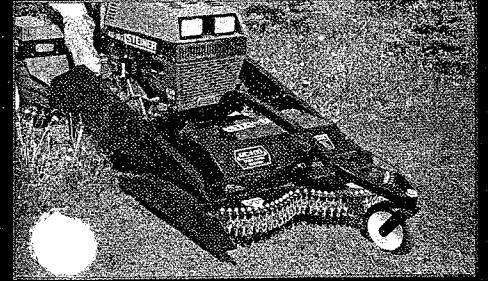
◀ REAR-DISCHARGE / MULCHING ROTARY DECK

Go from rear-discharge to mulching deck by simply installing the separate blade and baffle option. The deck flips up over 90 degrees to service the blades or remove caked-on clippings and debris from the underside.



◀ ROUGH CUT MOWER

Cut tall grass and woody stems up to one-inch thick with our heavy-duty Rough Cut Mower. Counter-rotating discs work in unison to cut and move material quickly and efficiently out the rear-discharge chute. Protective chains keep clippings and debris from flying out from under the mower deck.



◀ TREE FARM MOWER

Our tree farm mower takes down grasses and weeds between rows with ease. The out-front design gives you good visibility for two-sided trimming.

Specifications on page 14



◀ CORE AERATOR

Aerate turf for better health with our easy-to-use core aerator. Independent $\frac{3}{4}$ -inch tine assemblies provide $6\frac{1}{2}$ -inch by 7-inch coring pattern. To aerate deeper, just add more weight to the weight box.



◀ SOIL TILLER

Preparing the soil for planting is quick and easy with our tough tiller attachment. Tilling soil to a depth of 6 inches is ideal for planting beds. The 48-inch swath helps finish the job quickly. The durable chain-drive rotor is made with a solid $1\frac{1}{2}$ -inch steel shaft and holds 24 cutting tines.



◀ DURABLE TRENCHER

This tough trencher follows the line of trench with minimum effort. An easy-to-see depth indicator gauge makes it easy to adjust the depth of cut.



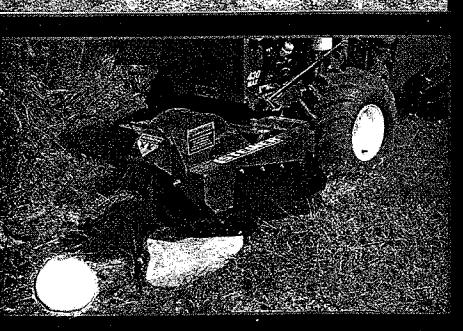
◀ SLIP SCOOP

Use the Slip Scoop for virtually any job that normally requires a wheelbarrow to easily move, lift, and haul materials around your property. The Slip Scoop is also ideal for excavating small areas quickly and easily.



◀ FRONT LOADER

Load and lift material with the tough front loader. The bucket is a productive 48-inches wide and holds up to 6 cubic feet of material. The loader has a self-contained hydraulic pump and a dedicated oil reservoir for power enough to transport up to 200 pounds of sand, soil, gravel, or manure.



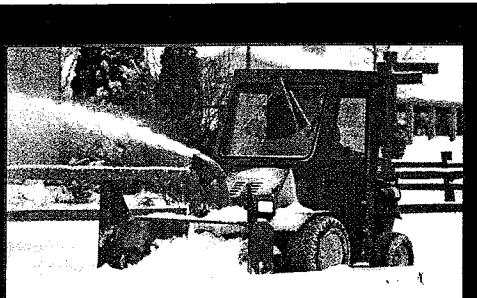
◀ STUMP CUTTER

This tough stump cutter features a durable 16-inch diamond cutter head. A PTO belt driven off of a right angle gearbox powers the cutter head for reliable performance. Hardened cutters are specifically located to let you grind both side-to-side and from the top down.

Specifications on page 14

MAKE SHORT WORK OF WINTER'S WORST WITH STEINER SNOW REMOVAL ATTACHMENTS

When snow piles on, put it in its place with Steiner snow removal attachments. Available for both the 430 and 235 tractors, they give you the power and ability to blow heavy snow, sweep or push it aside, and carry it away.



◀ SB448 AND SB454 SNOW BLOWERS

Both snow blowers feature heavy-duty 10- and 12-gauge steel side panels and housing with double-layered 3/16-inch base for solid durability, and a no-maintenance heavy-duty gearbox. Roller bearings and brass bushings support the high-RPM auger and 1100-RPM impeller. The serrated auger slices through icy snow, while a replaceable and adjustable high-carbon steel scraper bar and adjustable skid shoes scrape the surface clean. The discharge chute rotates hydraulically 237 degrees. Drift cutter bars and electric chute deflection control are optional.

◀ ROTARY SWEEPERS

Our efficient Rotary Sweepers efficiently remove light snow from sidewalks, driveways, and other paved areas quickly and easily. Convenient hydraulic controls let you angle the broom either left or right and sweep snow where you want it.

◀ POWER ANGLE BLADES

Push snow away with one of our sturdy Power Angle Blades. Available in three widths, you can angle the Power Blade left or right for efficient placement of snow. A spring-loaded forward-tilt safety trip protects the Power Angle blade from damage if you strike an object.

◀ V-BLADE

Select the most effective configuration for plowing snow with our versatile V-Blade. Just engage the electro-hydraulic controls on the move and choose V-blade, straight blade, or scoop positions to move snow the quickest and most efficient way for the conditions.

◀ TRACTOR CAB

Protect yourself from the elements year-round with our sturdy and comfortable tractor cab. It features safety laminated glass windows that you can remove in summer for more airflow.

Specifications on page 15

ONE TRACTOR DOES MANY CHORES WITH VERSATILE STEINER SPECIALTY ATTACHMENTS

It's amazing what a Steiner can do when you give it the right tools. Get more done and be more profitable with Steiner specialty implements including power blowers, vertical auger, lawn sweeper, and chipper/shredder.



POWER BLOWER

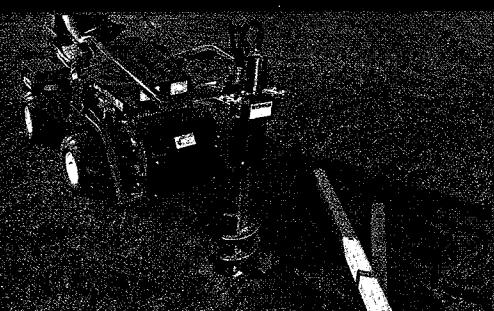
With a powerful eight-blade blower fan providing a windblast up to 150 mph, you'll clear debris from paved and grassed areas with ease.

TURBINE BLOWER

With our powerful turbine blower, you can rotate the nozzle 360 degrees to blow debris in the safest, most efficient direction. The axial-flow turbine-design blower runs quietly yet produces wind speeds up to 114 MPH. A blower-mounted kickstand combines with our convenient Quick-Hitch™ system to make attaching the blower to the 430 or 235 tractors quick and easy.

LAWN SWEeper

Remove grass clippings and leaves from turf with our efficient lawn sweeper. It features a three-paddle, staggered brush design that picks up and packs debris efficiently in to the large 24-cubic-foot hopper.



VERTICAL AUGER

Vertical auger is self-leveling so you can dig holes for fence posts or plant trees, even on sloped ground. Powered through an independent hydraulic circuit, our powerful auger features a reverse control for easier to withdrawal from the soil. The cutting edges tips are replaceable for long life.

CHIPPER / SHREDDER

Chip limbs up to 4 inches in diameter and shred soft material up to 1 ½ inches in diameter with the tough chipper/shredder. Four hardened knives efficiently process limbs and debris to produce mulch for gardens, pathways, or flowerbeds.

MACHINERY HITCH

Attach this handy machinery hitch to the front of the 430 or 235 tractor and maneuver flat bed trailers, boat trailers, or wheel-mounted implements and machines with tractor power. The out-front design provides total visibility for efficient operation.

Specifications on page 15

ATTACHMENT SPECIFICATIONS

SITE PREPARATION & TURF MAINTENANCE ATTACHMENTS

► STUMP CUTTER

Overall width: 34-1/2"
Overall height: 18"
Overall length: 37"
Cutterhead: 24 hardened cutters
Cutterhead diameter: 16"
Cutterhead width: 3-1/4"
Cutterhead speed: 1600 rpm
Cutting depth below ground level: 5"
Maximum recommended cutting height
above ground level: 5"
Weight: 250 lbs
Fits the 430 only

► SLIP SCOOP

Overall width: 53-1/4"
Overall height: 16"
Overall length: 33"
Width of bucket: 48"
Bucket capacity: 4.5 cu ft
Dumping: Hydraulic
Ground clearance (transport)
12-1/2" on Model 430; 11-3/4" on Model 410
Ground clearance (dumped)
7-1/4" on Model 430; 6-1/2" on Model 410
Scarifier teeth: 6-1" wide
Weight: 150 lbs

► LOADER

Lift height: 73"
Maximum lift capacity: 365 lbs
Maximum transport lift capacity: 200 lbs
Reach: Raised: 30"; Lowered: 27"
Bucket volume: (approx.) 6 cu ft
Bucket width: 48"
Weight: 702 lbs
Weightbar required
Fits the 430 only

► TILLER

Working width: 48"
Working depth: 6" maximum
Overall height: 29"
Overall width: 54"
Overall length: 38"
Tines: 24 replaceable, hardened
Drive: Chain drive from gearbox
Operation: Tractor in reverse motion
Bearings: Greaseable
Weight: 305 lbs

► TRENCHER

Overall width: 37"
Overall length: 5' 4"
Overall height: 34"
Auger diameter: 12"
Chain speed: 550 ft/minute
Drive shaft diameter: 1-1/2"
Cutting depth: 28"
Cutting width: 5-1/2", standard
Weight: 360 lbs

► CORE AERATOR

Overall width: 45-1/2"
Working width: 40-1/4"
Overall height: 28-1/2"
Overall length: 30"
Main shaft bearings: Greasable 1-1/4"
Aerating wheels: 7 steel with bushings/ pressurized lubrication fitting
Tines: 6 per wheel, 3/4" core tines
Aerating width (actual): 39"; (effective): 45-1/2"
Aerating depth: Up to 3-1/2"
Aerating pattern: 6-1/2" x 7" (3.6 holes sq/ft)
Weight: 390 lbs with 4 weights

► SIDE DISCHARGE ROTARY MOWER MODEL MD448

Overall width with discharge chute: 60-1/2"
Cutting width: 47"
Cutting height: 1" to 4-1/2"
Spindle drive: Single "V" belt
Spindles: Tapered roller assemblies
Blades: 3 blades, 16-1/4"
Discharge: Side
Wheels: 10 x 3.5 semi-pneumatic
Weight: 330 lbs
Drive shaft to PTO belt

► SIDE DISCHARGE ROTARY MOWER MODEL MD460

Overall width with discharge chute: 73"
Cutting width: 59-3/8"
Cutting height: 1" to 4-1/2"
Spindle drive: Single "V" belt
Spindles: Tapered roller assemblies
Blades: 3 blades, 20-1/2"
Discharge: Side
Wheels: 10 x 3.5 semi-pneumatic
Weight: 360 lbs
Drive shaft to PTO belt

► SIDE DISCHARGE ROTARY MOWER MODEL MD472

Overall width with discharge chute: 85"
Cutting width: 72"
Cutting height: 1" to 4"
Spindle drive: Single "V" belt
Spindles: Tapered roller assemblies
Blades: 3 blades, 24-3/4"
Discharge: Side
Wheels: 10 x 3.5 semi-pneumatic
Weight: 470 lbs
Drive shaft to PTO belt

► REAR DISCHARGE ROTARY MOWER DECK - MODEL MX460

Overall width: 60-3/4"
Cutting width: 60"
Cutting height: 1-1/2" to 4"
Spindle drive: Single "V" belt
Spindles: Tapered roller assemblies
Wheels: 10.25 x 3.25 semi-pneumatic
Blades: 3 blades, 20-3/4"
Weight: Rear discharge: 380 lbs, Mulching option: 400 lbs
Drive shaft to PTO belt

► TREE FARM MOWER

Overall width: 45-1/2"
Cutting width: 44"
Overall length: 79" including wheel shields
Cutting height: 2" to 5"
Spindle drive: Gearbox and 2 V belts
Spindles: 1" ball bearings
Blades: 2
Discharge: Rear
Weight: 272 lbs
Drive shaft to PTO belt

► ROUGH CUT MOWER

Overall width: 70"
Caster wheels: 13x5.00-6
Mower drive: PTO belt
Spindles: Cast iron housings, ball bearings
Cutting width: 66", offset 16" to right
Cutting height: 3"- 5-1/2"
Blades: Replaceable breakaway
Discharge: Rear
Weight: 550 lbs

NOW REMOVAL ATTACHMENTS

► SB448 & SB454 SNOW BLOWERS

SB448: 40", two stage snow blower for 430 tractor
 SB454: 54", two stage snow thrower for both 430 and 235 tractors (Drift cutter bars standard)
 Opening size: 21"
 Serrated Auger: 15"
 Impeller: 4 blades, 14" with brass bushing on impeller fan shaft
 Fan auger shear pins: Mounted on hardened bushings
 Roller Bearings: On auger side panels and nylon bushings on shaft
 Chute: Solid steel hydraulic chute with 237-degree rotation standard and optional electric chute deflection
 Skid shoes: 2" adjustable heavy-duty skid shoes, $\frac{1}{4}$ " inch high carbon steel
 Side panels: 10 gauge steel
 Housing (Auger and Impeller): 12 gauge steel
 Base: Double layered $\frac{3}{16}$ " steel base
 Scraper bar: 2" reversible made of $\frac{3}{16}$ " high carbon steel
 Hydraulic Chute Rotation: Hydraulic hose and quick-connect couplers standard
 Powder Coat Finish
 Warranty: 2-year commercial warranty
 Ship Weight: SB448 - 330 lbs; SB454 - 385 lbs

► POWER ANGLE BLADE

Angled width BD248, BD260, and BD272: 36", 48", 60"
 Overall height: 9"
 Overall length: 32"
 Blade height from cutting edge: 19"
 Hydraulic angling: 30° left and right
 Skid shoe: Adjustable
 Forward tilt: Spring-loaded
 Reversible cutting edge: 3" hardened
 Ground clearance: 14" on Model 430 Max
 Scarifier teeth: $\frac{3}{8}$ " x 4" through hardened steel
 Scarifier pitch: 2" sawtooth edge
 Weight BD248, BD260, and BD272: 160 lbs/185 lbs
 Optional caster wheel kit replaces skid shoes
 Optional $\frac{3}{4}$ " x 4" heavy-duty cutting edge

► ROTARY SWEEPER

Models	RS350	RS454
Overall width:	61"	62"
Overall height:	18"	18"
Overall length:	44"	44"
Broom width:	54"	54"
Broom diameter:	16"	24"
Working width:	54"	54"
Hydraulic angling (left & right):	30°	30°
Speed of broom:	300 rpm	265 rpm
Gearbox:	Bevelled gears	
Weight:	170 lbs	475 lbs
Ground clearance:	10" (Model 430) 11-1/2" (Model 235)	adjustable castor wheels

► V-BLADE

Overall width: 69-1/4"
 Cutting width: straight: 58"; scoop: 49"; v-blade: 53"
 Overall height: 21"
 Overall length: 31"
 Blade height from cutting edge: 21"
 Angling: 30° left and right, front and back
 Skid shoes: Adjustable
 Forward tilt: Spring-loaded
 Cutting edge, 2 position: $\frac{3}{8}$ " x 4" hardened
 Weight: 248 lbs

► TRACTOR CABS

Weight: 180 lbs
 Overall height: 69"
 Air flow: 225 CFM
 Windows: Safety laminated glass
 Windshield wipers: Electric 12 V
 Mounting: Frame and ROPS roll bar

SPECIALTY IMPLEMENTS

► POWER BLOWER

Overall width: 33"
 Overall height: 18-1/2"
 Overall length: 26"
 Skid shoe: Fixed
 Fan: 8-blade, welded
 Fan diameter: 14"
 Fan width: 6"
 Fan speed: Same as engine rpm
 Air flow: 3000 cfm at max rpm
 Ground clearance in raised position: 6" on Model 430 Max
 Weight: 95 lbs

► TURBINE POWER BLOWER

Overall width: 27"
 Overall height: 33"
 Overall length: 67"
 Airflow: 8000 cfm
 Wind speed: 114 mph
 Wind speed (deflected to front): 78 mph
 Sound level (while seated in operator seat): 91 dBA
 Weight: 235 lbs
 Weight on caster: 195 lbs

► MACHINERY HITCH

Overall width: 19"
 Overall length: 29"
 Lift capacity: 400 lbs
 Ball or pin hole: 1"
 Weight: 29 lbs

► LAWN SWEeper

Overall width: 62"
 Overall height: 52"
 Overall length to hitch: 82"
 Width of pickup: 40"
 Brush: 3-paddle staggered
 Depth wheels: Semi-pneumatic
 Hitch: Right, left, or center pull
 Carrying capacity: 24 cu ft
 Weight: 530 lbs

► VERTICAL AUGER

Overall width: 38"
 Overall height: 72-3/4"
 Overall length: 81"
 Depth of hole (for all size augers): 42"
 Auger torque: 600 ft/lbs
 Auger rpm (no load): 115 rpm
 Hydraulic pump flow (at 3600 engine rpm): 10 gpm
 Relief setting: 2500 psi
 Oil capacity: 27 quarts
 Weight (w/o auger but w/ oil, mounting shaft & clamps): 445 lbs
 Auger weight: 9" = 39 lbs, 12" = 53 lbs

► CHIPPER/SHREdder

Overall width: 35-5/8"
 Overall height: 44-1/2"
 Overall length: 53-1/4"
 Chipper knives: 4 hardened knives
 Maximum material size for chipper: 4" diameter
 Maximum material size for shredder: 1-1/2" diameter
 Weight: 430 lbs

Proposal

BACHER CORPORATION

Turf Equipment and Vegetation Control Machinery
 P.O. Box 733, Thompson Road, East Windsor Industrial Park
 East Windsor, CT 06088
 860-627-5924 Fax 860-292-6393 800-724-0635

Proposal Submitted to:

Town of Waterford
 Park & Rec Dept.
 Attn: Ryan McNamara
 15 Rope Ferry Road
 Waterford, CT 06385

Date
 October 19, 2011

We hereby submit specifications and estimates for:

	MSRP
One (1) Steiner, Model 430 with 21HP Kubota Diesel Engine with Field Trax Tires	\$17,950.00
Cab, Model CB430 w/wiper blade (requires ROPS roll bar)	2,385.00
ROPS Roll Bar with Seat Belt, Model RB430	702.00
Suspension Seat Kit	545.00
Rear Weight Bar w/ 4 weights, 3 pt hitch Model WB153	630.00
3 Point hitch, category "0"	885.15
Reverse Fan for Heat	200.00
High Output Alternator	780.00
Blade, V-Blade, BD358	<u>2,992.50</u>
	<u>\$27,069.65</u>
5% Discount State of CT Contract #10PSX0307	<u>-1,353.45</u>
	<u>\$25,715.55</u>
*Incoming freight, setup and delivery	<u>2,000.00 Net</u>
	<u>\$27,715.55</u>

5% Discount State of CT Contract #10PSX0307

*Incoming freight, setup and delivery

al

options

60" Mower Deck, Hi capacity, flip up, Model MD460	2,975.00
Core Aerator (with 4 weights)	1,921.00
Power Blower with deflector	1,299.00
Front End Loader with 2 weights	4,189.00
48" Slip Scoop, Hydraulic	1,225.00
48" Snow Blower, hydraulic chute, Model SB448	3,840.00
Electric Chute Deflector for SB448	<u>249.00</u>
Stump Cutter	<u>2,820.00</u>
54" Rotary sweeper with gauge wheels standard, Model RS350	<u>3,118.00</u>
48" Rototiller, Model TL348	<u>2,619.00</u>
Light Kit	600.00

Take 5% off of the above options per CT State Contract #10PSX0307

* If all above items are purchased, if only part of this proposal is purchased please contact me for a revised freight price

We Propose hereby to furnish the above - complete in accordance with the above specifications, for the sum of: :

Payment to be made as follows: Net 30 days

All material is guaranteed to be as specified. All work is to be completed in a workman like manner according to standard practices. Any alteration or deviation from above specifications involving extra costs will be executed only on written orders, and will become an extra charge over and above the estimate. All agreements contingent upon strikes, accidents or delays beyond our control. Owner shall carry fire and other necessary insurance. Our workers are fully covered by Workmen's compensation insurance. jd

Authorized
Signature

GARRY RIENDEAU

Acceptance of Proposal— The above prices, specifications and conditions are satisfactory and are hereby accepted. You are authorized to do the work as specified. Payment will be made as outlined above. Date of Acceptance

Signature
Signature



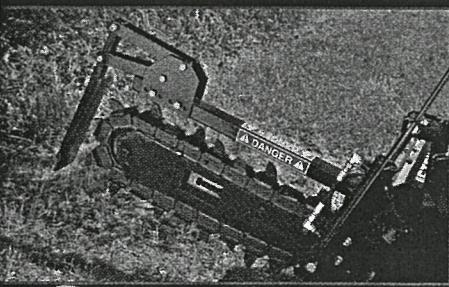
◀ CORE AERATOR

Aerate turf for better health with our easy-to-use core aerator. Independent $\frac{3}{4}$ -inch tine assemblies provide 6 $\frac{1}{2}$ -inch by 7-inch coring pattern. To aerate deeper, just add more weight to the weight box.



◀ SOIL TILLER

Preparing the soil for planting is quick and easy with our tough tiller attachment. Tilling soil to a depth of 6 inches is ideal for planting beds. The 48-inch swath helps finish the job quickly. The durable chain-drive rotor is made with a solid 1 $\frac{1}{2}$ -inch steel shaft and holds 24 cutting tines.



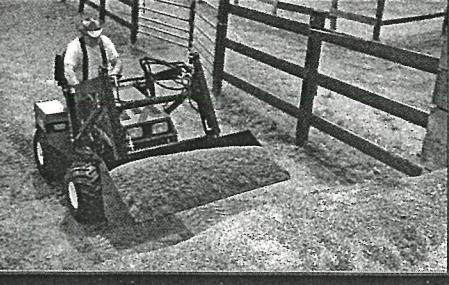
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◀ FRONT LOADER

Load and lift material with the tough front loader. The bucket is a productive 48-inches wide and holds up to 6 cubic feet of material. The loader has a self-contained hydraulic pump and a dedicated oil reservoir for power enough to transport up to 200 pounds of sand, soil, gravel, or manure.



◀ STUMP CUTTER

This tough stump cutter features a durable 16-inch diamond cutter head. A PTO belt driven off of a right angle gearbox powers the cutter head for reliable performance. Hardened cutters are specifically located to let you grind both side-to-side and from the top down.

Specifications on page 14

ATTACHMENT SPECIFICATIONS

SITE PREPARATION & TURF MAINTENANCE ATTACHMENTS

► STUMP CUTTER

Overall width: 34-1/2"
Overall height: 18"
Overall length: 37"
Cutterhead: 24 hardened cutters
Cutterhead diameter: 16"
Cutterhead width: 3-1/4"
Cutterhead speed: 1600 rpm
Cutting depth below ground level: 5"
Maximum recommended cutting height above ground level: 5"
Weight: 250 lbs
Fits the 430 only

► SLIP SCOOP

Overall width: 53-1/4"
Overall height: 16"
Overall length: 33"
Width of bucket: 48"
Bucket capacity: 4.5 cu ft
Dumping: Hydraulic
Ground clearance (transport): 12-1/2" on Model 430; 11-3/4" on Model 410
Ground clearance (dumped): 7-1/4" on Model 430; 6-1/2" on Model 410
Scarifier teeth: 6-1" wide
Weight: 150 lbs

► LOADER

Lift height: 73"
Maximum lift capacity: 365 lbs
Maximum transport lift capacity: 200 lbs
Reach: Raised: 30"; Lowered: 27"
Bucket volume: (approx.) 6 cu ft
Bucket width: 48"
Weight: 702 lbs
Weightbar required
Fits the 430 only

► TILLER

Working width: 48"
Working depth: 6" maximum
Overall height: 29"
Overall width: 54"
Overall length: 38"
Tines: 24 replaceable, hardened
Drive: Chain drive from gearbox
Operation: Tractor in reverse motion
Bearings: Greaseable
Weight: 305 lbs

► TRENCHER

Overall width: 37"
Overall length: 5' 4"
Overall height: 34"
Auger diameter: 12"
Chain speed: 550 ft/minute
Drive shaft diameter: 1-1/2"
Cutting depth: 28"
Cutting width: 5-1/2", standard
Weight: 360 lbs

► CORE AERATOR

Overall width: 45-1/2"
Working width: 40-1/4"
Overall height: 28-1/2"
Overall length: 30"
Main shaft bearings: Greasable 1-1/4"
Aerating wheels: 7 steel with bushings/ pressurized lubrication fitting
Tines: 6 per wheel, 3/4" core tines
Aerating width (actual): 39"; (effective): 45-1/2"
Aerating depth: Up to 3-1/2"
Aerating pattern: 6-1/2" x 7" (3.6 holes sq/ft)
Weight: 390 lbs with 4 weights

► SIDE DISCHARGE ROTARY MOWER MODEL MD448

Overall width with discharge chute: 60-1/2"
Cutting width: 47"
Cutting height: 1" to 4-1/2"
Spindle drive: Single "V" belt
Spindles: Tapered roller assemblies
Blades: 3 blades, 16-1/4"
Discharge: Side
Wheels: 10 x 3.5 semi-pneumatic
Weight: 330 lbs
Drive shaft to PTO belt

► SIDE DISCHARGE ROTARY MOWER MODEL MD460

Overall width with discharge chute: 73"
Cutting width: 59-3/8"
Cutting height: 1" to 4-1/2"
Spindle drive: Single "V" belt
Spindles: Tapered roller assemblies
Blades: 3 blades, 20-1/2"
Discharge: Side
Wheels: 10 x 3.5 semi-pneumatic
Weight: 360 lbs
Drive shaft to PTO belt

► SIDE DISCHARGE ROTARY MOWER MODEL MD472

Overall width with discharge chute: 85"
Cutting width: 72"
Cutting height: 1" to 4"
Spindle drive: Single "V" belt
Spindles: Tapered roller assemblies
Blades: 3 blades, 24-3/4"
Discharge: Side
Wheels: 10 x 3.5 semi-pneumatic
Weight: 470 lbs
Drive shaft to PTO belt

► REAR DISCHARGE ROTARY MOWER DECK - MODEL MX460

Overall width: 60-3/4"
Cutting width: 60"
Cutting height: 1-1/2" to 4"
Spindle drive: Single "V" belt
Spindles: Tapered roller assemblies
Wheels: 10.25 x 3.25 semi-pneumatic
Blades: 3 blades, 20-3/4"
Weight: Rear discharge: 380 lbs; Mulching option: 400 lbs
Drive shaft to PTO belt

► TREE FARM MOWER

Overall width: 45-1/2"
Cutting width: 44"
Overall length: 79" including wheel shields
Cutting height: 2" to 5"
Spindle drive: Gearbox and 2 V-belts
Spindles: 1" ball bearings
Blades: 2
Discharge: Rear
Weight: 272 lbs
Drive shaft to PTO belt

► ROUGH CUT MOWER

Overall width: 70"
Caster wheels: 13x5.00-6
Mower drive: PTO belt
Spindles: Cast iron housings, ball bearings
Cutting width: 66", offset 16" to right
Cutting height: 3" - 5-1/2"
Blades: Replaceable breakaway
Discharge: Rear
Weight: 550 lbs



Waterford Recreation and Parks Commission

TO: Rudie Beers, Finance Director
FROM: Ryan McNamara, Assistant Director
DATE: November 1, 2011
RE: Fleet Management Variance

A review has been conducted of the most recently approved First Selectman's Fleet Management Plan for FY 2012 – 2016. In previous plans there was equipment identified for non-replacement in concert with the current usage and operations. As most are aware, the Maintenance Division of R&P saw five (5) employees take advantage of the Retirement Incentive Program in June of 2011. This resulted in the contracting of the BOE grounds as well as an overall decrease in the recommended staffing. These two variables has provided some changes in the scope of work being requested and the best practices to efficiently and effectively complete these requests.

The issue of decreased staffing will lead most of this request for a variance. The snow removal responsibilities did not decrease in comparison to the lessened landscaping requirements at the BOE grounds. The R&P Maintenance division is responsible for approximately 15,000 sf of sidewalks or three (3) miles. This figure does not include snow removal from stairwells and landings. Responsibilities include sidewalks and parking lots for:

- Town Hall
- Emergency Services
- Police Department
- Youth Services
- Community Center
- Friendship School
- Leary Park
- Jordan Parkhouse
- Library
- O'Neill Theater
- Civic Triangle Park
- Waterford Beach Park
- Mago Point Park
- Dedrick Park
- Other Town areas as assigned

CAPITAL IMPROVEMENT PLAN: PROJECT CONSOLIDATION FORM-FY2013-2017**DEPARTMENT/AGENCY:FLOOD & EROSION CONTROL BOARD**

PROJECT NAME: IN ORDER OF DEPT. PRIORITY	S O U R C E						TOTAL FY 2013-2017
		FY-2013	FY-2014	FY-2015	FY-2016	FY-2017	
1 ALEWIFE COVE MAINTENANCE DREDGING	4			\$100,000			\$100,000
2							
3							
4							
5							
6							
7							
8							
9							
TOTAL							

Town of Waterford
Flood and Erosion Control Board
5-Year Capital Improvement Plan

The Flood and Erosion Control Board is attempting to secure a long-term maintenance permit from the State DEP in order to maintain and correct the sediment infill/erosion at the mouth of the Alewife Cove. Alewife Cove was originally dredged in 1987. The Cove has experienced some maintenance in the form of sand removal from the inside of the Ocean Beach jetty. This occurred twice within the first five (5) years of dredging. Eddy currents experienced from the outgoing tide have resulted in significant erosion at the base of the New London jetty and channel infilling has occurred in this area. Because of this serious problem, maintenance dredging had to be curtailed.

Part A of the solution to this problem is to secure a long-term maintenance permit for periodic (yearly) dredging behind the Ocean Beach jetty in conjunction with a dune replenishment project (see attached map). This project will be completed with City and Town equipment with a small engineering fee (\$2,000/yr) to document the sediment removed. This is a joint City/Town funded project.

Part B of the project is larger and somewhat more costly. This entails some channel dredging in the area of the jetty and stabilization of the bank behind the jetty. Engineering work will be required and it is possible that some of the hands-on work can be completed by the Town and City. Part B cannot be successful unless Part A has been permitted and in place. Therefore, the Board is requesting this project be funded for fall of 2011, i.e. FY 2011-2012. Part B will also be completed on a 50/50 basis with the City.

David Benvenuti, FECB Chairman
12/21/09
Attachment



GERWICK MEREEEN

CIVIL ENGINEERING & LAND SURVEYING

September 14, 2009

17 Industrial Road

P.O. Box 539

Waterford, CT 06385

Mr. David Benvenuti
Flood and Erosion Control Board
Town of Waterford
15 Rope Ferry Road
Waterford, CT 06385

Tel: 860.442.2201
Fax: 860.442.2205

RE: Beach Dredge Proposal
Alewife Cove
Job Number 01-145

Dear Mr. Benvenuti,

As per our recent discussion Gerwick Mereen LLC is pleased to submit the following proposal for work associated with Alewife Cove and the Jetty located on the eastern side of the Alewife channel at the western portion of Ocean Beach. The following scope is intended to be used in conjunction with a DEP COP application for Maintenance Beach Dredging in the area adjacent to the eastern portion of the jetty.

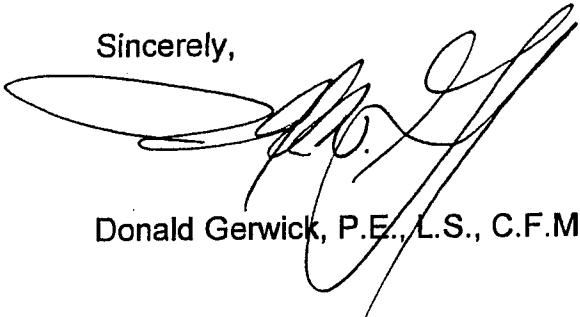
1. The work shall consist of cross sections every 25 feet, from the Jetty eastward to the approximate end of the nearby pavilion; cross sections shall extend from a depth of approximately 3 feet deep at MLW landward to vegetation or the area adjacent to the pavilion (please see attachment).
2. A schematic plan shall be prepared consistent with CT DEP format showing the area to be impacted, including an area of beach dune grass enhancement, and stockpile and disposal areas within the beach area.
3. At time of work, supply control stake for the contractor, rerun cross sections and prepare a schematic of completed conditions.

All work shall be tied into CT Grid coordinates and NGVD 88.

The proposed cost for the above is \$3,000.00. In addition, the cost of future work with regards to items 1 and 3, plus an update of item 2 is for the sum of \$2,000.00 each time the Board wants to replicate those items for future work, for a period not to exceed 5 years from the date of this proposal.

Thank you for your consideration.

Sincerely,



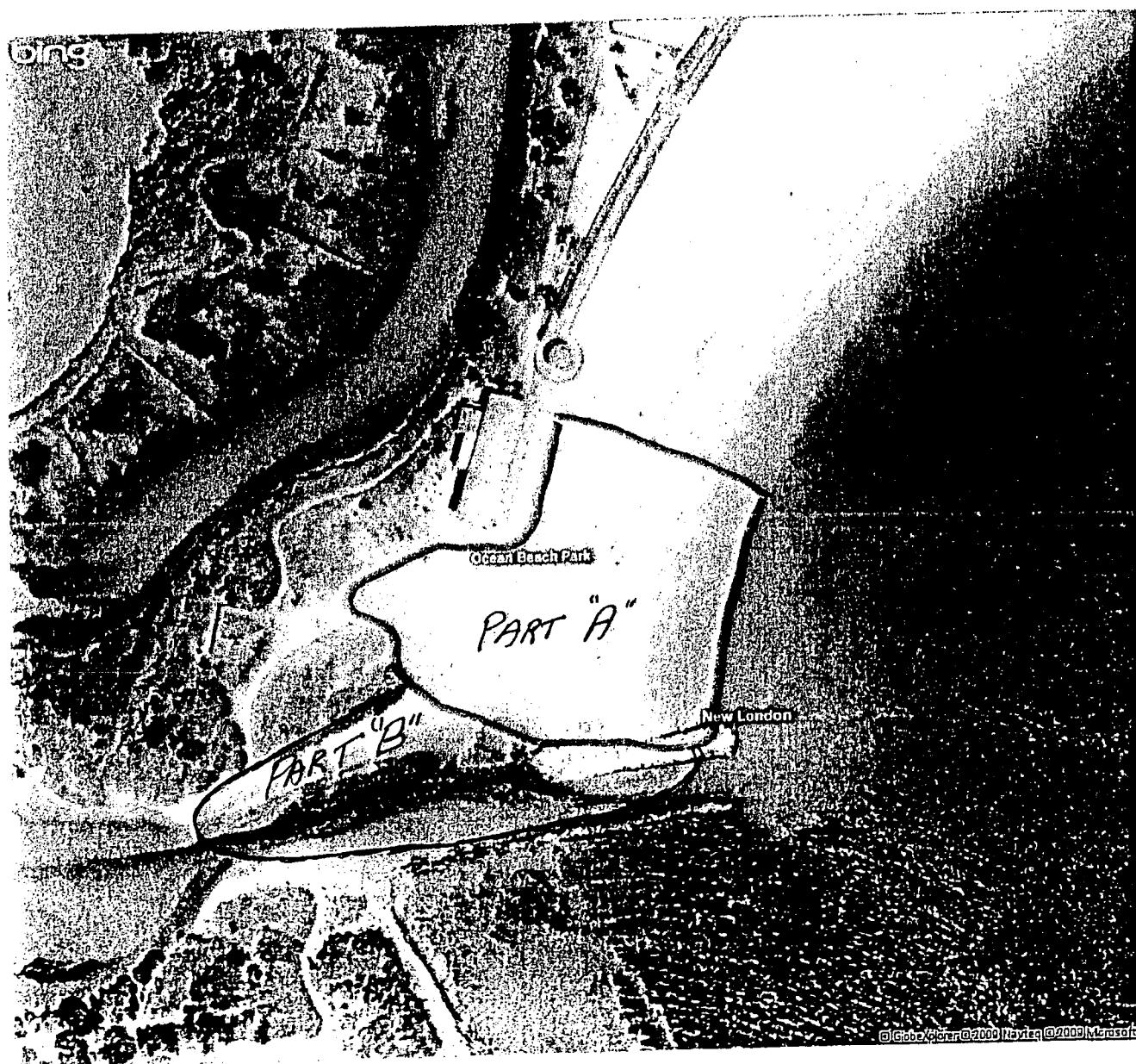
Donald Gerwick, P.E., L.S., C.F.M.

Print - Maps

Bing Maps

My Notes

FREE! Use Bing 411 to find movies,
businesses & more! 800-BING-411



CAPITAL IMPROVEMENT PROGRAM

PROJECT CONSOLIDATION FORM

DEPARTMENT/AGENCY: Information Technology Committee

<u>PROJECT NAME</u>	<u>FUNDING SOURCE</u>						<u>TOTAL</u> <u>FY 2013-17</u>
		<u>FY 2013</u>	<u>FY 2014</u>	<u>FY 2015</u>	<u>FY 2016</u>	<u>FY 2017</u>	
1. Computer to Plate System - Print Shop	1	\$ 8,500.00					\$ 8,500.00
2. Staffing & Scheduling Program - Police	1	\$ 28,750.00					\$ 28,750.00
3. Folder/Inserter - Print Shop	4		\$ 11,900.00				\$ 11,900.00
4. Audio Visual Upgrade - Auditorium	4		\$ 27,374.00				\$ 27,374.00
5. Town-wide Telecommunication Upgrade	4			\$ 63,594.00			\$ 63,594.00
6. Web-based Reporting Software - Police	4				\$ 19,250.00		\$ 19,250.00
							\$ -
							\$ -
							\$ -
							\$ -
							\$ -
TOTAL		\$ 37,250.00	\$ 39,274.00	\$ 63,594.00	\$ 19,250.00	\$ -	\$ 159,368.00
*SOURCE OF FUNDS							
CURRENT YEAR CAPITAL IMPROVEMENTS		1	OPERATING BUDGETS				2
WASTE WATER BUDGET		3	TRANSFER TO CAPITAL & NONRECURRING EXPENDITURE FUND				4
ALTERNATIVE FINANCING SOURCE(S)		5	LOCIP				6
DESIGNATE FUNDS FROM UNDESIGANTED PROJECTS CNR		7					

CAPITAL IMPROVEMENT PLAN
PROJECT DESCRIPTION FORM

A) AGENCY: Information Technology Committee

B) PROJECT NAME: Computer to Plate System – Print Shop

C) CONTACT PERSON: Rudie Beers

D) DEPARTMENT'S PRIORITY: #1 – Based Upon Previously Committed Funds

E) DESCRIPTION:

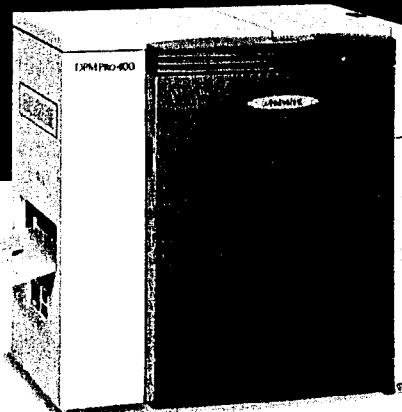
- a) Describe the type, purpose, and anticipated accomplishments of the project.
- b) Indicate the progress to date on the project.
- c) List other projects it is coordinated with whether in your Department or with other Departments.
- d) Describe the affect the project will have on your annual Department expenditures:
- e) Attach plan, estimate, service area map and/or other support documentation.

An appropriation of \$23,000 was funded in FY12 for this purpose. Subsequent to the appropriation and upon completion of a site survey, it was discovered that the Presstek AB Dick Digital Plate Master Computer would require the Town to retro-fit the printer at a cost that will exceed the appropriation. The Purchasing Agent has been made aware of new technology that was not available at the time of the original appropriation request that requires fewer chemicals and therefore less expensive to operate. This machine is a DPM Pro 400 Automated Polyester Plate CTP System and would cost \$31,500.00. Although the difference of \$8,500 does not strictly meet the dollar level requirement to be included in the CIP, this purchase complements a prior capital request and therefore, should be considered.

DPM Pro 400

Automated Polyester Plate CTP System

- Excellent solution for small format presses
- High speed plate production
- Low cost of ownership
- Easy to use, fully automated
- High resolution output
- Low chemistry use
- Compact size



PRESSTEK
A SMARTER WAY TO PRINT

DPM Pro 400

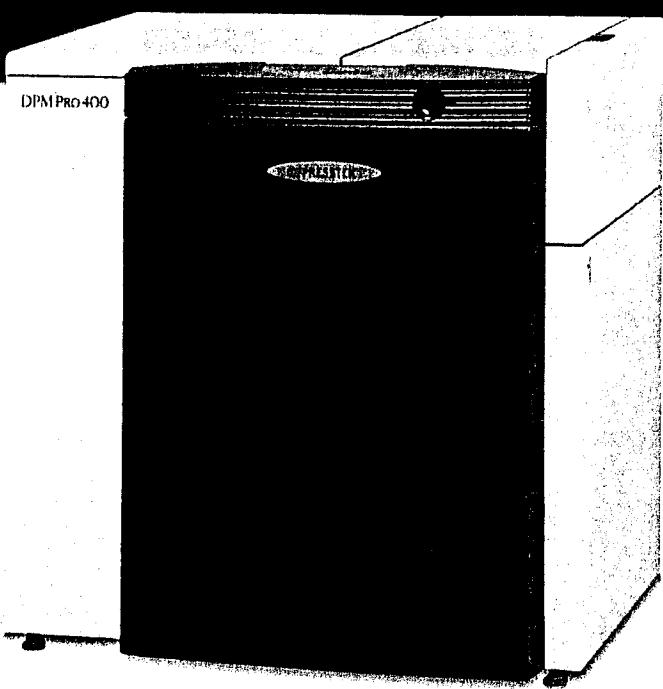
Increase Productivity and Turnaround

The Presstek DPM Pro 400 is an easy-to-use, fully automated, high resolution polyester plate CTP system. It is designed for use with small-format presses and supports plate widths up to 16.31" (414 mm). The DPM Pro 400 is faster, more economical to use, more environmentally-friendly, and more compact than comparable choices.

The DPM Pro 400 features an advanced internal plate processor. Its automation, speed, and low cost of operation will help make your business more capable, profitable and competitive.

DPM Pro 400 Features:

- Fully automated digital system
- Produces polyester and paper-based plates
- Minimal operator training and intervention
- High resolution output up to 2400 dpi
- Up to 78 plates per hour
- Low chemistry consumption
- Ideal CTP solution for small presses
- Versatile and well suited for a wide range of printing applications
- Compact size and daylight operation



Speed Up Production and Deliver High Quality Output

Digital platemaking with a DPM Pro 400 makes it possible to handle more jobs with increased efficiency and precision. There are no intermediate steps required, so process variables in platemaking are significantly reduced.

The DPM Pro 400 requires little time to produce press-ready plates. Simply load the MEGAPRO material and the DPM Pro 400 operates with a minimum of operator intervention. The DPM Pro 400 receives 1-bit TIFF files from your RIP, then images, processes and sizes your plates, all in one streamlined automatic operation.

The DPM Pro 400's processor is built-in, self-contained and highly efficient. Presstek MEGAPRO plates and MEGAPRO LC chemistry work as an optimized system to deliver extremely stable, repeatable and accurate plate development.

An integrated cutter sizes plates precisely, so there's no wasted plate material. You go directly from desktop to press-ready plates in one automated procedure. An optional punching system completes your CTP workflow.

Be More Capable, Profitable and Competitive

Because it is fully automated, the DPM Pro 400 becomes an integral part of your digital workflow, freeing prepress personnel to perform other tasks and work more productively. Simplified platemaking, fast turnaround time, and high throughput of plates enable you to process more jobs and better respond to short lead times.

DPM PRO 400

DPM Pro 400 Benefits:

High Performance: The DPM Pro 400 can produce up to 78 plates per hour at 1200 dpi. You can increase resolution to 1500, 1800 or 2400 dpi to fit individual job requirements. With a range of resolution choices, the DPM Pro 400 covers a wide range of applications, from forms and stationery to complex four-color work.

Versatile Formats: Plate widths range from 9" to 16.31", with lengths running from 8.65" to 22.83". Plate sizes are ideally suited for two-up presses and duplicators.

Easy Operation: No special operating conditions or environment are required. Plate rolls are loaded in daylight conditions. The automated internal processor requires only periodic replenishment of chemistry. The very small footprint requires minimal space and fits almost any prepress environment.

Minimal Chemistry: The DPM Pro 400's unique processing technology provides extremely stable plate development. It does so by applying only a thin layer of chemistry to the plate surface to complete the image development process. This lowers your cost by reducing the chemistry consumption and disposal that are required by most other dip tank processor systems, and saves time by minimizing maintenance.

Connectivity

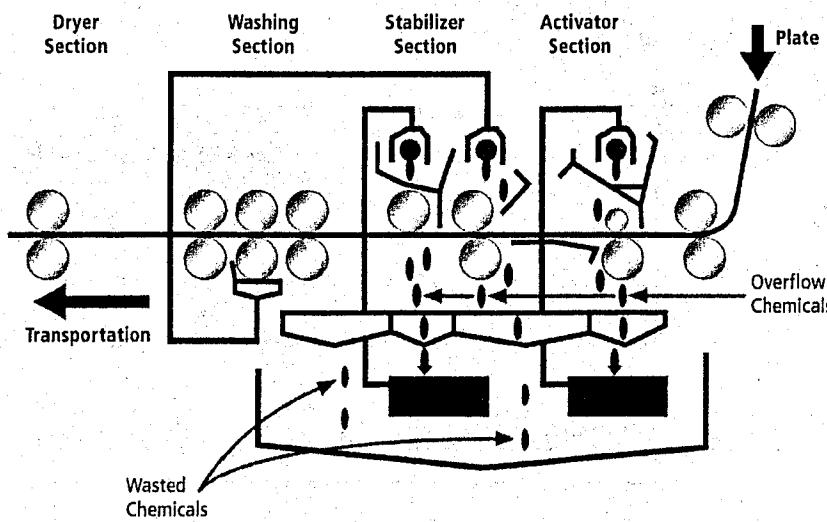
The DPM Pro 400 is compatible with a range of industry standard RIPs and workflows. Presstek's integrated digital solutions include workflow solutions that optimize the performance of your Presstek equipment.

MEGAPRO Plates

The DPM Pro 400 uses Presstek's MEGAPRO plate line, including polyester and paper-based plates. Plates, activator, stabilizer and fountain solutions are used together as a system to achieve optimum results.

MEGAPRO polyester plates have a high resolution emulsion capable of producing high contrast, sharp images. They can run on virtually all offset presses and are compatible with a wide variety of dampening systems and inks.

MEGAPRO 5 mil and 8 mil polyester plates support run lengths up to 25,000 impressions. Lower cost 7 mil MEGAPRO paper-based plates support run lengths up to 10,000 impressions.



The integrated processor offers both economical and environmental benefits. The system operates with two ready-to-use solutions for processing spooled MEGAPRO plates. The unique processor design uses two-thirds less chemistry than most other systems.

Presstek DPM Pro 400 Specifications

Recording Technology	Capstan
Light Source	Red laser diode, 635 nm
Media Type	MEGAPRO 5 mil and 8 mil rolled polyester plates for runs up to 25,000 impressions* MEGAPRO 7 mil rolled paper-based plates for runs up to 10,000 impressions*
Plate Width	9" to 16.31" (229 mm to 414 mm)
Plate Length	8.65" to 22.83" (220 mm to 580 mm)
Recording Width	Maximum 15.9" (404 mm)
Resolution	1200, 1500, 1800 and 2400 dpi
Screen Ruling	175 line screen recommended maximum
Recording Speed	48" (1236 mm)/minute at 1200 dpi 31" (791 mm)/minute at 1500 dpi 22" (552 mm)/minute at 1800 dpi 12" (309 mm)/minute at 2400 dpi
Output Speed	First plate 114 sec. 78 plates/hour at 1200 dpi 69 plates/hour at 1500 dpi 53 plates/hour at 1800 dpi 29 plates/hour at 2400 dpi
Interface	Fast-PIF (16 bit)
RIP (not included)	Presstek Momentum RIP, Presstek Momentum Pro RIP/Workflow or other industry standard workflow
Processor	Internal, automated
Tank Capacity	Main tank: .145 gal (0.55 liter) .528 gal (2 liter) Activator .159 gal (0.60 liter) .528 gal (2 liter) Stabilizer .068 gal (0.26 liter) .264 gal (1 liter) Wash-off water
Physical Specifications	
Power Requirements	100/120 VAC, 15 AMP, 50/60 Hz
Dimensions (W×D×H)	39.375" × 31.31" × 42.125" (1000 mm × 795 mm × 1070 mm)
Weight	456 lb. (207 kg)
Operating Conditions	73.4° F ± 9° F (23° C ± 5° C) and 50–70% relative humidity
Options	
Punching	• Integrated converting punching unit / Beil or DS system • External Beil Punching Unit

* Actual numbers will be affected by ink and paper conditions.

Specifications are subject to change.

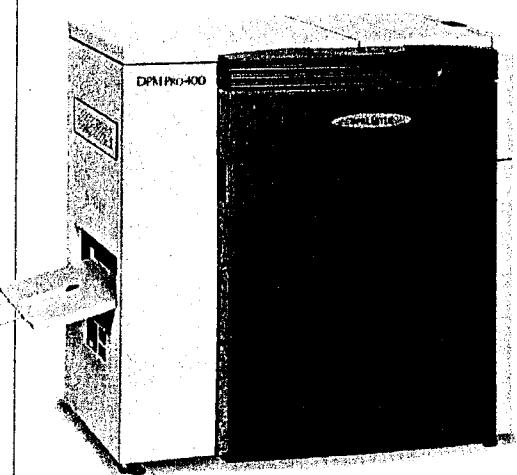
Purchase MEGAPRO plates, MEGAPRO LC chemistry and
other printing supplies at shop.presstek.com

Printed on a Presstek DI® digital offset press on Chorus Art 100lb. Silk Cover. Chorus Art stock contains 50 percent recycled fiber, including 25 percent post-consumer waste, and is Forest Stewardship Council certified.



Service and Support

Presstek provides an integrated support network of field engineers, customer care representatives, and technical support engineers—all supported by advanced information technology systems. Expert installation, training and maintenance supports your investment so it will pay dividends quickly and reliably for years to come.



For more information about
Presstek Products:

Presstek, Inc.

55 Executive Drive
Hudson, NH 03051 USA
Tel: 603-595-7000
www.presstek.com



QUOTATION FOR:

Kate Rotella

Mike Bargnesi

Town of Waterford

15 Rope Ferry Rd.

Waterford, CT. 06385

860-440-0540

PressTek

55 Executive Dr.

Hudson, NH. 03051-4903

Traditional System Specialist

Phone 520-232-3529

Fax 603-546-4285

kosmus@presstek.com

Kim Steven Osmus

Langford Meredith

24-Oct-11

We are pleased to submit the following quotation for your consideration:

PRINT 2011 SPECIAL PROGRAM

MODEL	DESCRIPTION	QUANTITY	PRICE
DPMPro400	<p>Presstek Digital PlateMaster - Computer-to-Plate</p> <p>Includes: Computer, Rip, & Training</p> <p>STANDARD FEATURES DPMPro400 / DPM34HSC</p> <p>Highest Resolution 2400dpi - 2400dpi</p> <p>Plate Cost \$263.00 \$263.00</p> <p>Speed/ Plates per Hour 78 40</p> <p>* Processor Spray Tank</p> <p>* Power 120Volt 220Volt</p> <p>* Foot Print 39.4 X 31 X 42 54 X45 X 30</p> <p>* Plate Width 9" to 16.3 13",13.2, 13.3</p> <p>* Warranty One Year 90 Days</p> <p>* Chemicals - Based on two Cases Activator One Case Stabilizer Per 45 Days = \$180.00 DPMPro400 uses 50% less \$90 X 60 Cleanings = {\$5400 } = Saving over 7.5 years</p> <p>*Advance DPMPro400 for Town of Waterford</p> <p>Payment Terms - 50% Prior to shipment & 50% Net 30 Days After Install</p>	1	\$30,000.00
		Shipping	\$1,500.00
		Sub Total	\$31,500.00
		Applicable taxes	
		Total Purchase	\$31,500.00

PressTek offers a variety of Lease options

Lease Company	Type	Term Months	Purchase Option	Advance Payments	Monthly Cost
Leasing	Commercial	60	\$1.00	0	\$664.97
60 months	Commercial				
Zero Down	Commercial				
\$1.00 purchase option					
Subject to app	Commercial				
By Leasing Co	Commercial				
	Commercial				

Delivery: 30-45 Working Days	F.O.B. Destination	Quotation Valid Through 11/31/2011
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CAPITAL IMPROVEMENT PLAN
PROJECT DESCRIPTION FORM

A) AGENCY: Information Technology Committee

B) PROJECT NAME: Telestaff Staffing & Scheduling Program (Police)

C) CONTACT PERSON: Rudie Beers

D) DEPARTMENT'S PRIORITY: #2

E) DESCRIPTION:

- a) Describe the type, purpose, and anticipated accomplishments of the project.
- b) Indicate the progress to date on the project.
- c) List other projects it is coordinated with whether in your Department or with other Departments.
- d) Describe the affect the project will have on your annual Department expenditures:
- e) Attach plan, estimate, service area map and/or other support documentation.

This request is for the purchase of new staffing and scheduling software for the Police Department. This request will allow the Police Department to meet service level demands, maintain officer safety and adhere to budgetary requirements. This program will automatically and telephonically make notifications to a pre-programmed group of officers until necessary shift vacancies are filled. If the vacancy cannot be filled from the list, the system will apply the mandate rules and select an officer to fill the shift upon supervisor approval. The Department feels this software will enhance its ability to maintain adequate patrol staffing in an efficient manner by allowing shift supervisors to devote their time to more pressing supervisory matters.

5. **Product Components.** The following constitutes a list of TELESTAFF components and associated fees that CUSTOMER does hereby agree to purchase from PDSI available in connection with the use of TELESTAFF. See Appendix A for the recommended TELESTAFF hardware specifications.

Initial	Component	Fee
	TELESTAFF Enterprise Software License for 50 POLICE Staff Members	\$ 7,000
	TELESTAFF Implementation Services for 50 POLICE Staff Members	\$ 12,000
	WEBSTAFF - PDSI Hosted Setup and First Year Service for 50 POLICE Staff Members	\$ 2,500
	Dongles (4) Qty: 1	\$.750
	Concurrent Database Connection Qty: 4	\$ 500
	4 port Dialogic Card Qty: 1	\$ 1,000
	TeleStaff Gateway Qty: 1	\$ 5,000
	Total Initial Acquisition Cost	\$ 28,750
	Optional: Auctions Module	\$ 2,500

6. **Pricing.** PDSI agrees to the fees reflected above in paragraph 5 for 90 days after 11/25/2008 or until 2/23/2009.
7. **Payments.** CUSTOMER agrees that all pricing and amounts due hereunder are based on United States currency and that all amounts remaining unpaid for more than thirty (30) days following the date of the invoice shall be subject to an interest charge at the monthly rate of 1.5%. CUSTOMER does hereby agree to the payment terms for each component.

Initial	Component	Due
	TELESTAFF Enterprise Software License and applicable Sales Tax for 50 POLICE Staff Members	Due Upon Receipt
	TELESTAFF Implementation Services for 50 POLICE Staff Members	Due Upon Receipt
	WEBSTAFF Setup and First Year Service for 50 POLICE Staff Members	Net 30 Days from Execution Date
	Dongles (4) and applicable Sales Tax	Net 60 Days from Execution Date
	Concurrent Database Connection and applicable Sales Tax	Net 60 Days from Execution Date
	4 port Dialogic Card and applicable Sales Tax	Net 60 Days from Execution Date
	TeleStaff Gateway and applicable Sales Tax	Net 90 Days from Execution Date

8. **Travel Expenses.** CUSTOMER agrees to pay for all travel expenses related to TELESTAFF implementation and training services as defined in Appendix B.
9. **Implementation Services.** Implementation Services include configuration of TELESTAFF as defined in Appendix B. CUSTOMER acknowledges that training and/or reconfiguration requested by CUSTOMER in addition to that defined in Appendix B will be at an additional cost.
10. **Telephony Service.** CUSTOMER acknowledges that the telephony capabilities included in TELESTAFF are designed to be compatible with POTS analog phone service from a local phone services provider, and that PDSI warrants the correct operation of the TELESTAFF telephony components only when connected to POTS analog phone lines. Should CUSTOMER attempt to connect TELESTAFF to PBX or other digital phone services, PDSI will not warrant correct telephony behavior nor will provide support for CUSTOMER's unique telephony solution.
11. **Annual Service and Support.** Service and Support of TELESTAFF is provided at no additional charge during the first twelve (12) months following the Execution Date. See Appendix C for the definition of Service and Support. CUSTOMER does hereby acknowledge that on each anniversary of the Execution Date, CUSTOMER may renew service and support under the following events:

Initial	Event	Payment
	TELESTAFF for 50 POLICE Staff Members - 1st Anniversary of the Execution Date	\$ 1,540
	Subsequent anniversaries during term	3% maximum increase over previous period

Should CUSTOMER elect not to renew Service and Support on the anniversary of any Execution Date, CUSTOMER acknowledges that any subsequent re-enrollment for Service and Support will only be accepted by PDSI after CUSTOMER cures the previous lapse in Service and Support by paying PDSI the Service and Support fee for the lapsed periods. In addition, CUSTOMER acknowledges that PDSI may assess CUSTOMER a Service and Support re-instatement fee that will not exceed ten percent (10%) of the Service and Support fee for the lapsed periods. PDSI reserves the right to discontinue Service and Support of previous releases of TELESTAFF and WEBSTAFF as defined in Appendix C.

CAPITAL IMPROVEMENT PLAN
PROJECT DESCRIPTION FORM

- A) AGENCY:** Information Technology Committee
B) PROJECT NAME: Print Shop Folder/Inserter Machine
C) CONTACT PERSON: Rudie Beers
D) DEPARTMENT'S PRIORITY: #3

E) DESCRIPTION:

- a) Describe the type, purpose, and anticipated accomplishments of the project.
- b) Indicate the progress to date on the project.
- c) List other projects it is coordinated with whether in your Department or with other Departments.
- d) Describe the affect the project will have on your annual Department expenditures:
- e) Attach plan, estimate, service area map and/or other support documentation.

This request is for the purchase of equipment to replace the existing Print Shop Folder/Inserter Machine. The existing equipment is slowly deteriorating and the office has experienced over \$3,000 in repair costs during the last two years, including total replacement of the mother board. This piece of equipment is used primarily by the Utility Commission for quarterly use charge billings; the Tax Office for the Motor Vehicle Supplement bills and delinquent tax notices and the Board of Education for their bi-weekly payrolls and the High School mass mailings.

Kate Rotella

From: Kim Dibble [kdibble@neopostnewengland.com]
Sent: Wednesday, November 24, 2010 8:16 AM
To: Kate Rotella
Subject: Purchase for D2 62 2.5 Folder Inserter

Hi Kate,

The purchase price is:

DS 62 2.5 Folder Inserter Equipment: \$11,900

Annual Maintenance: \$1,190

Shipping and Handling: \$181

Thanks,

Kim

Kim Dibble
Named Account Rep

Neopost New England

Metals Drive

Bethel, CT 06489

C 860.462.3220

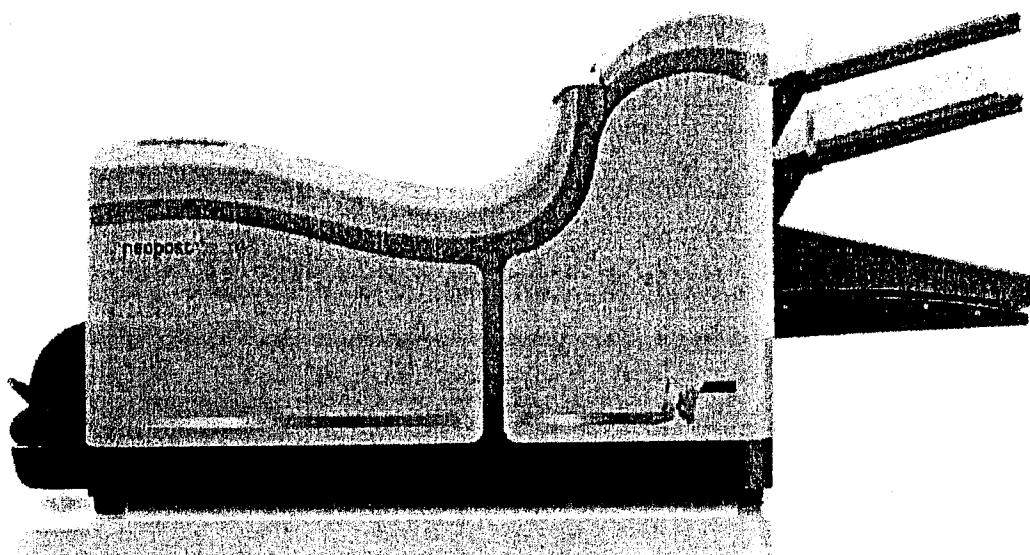
P 860.276.0276 x2147

F 203.632.1010

www.neopostnewengland.com

neopost
new england

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DS-62

YOUR OFFICE

ASSISTANT

The versatile folder inserter to quietly fulfill all mailing needs

WE VALUE YOUR MAIL

neopost

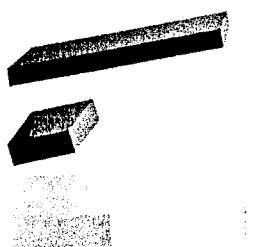
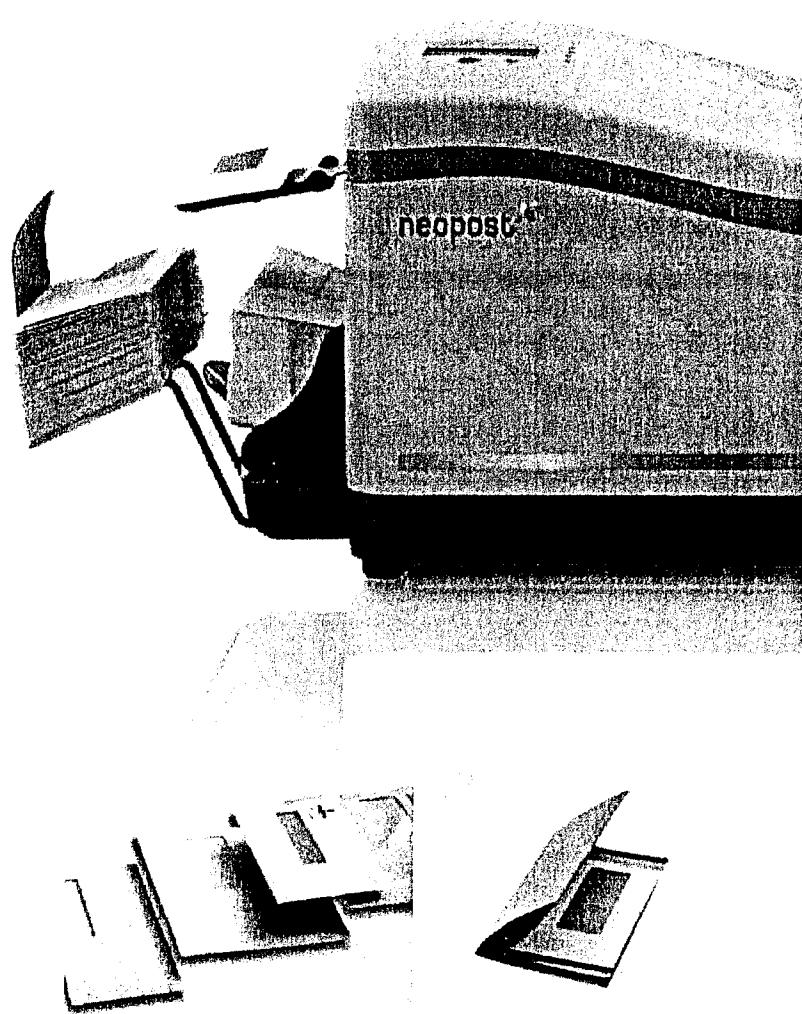
DS-62

YOUR OFFICE ASSISTANT

Remember the time when you had to put your mail together by hand? Manually collating, folding, inserting, sealing and stacking your mail pieces is a time-consuming business.

There is a better way. The DS-62 folder inserter handles all your mail automatically. It saves you time and money while producing professional looking mail documents and best of all, it is also as easy to use as a copier.

The DS-62 allows you to process a wide variety of mail items for direct mail marketing campaigns, invoicing, monthly statements and much more. It will take care of virtually all of your company's mailing needs!



ONE SOLUTION FOR A WIDE VARIETY OF MAIL PIECES

Does your mail vary in paper size and paper quality? Does it consist of invoices, cards or reply envelopes? The DS-62 does it all for you.

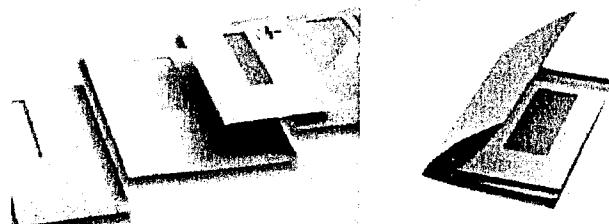
The unique flexFeed® feeders enable you to process any document size, including anything from full sheets down to 1/3 inserts from each flexFeeder, giving you total versatility.

CREATE YOUR OWN PROFESSIONAL LOOKING MAIL DOCUMENTS

Effective communication starts with a professional looking mail document. And with the DS-62, you don't even have to think about it. The DS-62 collates and assembles all documents before folding them. The result is that your customer receives an automatically collated professional package.

With powerFold™ you can fold up to 5 pages in tri-fold, neatly and silently.

The Daily Mail mode offers to process up to 5 sheets fed by hand, stapled or unstapled.



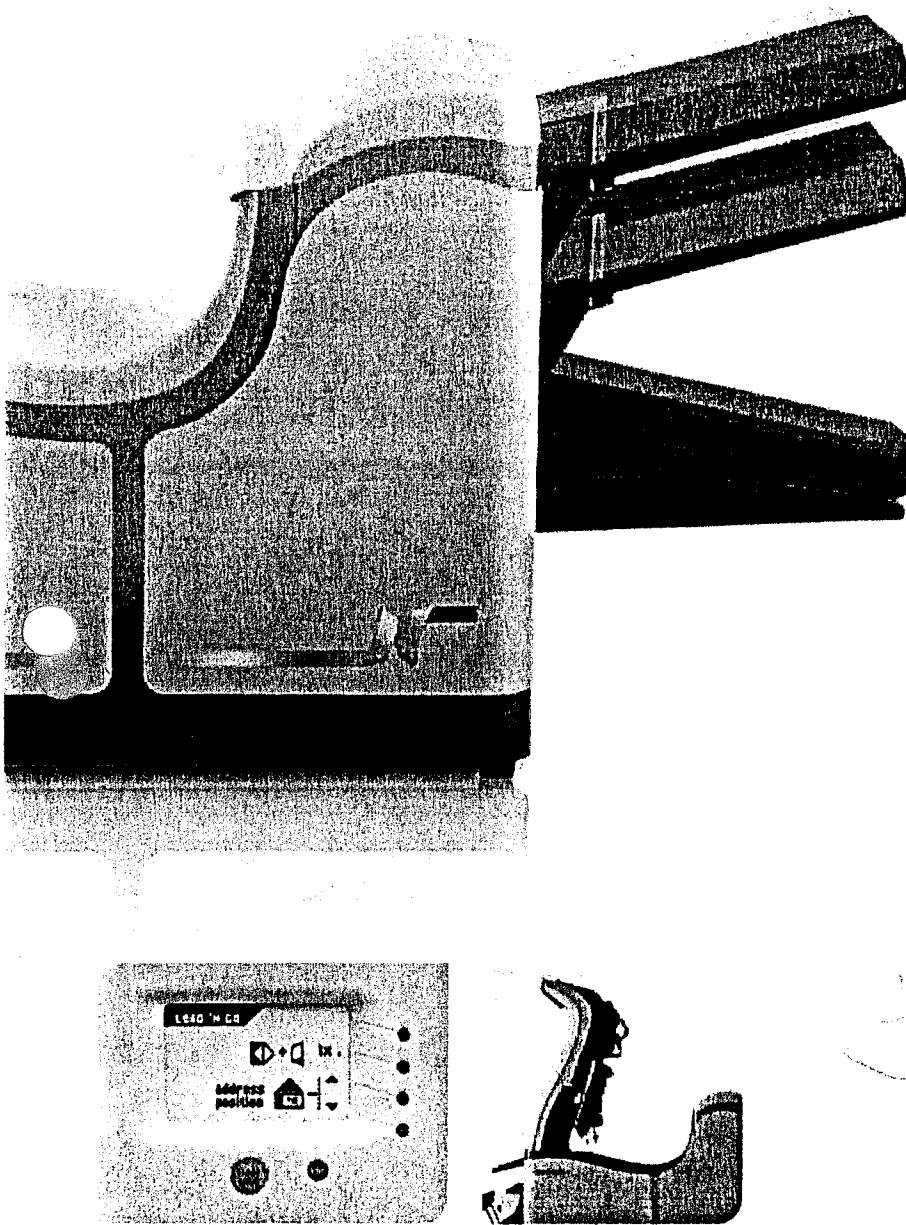
EASY TO USE

AUTOMATION WITH LOAD'N GO

LOW NOISE LEVEL

FULLY ACCESSIBLE – ERGONOMIC DESIGN

PROFESSIONAL LOOKING MAIL DOCUMENTS



AUTOMATICALLY ADAPTING TO YOUR NEEDS

Thanks to load'n Go®, there are no special skills needed to operate the DS-62. Just load documents and envelopes, press the button and Go! The DS-62 will adjust all settings automatically and save them in the memory for instant recall. Regular jobs? Just run load'n Go and store the settings in one of the 9 available jobs.

BENEFITS EVERY OFFICE ENVIRONMENT

With its low noise level, flexibility and ease-of-use, and compact design, the DS-62 is ideal for any office environment. In addition, you will have quick, safe and easy access to all parts of the paper path.

MAXIMIZE EFFECTIVENESS WITH PRINTMACHINE

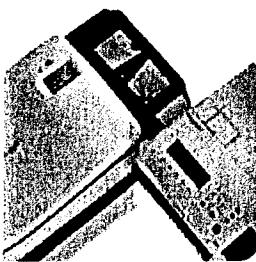
PrintMachine output management software streamlines your customer communications by automating the document management process. Create more professional and informative customer communications, deliver documents to customers with greater speed and accuracy, and reduce costs by grouping and consolidating customer documents while taking advantage of postal automation discount technologies.

OPTICAL MARK RECOGNITION TO ENSURE CORRECT DELIVERY

Using a sequence of marks, the DS-62 is able to securely process single or multiple page documents to ensure they reach the correct recipient.

DS-62

QUIET FOLDER INSERTER



TOTAL MAIL FLOW OPTIMIZATION – SEND MORE FOR LESS

Choose the most convenient way of stacking your filled envelopes to optimize productivity and space. *insert'n Mail™* allows you to automate your total mail flow by connecting the DS-62 to a Neopost mailing system.

CONTENT CONTROL AND SECURITY YOU CAN RELY ON

To ensure that every recipient gets the correct mail piece, Neopost's patented electro-mechanical thickness detection protects against double feeding. It is insensitive to paper dust and works with toner or colored paper.

FEATURES & BENEFITS

OPERATIONAL EFFICIENCY

Speed	Up to 2,200/hour
load'n Go® automatic settings	Yes
flexFeed® multi-format feeder	Yes
Multiple sheet feeding	Yes
Cascade from all feeder	Yes
Number of feeders	2 flexFeeders + 1 insert feeder
powerFold™ folding capacity	Yes (up to 5 sheets 20 lb.)
Job memory	9
Daily mail	Yes
Documents feeder capacity	325 sheets (20 lb.)
Envelopes feeder capacity	150
Fold types	c-fold, z-fold, single, double parallel, no-fold
Different exits	Optional catch tray, side exit
Documents height	3.5" – 14"
Documents width	5" – 9"
Documents weight	16 – 67 lb.
Envelopes length	3.5" – 6.4"
Envelopes width	6.3" – 9.8"
Set thickness	.08"

MAIL QUALITY & SECURITY

secure'n Feed™ double detection	Yes
Accumulate before folding	Yes
Optical Mark Recognition (OMR)	Optional
powerFold™	Yes (up to 5 sheets 20 lb.)
Tip to tip secure sealing	Yes

BUDGET OPTIMIZATION

PrintMachine software	Optional
insert'n Mail™ system integration	Optional

SYSTEM SPECIFICATIONS

BASE

Length x Height x Depth	47.2" x 20.5" x 16.5"
Weight	165 lbs.

WHY CHOOSE NEOPOST?

Neopost is a world leader in mailing and logistics solutions. Our innovative solutions bring simplicity and efficiency to your mailing process to make your business run better. Neopost brilliantbasics benefits provide excellence in all our offers, from products to support and services. They bring you the best in operational efficiency, mail quality and security, budget optimization and online management. Whether for advice or support, you enjoy our commitment to supply first-class service – on the phone, on site, or online. Benefit from immediate response times and remote diagnosis at our call centers, and fast dispatch of service engineers when needed. Find out more at www.neopost.com;brilliantbasics

neopost

1-888-NEOPOST (636-7678)

www.neopostinc.com

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All information in this document, including descriptions of features, functions, performance and specifications is subject to change without written notice at any time.

EAGLE Registrations Inc.
SERVICE • INTEGRITY • VALUE

ISO 14001 and OHSAS 18001
Registered by EAGLE Registrations Inc.

ROI and Benefits

- Eliminate costs for printing and mailing tax bills from QDS
- Reduced total postage costs to \$13,192 by posting each piece at 34¢
- Reduced returned tax bills allowing Town of Waterford the ability to get the tax bill to the resident the first time.
- Additional postage savings for monthly miscellaneous mail.

Pricing Proposal

The following is pricing for

- BulkMailer Pro which provides address correction and validation, bar coding for postage discounts
- DS 62 2.5 Folder Inserter
- IS 480 Mailing Machine
- IS 5000 Mailing Machine

DS 62 2.5 Folder Inserter

60 month lease: \$278.77 per month

Includes Maintenance

BulkMailer Pro Software

60 month lease: \$103.94 per month

IS480 with a 10 lb. Scale

60 month lease: \$401.45 per month

Includes Electronic Return Receipt and Bar Code Scanner

IS480 with a 10 lb. Dynamic Scale

60 month lease: \$465.27 per month

Includes Electronic Return Receipt and Bar Code Scanner

IS 5000 with a 10 lb. Scale

60 month lease: \$504.35 per month

CAPITAL IMPROVEMENT PLAN
PROJECT DESCRIPTION FORM

- A) AGENCY:** Information Technology Committee
B) PROJECT NAME: Audio/Visual Upgrade Town Hall Auditorium
C) CONTACT PERSON: Rudie Beers
D) DEPARTMENT'S PRIORITY: #4

E) DESCRIPTION:

- a) Describe the type, purpose, and anticipated accomplishments of the project.
- b) Indicate the progress to date on the project.
- c) List other projects it is coordinated with whether in your Department or with other Departments.
- d) Describe the affect the project will have on your annual Department expenditures:
- e) Attach plan, estimate, service area map and/or other support documentation.

This request is for the purchase of equipment to replace the existing audio/visual network in the Town Hall Auditorium. The existing system is outmoded. The need for various cabling presents an on-going safety hazard. In addition, the existing video system does not interact well with new technology and often is ineffective in presenting information to the various boards and the public. The system outlined in the proposal would be wireless with flat screen monitors and a projection unit. The cost is presented for informational purposes only and will be bid in accordance with the Town's requirements when the project is ready to be moved forward.



COMMUNICATIONS INC.

Technology Solutions
For Audio-Visual
Communications,
Teleconferencing and
Professional Video

December 10, 2008

Rudie Beers
Director of Finance
Town of Waterford
15 Rope Ferry Rd
Waterford Ct. 06385

REF: Job # P-102182

Ms. Beers:

It is our pleasure to present this proposal for Town Hall Video and Audio upgrades to Waterford Town Hall for your review. The scope of work and pricing included in our proposal is based on our meeting on 11/21/08. Please use this proposal for budgetary purposes only. We can modify system based on your final requirements.

For over 60 years, HB Communications has been the Northeast's leading integrator of presentation, conferencing, and communications technology. Our 350 employees have a single mission: your complete satisfaction. Our industry certified engineers, licensed technicians, and factory trained service personnel are poised to deliver your project on time and on budget. HB is proud of the systems that we have installed and welcome the opportunity to demonstrate the quality of our workmanship by an on-site tour of a facility that is similar in scope.

We appreciate your consideration on this project and urge you to contact our references enclosed in this proposal. Upon review of the enclosed documents, please contact me with questions or concerns.

Sincerely,

Josh Robinson
Senior Account Executive
HB Communications, Inc.

Corporate Office
40 Dodge Avenue
North Haven, CT 06473-0869
Phone: 203.234.9246
Fax: 203.234.2813

Boston Office
1432 Mass Street
Wellesley, MA 02453
Phone: 781.858.6646
Fax: 781.858.6646

New York Office
177 Mill Lane
Mount Kisco, NY 06092
Phone: 914.634.0810
Fax: 914.634.9273

Sales & Mktg: 800.243.4414
Customer Service: 800.452.4441
www.hbcommunications.com

PROPOSAL



Project: Waterford Town Hall

Proposal # P-102182

FUNCTIONAL DESCRIPTION

Display

- HB Communication will wall mount two (2) 46" LCD monitors to the left and right of the meeting table in the front of the hall to allow board members to have viewing access to displayed material.
- HB Communications will also install two (2) 3000 ANSI Lumens DLP data projectors on the outside wall, left and right of center. These images will be large enough to accommodate viewing of all data from designated public sitting areas.

Projection Screen

- Two (2) manual pull-down projection screens are included in the system. The projected image will be 80" wide, 60" high, and will begin approximately 36" above the finished floor.

Playback/Program Audio

- Program audio from the computer (s), and owner furnished DVD/VCR combo player can be heard through ten (10) ceiling-mounted loudspeakers strategically placed for the best audio coverage possible in the given space.

Speech Reinforcement

- A speech-reinforcement system will be provided with a Revo Labs wireless microphones system. This system consists of eight (8) wireless microphones and will allow for flexible and efficient speech reinforcement for all speakers at the front table location.
- An automatic microphone mixing system will combine all microphones, heard through ceiling-mounted loudspeakers.

Sources

The following sources can be selected for viewing. * denotes that connections will be provided, but computer(s) are provided by the Customer.

- Laptop at wall*
- (OFE) DVD/VCR Combo
- A custom plate will be installed at a TBD location and allow for VGA w/ audio connectivity.

Equipment Rack

- All of the HB supplied equipment that supports the proposed system will be mounted in an equipment rack with a locking front door and located in a TBD location.

PROPOSAL

HB COMMUNICATIONS
INC

CLARIFICATIONS

At the end of this proposal is a list of typical assumptions, conditions, and clarifications. Those terms hold true except as noted below:

- All 120V AC power is to be done by others.
- Proper backing may be required at display location.

PROPOSAL

HB COMMUNICATIONS
INC.

PRICING

Item	Qty.	Manufacturer	Model #	Description	Unit	Total
Display						
1	2	Mitsubishi	LVP-XD520U	3000 lumen projector	1,210.00	2,420.00
2	2	Chief	RPA-U	projector mount	149.00	298.00
3	2	Chief	CMS-660	ceiling tile adapter	88.00	176.00
4	2	Sharp	LC46D65U	46" LCD monitor	1,633.00	3,266.00
5	2	Chief	Pro-U	wall mount	223.00	446.00
6	2	Dalite	40237	60x80 Wall Mounted Projection Screen	248.00	496.00
Audio						
7	1	Revolabs	Exesys-BLK-NM	base system	4,165.00	4,165.00
8	8	Revolabs	05-TBLMIC-DR-11	boundary microphones	190.00	1,520.00
9	1	Shure	SCM-810	Mc mixer	1,419.00	1,419.00
10	1	Shure	DFR-22	feedback suppressor	721.00	721.00
11	1	Tos	A912MK11	power amplifier	730.00	730.00
12	10	JBL	Control24ct	ceiling speakers	114.00	1,140.00
Signal amplification						
13	1	Extron	60-692-21	audio video amplifier	352.00	352.00
14	1	Extron	60-489-01	rgbhv DA	791.00	791.00
15	4	Extron	26-531-11	SY BNC cables	25.00	100.00
16	1	RDL	ST-D10K	audio combiner	61.00	61.00
Equipment Rack						
17	1	Middle Atlantic	Slim 5-21	Equipment Rack	268.00	268.00
18	1	Middle Atlantic	DO-5-21	Locking Front Door	138.00	138.00
19	1	Middle Atlantic	SP-5-21	Side Doors	169.00	169.00
20	1	HB	cables & connectors	cables, connectors, hardware, etc.		705.00
TOTAL EQUIPMENT:						19,381.00
Technical Services						
1	HB	Engineering		system design & engineering	1,020.00	
2	HB	Drafting		drafting & documentation	300.00	
3	HB	Pre-Installation		shop assembly & testing	1,680.00	
4	HB	Installation		on site installation & testing	3,420.00	
5	HB	Project Management		project management & site coordination	1,035.00	
6	HB	Freight		freight in/out	538.00	
7	HB	Site Warranty		6-months, see terms	no charge	
TOTAL NON-EQUIPMENT:						7,993.00
1. See attached proposal for payment terms & conditions. 2. Prices shown are good for 30 days. 3. Please reference quote # P-102182-R1.						
TOTAL PROJECT - excluding applicable taxes:						\$ 27,374.00

PROPOSAL



PAYMENT TERMS

Prior to commencement of work, a purchase order and a deposit equaling 30% of the entire project value are required.

Progress billing will prevail. Each month for the duration of the project, HB will issue an invoice. This invoice will contain charges for all costs incurred to date, including goods "bought and stored".

SCHEDULE

Typically completion time for a project of this type is 6 – 8 weeks after receipt of order, deposit, and approved scope of work.

APPROVALS

On behalf of the Customer, I accept and approve the functional description, scope, and line item pricing of the project described in this document.

Signed: _____

Title: _____

Date: _____

PROPOSAL

Standard Terms & Conditions

Unless otherwise noted elsewhere in this proposal from HB Communications, Inc. ("HB"), the following terms and conditions apply to this proposal. If these presumptions are not met, additional costs may apply.

DRAWINGS: HB will furnish appropriate drawings for coordination of other trades and services as required.

LABOR COSTS: All labor is priced as non-union, straight time, and during normal business hours. Costs for union and/or overtime will be considered outside of the scope of this offering.

MILLWORK: Fabrication or modification of all casework and millwork is the responsibility of others and not included herein. HB will furnish drawings or specifications for dimensional, electrical and thermal requirements.

CABLE PATHS: Prior to the start of work by HB, adequate cable pathways and/or conduit furnished by others must be complete. Conduit must meet NEC standards for bends/radius and fill.

POWER: All permanent high voltage power (120vAC and higher) is the responsibility of others and not included herein. All receptacles must be properly grounded per NEC standards and applicable building codes, and all circuits must be of sufficient capacity to provide full turn on power to all components.

All high-voltage power purposed to feed to HB-furnished equipment must be independent ground and same phase. Variance from this must be discussed and agreed by all parties prior to the start of work.

PLENUM: Cable and equipment to be installed in air handling spaces is plenum rated as required by code. All other cable is priced as non-plenum.

STRUCTURED WIRING AND SERVICES: All voice and data communications cabling associated with this proposal is the responsibility of others. All work in this regard must be complete prior to the start of work by HB. All voice and data services associated with this proposal, including those for audio or video conferencing, must be active and tested per specifications and the installation and testing schedule.

RESTOCKING FEES: Any equipment ordered for the project and then returned at the discretion of the Customer will be subject to a 25% restocking fee.

SOFTWARE LICENSE: Source code for all software developed within the scope of this offering remains the property of HB Communications, Inc. Licensing terms are available upon request.

VIBRATION, AMBIENT NOISE & ACOUSTICS: Excessive ambient noise and vibration may affect the performance of the system(s) in this proposal. HB will be held harmless in this circumstance and resolution of these conditions are the responsibilities of others.

OWNER/CUSTOMER FURNISHED EQUIPMENT (OFE/CFE): All equipment not furnished within the scope of this proposal must meet original specifications and functionality. Cost of all remedies will be considered outside the scope of this proposal and may be at additional cost.

SCHEDULE: Project schedule changes and delays outside of the control of HB will be accommodated whenever possible. Costs incurred by HB because of delays outside of HB's control will be considered outside of scope and may be considered billable.

COMPLETION AND TEST: The project schedule must allow sufficient time for completion of all installation and final testing of systems prior to occupancy and use. HB will be held harmless for schedule changes outside of its control, and additional costs, including overtime labor, will be considered outside the scope of this proposal.

SITE CONDITIONS: Prior to commencement of work by HB, the job site must meet OSHA requirements as a healthful workplace. Appropriate safety policies should be set, maintained, and enforced by all work forces.

Prior to the delivery of all equipment and electronics, all construction must be complete and spaces free of dust and dirt. If, at the discretion of the Customer, equipment is brought to site before it is clean, functionality, longevity, and warranties may be compromised.

PROJECTION SCREENS: Installation and wiring of all projection screens are to be by others; associated costs are not included in this offering.

WALL OR CEILING FINISH WORK: The customer will assume responsibility for repairing, patching and painting all wall or ceiling finishes that may be disturbed as the result of installation work associated with this proposal.

SEISMIC CERTIFICATION & COMPLIANCE: The cost of any seismic studies or remedies required by local or state-building codes is not included in this proposal.

PERMITS: The cost of any building permits required by any municipalities will be passed on to the owner.

CREDIT APPROVAL: HB reserves the right to modify payment terms as stated or implied in this proposal, pursuant to results from our credit approval process.

Client Responsibilities

DELIVERY & ACCEPTANCE: Prior to delivery of any equipment to site, one or more representatives for the Customer will be designated as having the authority to sign for deliveries. A line-item packing list will be furnished with all deliveries.

In all cases the Customer accepts responsibility for the security and loss liability of all equipment located at the job site. Acceptance of delivery does not imply that systems have been accepted as functional. Unless other arrangements are made, a secure, lockable space will be furnished for storage of equipment and material.

ACCEPTANCE OF SYSTEM: HB utilizes an "Installation Sign-Off Form" to signify project completion. This document provides a mechanism for the Owner to acknowledge completion or receipt of beneficial use of the system or to identify performance exceptions to the system. This allows HB to remedy such exceptions on a timely basis, and identifies the beginning of the warranty period.

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Rudie Beers

From: Robinson, Josh [jrobinson@hbcommunications.com]
Sent: Wednesday, December 10, 2008 6:06 PM
To: Cavallere-Dapkin, Carol; Rudie Beers
Subject: RE: P-102182 - Waterford Town Hall Proposal

Rudie,

In regards to you response about the pricing structure and time frame lets just say the pricing is good until you decide to purchase. If we need to revisit for a change to the proposal than that might change things but as of now this pricing is approved moving forward. It was great meeting with you and I look forward to talking with you in the future.

Thanks

Josh

Josh Robinson
Sales Associate
HB Communications
60 Dodge Ave.
North Haven Ct. 06473
Ph 800-243-4414 ext. 7149
Fax 203-234-2013
www.hbcommunications.com

From: Cavallere-Dapkin, Carol
Sent: Wednesday, December 10, 2008 2:11 PM
To: 'rbeers@waterfordct.org'
Cc: Robinson, Josh
Subject: P-102182 - Waterford Town Hall Proposal

Hello Ms Beers,

Please find the attached proposal for HB project# P-102182 – Waterford Town Hall.

Note: This is a budgetary quote only; pricing is subject to change after 60 days. If you have any questions please contact Josh Robinson or myself.

Thank you,

Carol Cavallere-Dapkin
Sales Support

HB Communications
60 Dodge Ave.
North Haven, Ct. 06473
phone: (203) 747-7144
fax: (203) 234-2013
toll free: (800) 243-4414 ext.7144
carol.cavallere@hbcommunications.com

Rudie Beers

From: Rudie Beers
Sent: Wednesday, December 10, 2008 3:05 PM
To: 'Cavallere-Dapkin, Carol'
Subject: RE: P-102182 - Waterford Town Hall Proposal

Ms. Cavallere-Dapkin:

Thank you for your quote. As I indicated to Mr. Robinson, this quotation will serve as a budgetary guideline for an appropriation that will hopefully be available for us to begin the project in July 2009. As I told Mr. Robinson, if his quote was only good for a specified period of time, please indicate what your normal inflationary escalator would be. Please provide this information at your earliest possible convenience.

Thank you,
Rudie Beers
Director of Finance
Town of Waterford, CT

-----Original Message-----

From: Cavallere-Dapkin, Carol [mailto:carol.cavallere@hbcommunications.com]
Sent: Wednesday, December 10, 2008 2:11 PM
To: Rudie Beers
Cc: Robinson, Josh
Subject: P-102182 - Waterford Town Hall Proposal

Hello Ms Beers,

Please find the attached proposal for HB project# P-102182 – Waterford Town Hall.
Note: This is a budgetary quote only; pricing is subject to change after 60 days. If you have any questions please contact Josh Robinson or myself.

Thank you,

Carol Cavallere-Dapkin
Sales Support

HB Communications
60 Dodge Ave.
North Haven, Ct. 06473
phone: (203) 747-7144
fax: (203) 234-2013
toll free: (800) 243-4414 ext.7144
carol.cavallere@hbcommunications.com

Rudie Beers

From: Cavallere-Dapkin, Carol [carol.cavallere@hbcommunications.com]
Sent: Wednesday, December 10, 2008 2:11 PM
To: Rudie Beers
Cc: Robinson, Josh
Subject: P-102182 - Waterford Town Hall Proposal

Hello Ms Beers,

Please find the attached proposal for HB project# P-102182 - Waterford Town Hall.

Note: This is a budgetary quote only; pricing is subject to change after 60 days. If you have any questions please contact Josh Robinson or myself.

Thank you,

Carol Cavallere-Dapkin
Sales Support

HB Communications
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North Haven, Ct. 06473
phone: (203) 747-7144
fax: (203) 234-2013
toll free: (800) 243-4414 ext.7144
carol.cavallere@hbcommunications.com

CAPITAL IMPROVEMENT PLAN
PROJECT DESCRIPTION FORM

- A) AGENCY: Information Technology Committee**
B) PROJECT NAME: Town-wide Telecommunications Upgrade
C) CONTACT PERSON: Rudie Beers
D) DEPARTMENT'S PRIORITY: #5

E) DESCRIPTION:

- a) Describe the type, purpose, and anticipated accomplishments of the project.
- b) Indicate the progress to date on the project.
- c) List other projects it is coordinated with whether in your Department or with other Departments.
- d) Describe the affect the project will have on your annual Department expenditures:
- e) Attach plan, estimate, service area map and/or other support documentation.

This request is for the purchase of new hardware to enhance the existing integrated telephone system. The Board of Education converted to a voice over IP system in 2010. It is the Information Technology Committee's intent to merge telephone communications with the Board of Education, thereby eliminating the cost of the core hardware and software that was originally proposed in past requests, substantially reducing the cost associated with the conversion from the Mitel SX2000 system currently in use to the voice-over IP system currently being used by the Board of Education. The Mitel system will not be supported after 2015. The costs are being presented for informational purposes only and will be bid in accordance with the Town's requirements when the project is ready to be moved forward.

TOTAL COMMUNICATIONS, INCORPORATED PROPOSAL

TELECOMMUNICATION SYSTEM

for

Waterford Town Hall



prepared by
TOTAL
COMMUNICATIONS, INC.

 **MITEL**

Presented by
Al Perkins
Account Representative

October 23, 2008

Waterford Town Hall Complex

Mitel IP Phone Deployment

Qty	Description	Unit	Sell
150	Mitel 5330 IP Phone (Backlit)	\$ 284.00	\$ 42,600.00
150	Mitel IP Phone License	\$ 120.00	\$ 18,000.00
6	IP Conference Saucer Dark Grey	\$ 384.00	\$ 2,304.00
6	IP Conference Module (5300 Series)	\$ 115.00	\$ 690.00
	Equipment Total		\$ 63,594.00
	Labor		\$ 20,063.00
	Total		\$ 83,657.00

*Pricing includes Total placing the phones, initial set up, Training and Project Management.

**Pricing does not include any Data Equipment or Data Services.

Presented By:

Al Perkins
11/25/2008

Waterford Town Hall

Mitel SX2000 to 3300 Migration

Description	Qty	Unit	Price
3300 IP Communications Platform - Core Hardware and Software			
DUAL FIM 820NM MUTI MODE MMC	3	384.00	1,152.00
PWR CRD C13 10A 125V-NA PLUG 3 PK	1	26.88	26.88
DUAL T1/E1 TRUNK MMC	1	1,152.00	1,152.00
MXe AC POWER SUPPLY	1	384.00	384.00
MXe RAID SUB-SYSTEM	1	1,920.00	1,920.00
3300 MXe II	1	2,304.00	2,304.00
3300 HDD 2PK FOR MXe	1	614.40	614.40
3300 MXe II EXPANSION KIT	1	2,035.20	2,035.20
DSP II MMC	1	921.60	921.60
3300 GATEWAY S/W PKG (1400 DEVICES)	1	8,448.00	8,448.00
3300 IP Communications Platform - Solution Licenses			
(AMC)3300 - 1 MAILBOX LICENSE	10	48.00	480.00
(AMC)3300 COMPRESSION LIC-(8 CHANNELS)	1	528.00	528.00
Program: SX-2000 LIGHT and MICROLIGHT to 3300 ICP			
(AMC) SX2K Digital Link3300 NTWKLINK MIGR	8	0.00	0.00
Software Assurance			
3300 SW ASSURANCE - 1YR 1400 DEVICE	1	1,920.00	1,920.00
			21,888.00

Description	Qty	Unit	Price
3300 IP Communications Platform - Core Hardware and Software			
DUAL FIM 820NM MUTI MODE MMC	1	384.00	384.00
PWR CRD C13 10A 125V-NA PLUG 3 PK	1	26.88	26.88
3300 MXe II	1	0.00	0.00
DSP II MMC	1	921.60	921.60
3300 GATEWAY S/W PKG (200 DEVICES)	1	0.00	0.00
3300 IP Communications Platform - Solution Licenses			
(AMC)3300 COMPRESSION LIC-(8 CHANNELS)	1	528.00	528.00
Software Assurance			
3300 SW ASSURANCE - 1YR 200 DEVICE	1	720.00	720.00
			2,580.40

Description	Qty	Unit	Price
Network and Systems Management			
(AMC) 5 MANAGED SYS LIC ENTERPRISE MGR	1	2,880.00	2,880.00
ENTERPRISE MANAGER WITH OPS MANAGER	1	3,840.00	3,840.00
Mitel 3300 5550 IP Console	1	1,920.00	1,920.00
Upgrade Call Accounting		632.00	632.00
(AMOSS)SX2000 MANAGEMENT OPTION	1	0.00	0.00
			9,272.00

Equipment	\$	33,738.58
Labor	\$	16,625.00
Total	\$	50,363.58

* PC will be needed for the Operator.

**Pricing does not include Server for OPS/Enterprise Manager

Presented By:

AJ Perkins

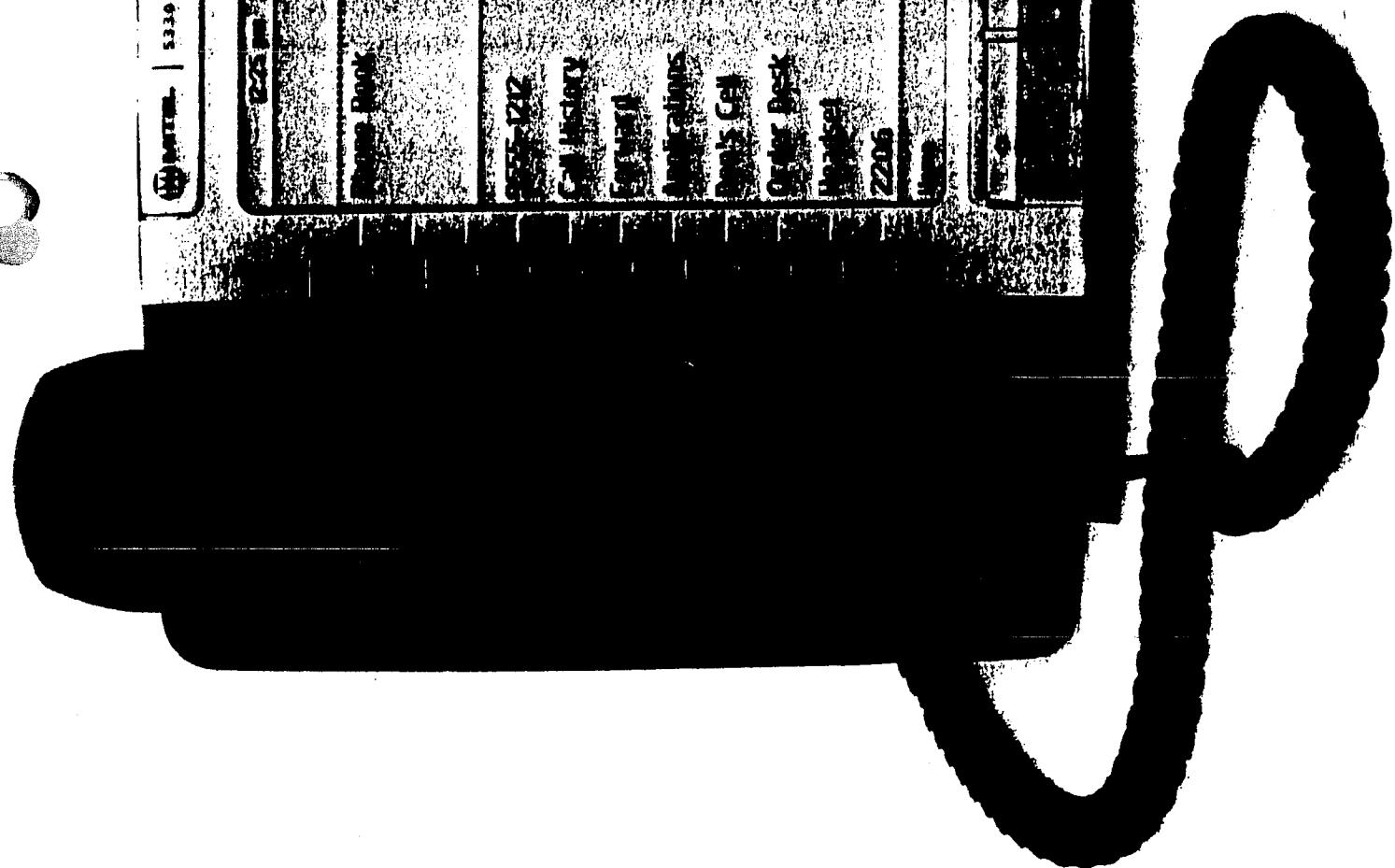
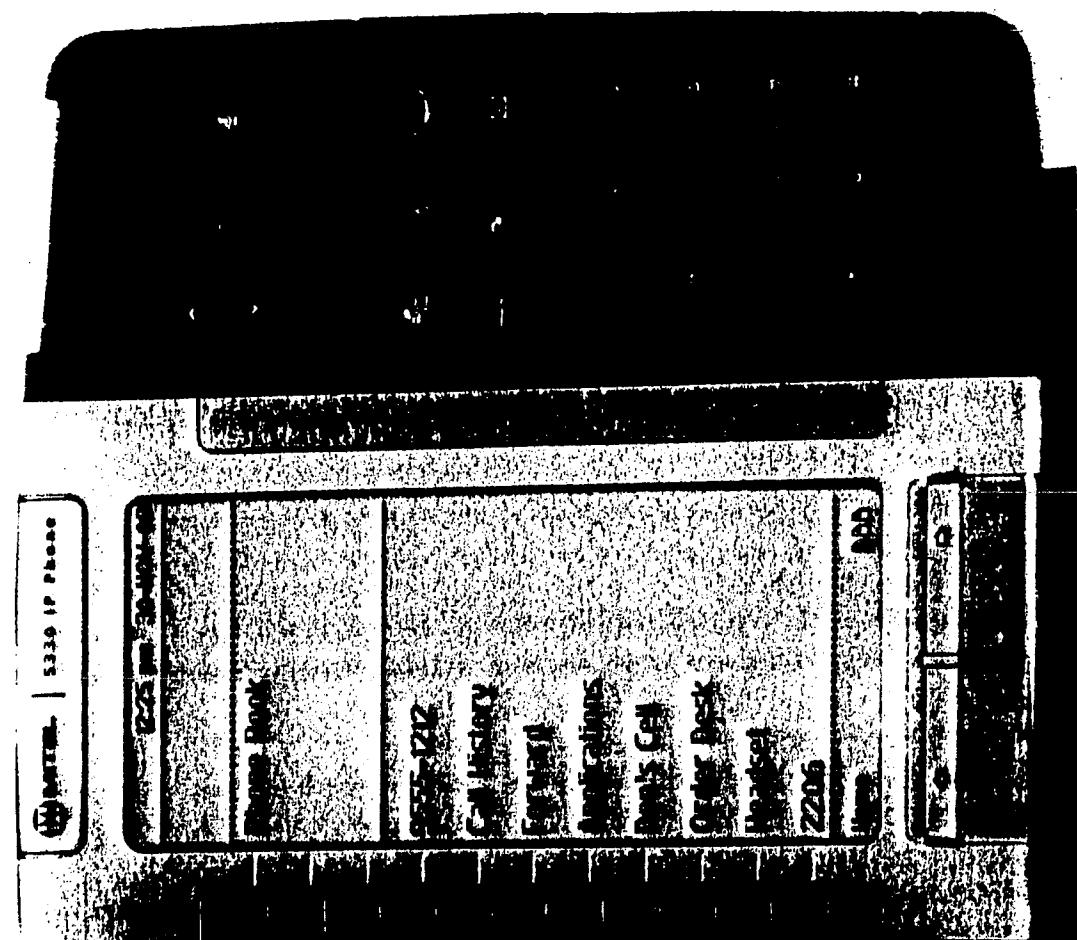
10/24/2008

MITEL

3300 IP Communications Platform



 **MITEL**



Waterford Town Hall

AVST Voice Mail Upgrade

Qty	Description	Unit	Sell
1	Mitel 3300 MITAI IP Integration (Server License)	\$ 1,600.00	\$ 1,600.00
12	Mitel 3300 MIAUDIO IP Port (Requires MITAI, Up to 48 ports)	\$ 208.00	\$ 2,496.00
12	Mitel 3300 IP Phone Licenses	\$ 120.00	\$ 1,440.00
12	2 years-XpressCare Renewal-CallXpress with 50 UM clients per port	\$ 228.00	\$ 2,736.00
5	2 years-XpressCare Renewal - UM Client per client over 50	\$ 11.00	\$ 55.00
1	CallXpress 7.x USB Security Dongle	\$ -	\$ -
		Equipment	\$ 8,327.00
		Labor	\$ 2,000.00
		Total	\$ 10,327.00

*You may use the existing Server or provide a new one.

Presented By:

Al Perkins
10/22/2008

TOTAL
COMMUNICATIONS, INC.

TELEPHONE DIRECTORY

Name	Prm Nam	Telephone Number	Prv #	# Typ	Department	Location
	N 519		N	Int	PLANNING	
	N 559		N	Int		
	N 853		N	Int	BOE-SUP	
	N 503		N	Int	TAX	
	N 824		N	Int	SANITARIAN	
	N 522		N	Int	VNA	
	N 467		N	Int	VNA	
ALARM RM 138	Y 610		N	Int		COMM CTR
ANN NOLAN	Y 755		N	Int		COMM CTR
AR CLERK	N 817		N	Int	TAX	
ASSESSMENT AIDE	N 561		N	Int	ASSESSOR	
ASSESSOR FAX	N		N	Int	ASSESSOR	
ASSESSOR MODEM	N 592		N	Int	ASSESSOR	
ASSESSOR OFFICE	N 820		N	Int	ASSESSOR	
ASSESSORS OFFICE	N 525		N	Int	ASSESSOR	
ASSESSORS OFFICE	N 819		N	Int	ASSESSOR	
AUDITORIUM	N 837		N	Int	SELECTMANS	
BARBAR AUBE	N 833		N	Int	HUMAN RSC	
BASEMENT MEETING RM	N 513		N	S/C		
BECKHAM, GAIL	N 767		N	Int		
BERNARD PISACICH	N 846		N	Int		COMM CTR
BILLIARDS RM	Y 747		N	Int	PLANNING	
BLDG. SECRETARY	N 563		N	Int	BOE-BUS	
BOARD OF ED-BUS	N		N	Int	BOE-BUS	
BOARD OF EDUCATION	N 801		N	Int		COMM CTR
BRIAN FLAHERTY	Y 757		N	Int		
BRUCE MILLER	N 891		N	Int	FIRE MARSH	
BUDIO/TV	N 528		N	Int	FINANCE	
BUILDING/HEALTH	N 826		N	Int	BUILDING	
CAROL LIBBY	N 827		N	Int	PLAN&ZONE	
CASSIE C.	N 854		N	Int	BOE-SUP	
CATALOGING	N 587		N	Int	LIBRARY	
CATHERINE REESE	N 832		N	Int	RESOURCES	
CHARLES LOBACZ	N 821		N	Int	ASSESSOR	
CHERIE RICHARDS	N 527		N	Int		
CHIEF ENGINEER-WPCA	N		N	Int	WPCA	
CHILDRENS DESK	N 571		N	Int	LIBRARY	
CHUCK PALARDY	N 781		N	Ext		
CIRCULATION DESK	N 569		N	Int	LIBRARY	
CIVIC TRI MAINT BLDG	N 895		N	Int		COMM CTR
COMM CTR MAIN	Y 884		N	Int		COMM CTR
COMPUTER	N 589		N	Int	LIBRARY	
COMPUTER CLASS	Y 614		N	Int		COMM CTR
CONF RM BOE	N 532		N	Int	BOE-BUS	
CONF RM BOE	N 581		N	Int		
CONF RM FINANCE	N 907		N	Int	FINANCE	
CONF RM P&R	Y 744		N	Int		COMM CTR
CONF RM SENIOR	Y 742		N	Int		COMM CTR
CONF RM FIRE MARSH	N 547		N	Int	FIRE MARSH	

FORMPRINT

TELEPHONE DIRECTORY

Name	Prm Nam	Telephone Number	Prv #	# Typ	Department	Location
CONF. ROOM	N	909	N	Int	PLANNING	
CONFERENCE ROOM	N		N	Int	BOE-SPED	
CONSTANCE HERMANN	N	892	N	Int	FIRE MARSH	
CORDLESS PHONE	N		N	Int	SELECTMANS	
COUNSELOR	N	574	N	Int	YOUTH SVC	
COURTESY EXT P&Z	N	580	N	Int	PLANNING	
CRAFT RM 103	Y	745	N	Int		COMM CTR
CRAFT RM 105	Y	746	N	Int		COMM CTR
CRAIG POWERS	N	851	N	Int		
CURTIS CARLOUGH	N	780	N	Ext		
DARLENE CELOTTO	N	809	N	Int	TAX	
DAWN CHOISY	N	509	N	Int	PLANNING	
DEB VIRGA	N	807	N	Int		
DEBRA BLOUNT	N	783	N	Ext		
DIANE DRISCOLL	Y	766	N	Int		COMM CTR
DIANE KEEMON	N	830	N	Int	TOWN CLERK	
DIANNE LOWTHER	N	860	N	Int		
DINING RM	Y	770	N	Int		
DISPATCH	N	534	N	Int	BOE-SPED	
DOLORES BURDIK	N	517	N	Ext		
DONALD BRIGHAM	N	774	N	Ext		
DONNA PAYNE	N	787	N	Int	BOE-BLD	
ED CRANE	N	563	N	Int	ELECTRICAL	
ED STEIN	N	905	N	Int	FIRE MARSH	
ELAINE PERRY	N	545	N	Int		
ELEVATOR	N	529	N	Int		
ELEVATOR Y.S.BLDG	N	841	N	Int		
ELLEN FOURGERE	Y	754	N	Int		COMM CTR
EOC	N	575	N	Int	EOC	
EOC	N	551	N	Int	EOC	
fax	N	506	N	Int		
FAX BOB	N	870	N	Int	BOE-BLD	
FAX P & Z	N	879	N	Int	PLAN&ZONE	
FAX-P&R DEPT	Y	791	N	Int		COMM CTR
FAX-SENIOR CTR	Y	790	N	Int		COMM CTR
FAX-SERVER ROOM	N	898	N	Int		
FINANCE	N	842	N	Int	FINANCE	
FINANCE FAX	N	579	N	Int	FINANCE	
FINANCE MODEM	N	845	N	Int		
FIRE MARSHAL	N	871	N	Int	FIRE MARSH	
FIRE MARSHAL	N	872	N	Int	FIRE MARSH	
FIRE MARSHAL FAX	N	894	N	Int		
Fire Marshall	N	544	N	Int	FIRE MARSH	
FIRE MARSHALL, FAX	N	894	N	Int		
FIRST SELECTMAN	N	557	N	Int		
FIRST SELECTMAN	N	834	N	Int	FIRST SELC	
FITNESS RM	Y	615	N	Int		
FRANK HOAGLAND	N	823	N	Int	SANITARIAN	
FRED LATHROP	N	786	N	Ext		

FORMPRINT

TELEPHONE DIRECTORY

Name	Prm Nam	Telephone Number	Prv #	#	Department	Location
				TYP		
GAIL BECKHAM	Y	764	N	Int		COMM CTR
GAIL MILLER	N	562	N	Int		
GAS BOY PUMPS	N	508	N	Int		
GERRY FRECHETTE	N	785	N	Ext		
GINNY BIELUCKI	N	843	N	Int	FINANCE	
GYM	Y	748	N	Int		
GYM 2	Y	749	N	Int		COMM CTR
HEIDI MURRAY	N	880	N	Int	BOE-BUS	
HILARY WILLARD	Y	765	N	Int		COMM CTR
INTER LIBRARY LOAN	N		N		LIBRARY	
IT ADMIN	N	524	N	Int		
IT HELP DESK	N	523	N	Int		
JAMES BARTELLI	N	763	N	Ext		
JAMES MINER	N	857	N	Int	BOE-BLD	
JANET HANNEY	N	564	N	Int	PLANNING	
JEANETTE PINCH	N	567	N	Int	BOE-SPED	
JEFFREY LATHROP	N	875	N	Int	FIRE MARSH	
JOAN BARNES	N	876	N	Int	FIRST SLC	
JOHN MURPHY	N	825	N	Int	BUILDING	
JOHN TALLIS	N	521	N	Int		
JOYCE CUMMING	N	852	N	Int	BOE-SUP	
JUDITH LISKOV	N	868	N	Int	LIBRARY	
Judy Hentzman	N	585	N	Int	LIBRARY	
K. ROTELLA	N	540	N	Int	FINANCE	
KAREN FERRARA	N	550	N	Int		
KAREN KOPEC	N	835	N	Int	BOE	
KATHIE MAINE	N	862	N	Int		COMM CTR
KERRY SULLIVAN	Y	758	N	Int		COMM CTR
KITCHEN OFFICE	Y	771	N	Int		
LIBARY PARK	N		N			
LIBRARY	N	805	N	Int	LIBRARY	
LIBRARY HUNT	N	306	N	Int	LIBRARY	
LIBRARY MODEM	N	463	N	Int		
LIBRARY MODEM	N	908	N	Int	LIBRARY	
LINDA FINNEGAN	N	572	N	Int	FIRE MARSH	
LINDA GEER	N	0	N	Int	OPERATOR	
LINDA GEER	N	800	N	Int	OPERATOR	
LOBBY INTERCOM	N		N			LOBBY
LOBBY SAFETY COMPLEX	N	210	N	Int		
LOUISE APPLEBY CONF	N	531	N	Int	SELECTMANS	
LUNCH ROOM	N	514	N	Int		
LUNCH ROOM	N	533	N	Int		
LUNCH ROOM EOC	N	211	N	Int	FIRE MARSH	
M. GLIDDEM	N	516	N	Int	PLANNING	
M. PINKHAM	N	838	N	Int	REGISTRAR	
MAGO POINT	N		N			
MAINT LIBRARY	N	582	N	Int	LIBRARY	
MARGARET LI PARULO	N	570	N	Int	LIBRARY	
MARK BURNHAM	N	815	N	Int	TAX	

FORMPRINT

TELEPHONE DIRECTORY

Name	Prm Nam	Telephone Number	Prv #	#	Department	Location
			Typ			
MARK PARKER	N	573			FIRE MARSH	
MARK WUJTEWICZ	N	500			P & Z	
MARK/MODEM	N				PLANNING	
MARY ANNE SEXTON	N	784			YOUTH SVC	
MAUREEN DELAUBO	N	848			P & Z	
MAUREEN FITZGERALD	N	501			LIBRARY	
MEZZ LIBRARY	N				ASSESSOR	
MICHAEL BEKECH	N	822			BOE-BUS	
MICHAEL DeRAY	N	850			PRINT ROOM	
MIKE B&NANCY LONG	N	511			FIRE MARSH	
MODEM	N	549			BOE-BUS	
MODEM BOE OFFICE	N	596			BOE-SPED	
MODEM BOE-SPEC SERV	N	861			FINANCE	
MODEM FINANCE	N	593			FIRE MARSH	
MODEM(BRUCE)	N				ASSESSOR	
MODEM/COMP LINE	N	859			LIBRARY	
MULTI PURP LIBRARY	N	584			COMM CTR	
MULTIPURPOSE 141	Y	612			COMM CTR	
MULTIPURPOSE 142	Y	613				
NANCY JAMES	N	776				
NEFTALI SOTO	N	762				
NINA DIGLORIA	N	535				
NUTRITION MEAL SITE	N	885				
NUTRITION SITE	N				OUTREACH	COMM CTR
OLD NORWICH	N					
ONG	N	400				
ONG L-2	N	300			OUTREACH	
OUTREACH	N				LIBRARY	
OVERDUES LIBRARY	N	586			PLANNING	
P & Z	N	903			P & Z	O
P&Z	N	814				COMM CTR
P&R RECEPTION	Y	772				COMM CTR
P&R WORK RM	Y	743				
PARK&REC OUTSIDE PH	N					
PARK&REC PAGE	N	32				
PARKS&REC	N				PARKS&REC	
PATRICIA O'NEILL	N	855			BOE-BLD	
PERMIT SOFT.MODEM	N	512				MODEM
PETER SCHLINK	N	874			FIRE MARSH	
PHONE ROOM	N	904			PHONE ROOM	
PHONE SYSTEM ROOM	N	186			TCI	
PLANNING REVIEW	N	462			FIRE MARSH	
PLANNING REVIEW	N	461			FIRE MARSH	
PLANNING ZONE	N					
PLANNING&ZONE	N	813			PLAN&ZONE	
Polcat tapit	N	171				
PROBATE	N	591			PROBATE	
PSYCH.	N					
Public Works	N				BOE-SPED	

FORMPRINT

TELEPHONE DIRECTORY

Name	Prm Nam	Telephone Number	Prv #	# Typ	Department	Location
PUBLIC WORKS DEPT	N	864	N	Ext	FIRE MARSH	
RANDALL COLLINS	N		N	Int	BOE-SUP	COMM CTR
REC & PARKS DEPT	N	881	N	Int		COMM CTR
RECEPTION	Y	740	N	Int	LIBRARY	
REF DESK LIBRARY	N	568	N	Int	REGISTRAR	
REGISTRAR	N	518	N	Int	REGISTRAR	
REGISTRAR	N	539	N	Int		
RICHARD GROVE	N		N	Int	TOWN CLERK	
ROBERT NYE	N	829	N	Int	PUB WORKS	
RON CUSANO	N		N	Ext	FIRE MARSH	
RONALD CUSANO	N	759	N	Int	FINANCE	
ROSEANN MARTELL	N	844	N	Int	LIBRARY	
ROSLYN RUBINSTEIN	N	869	N	Int	FINANCE	
RUTH BEERS	N	840	N	Int		COMM CTR
RYAN McNAMARA	Y	756	N	Int		COMM CTR
SALLY RITCHIE	Y	753	N	Int		
SANDRA KENNISTON	N	760	N	Ext	OUTREAGH	
SEAN KANE	N		N	Int	SELECTMANS	
SELECTMAN MODEM	N		N	Int	OUTREACH	COMM CTR
SENIOR CTR	N	530	N	Int		COMM CTR
SENIOR CTR PAGE	N	31	N	Int		COMM CTR
SENIOR RM 109	Y	768	N	Int		COMM CTR
SENIOR SERVICES	N	811	N	Int		COMM CTR
SENIOR SERVICES	N	839	N	Int		COMM CTR
SERVER RM	Y	611	N	Int		COMM CTR
SHORE RD	N		N	Int	PUB. WORKS	
SHORTY CHAPMAN	N		N	Int	BOE-SPED	
SMITH COVE	N		N	Int		
SPEC EDUCATION	N		N	Int	PLANNING	SPCL SVS
Special Services	N	802	N	Int		
STORAGE BASEMENT	N	538	N	Int	YOUTH SER	
SUE ROSENFIELD	N	806	N	Int		COMM CTR
SUSAN RADWAY	N	578	N	Int		
TACY LEE	Y	752	N	Int	TAX	
TAX COLLECTOR	N	816	N	Int		
TAX OFFICE	N	560	N	Int		
temp office 2	N	566	N	Int		
temp office fax	N	401	N	Int		
TERRY YOUNG	N	849	N	Int	BOE-BUS	
TOM WAGNER	N	505	N	Int	PLANNING	
TOWN CLERK	N	831	N	Int	TOWN CLERK	
TRACI SANTOS	N	536	N	Int	BOE-BUS	
TRACIE SNEED	N	858	N	Int	BOE-SPED	
TRACYS MODEM	N	558	N	Int		
UTILITY COMMISSION	N	886	N	Ext	TOWN CLK	
VAULT CLERK	N	507	N	Int	TAX	
VAULT TAX COLL.	N	502	N	Int		
VMAIL RMATS	N	598	N	Int	VNA	
VNA STORAGE	N		N	Int		

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WATERFRD

CONTINUED

FORMPRINT

PAGE 6

TELEPHONE DIRECTORY

Name	Prm Nam	Telephone Number	Prv #	# Typ	Department	Location
Voice Mail	N 599		N	Int		
WAITING RM SELECTMAN	N 504		N	Int	SELECTMANS	
Water Pollution	N		N			
WENDY PIAS	N 836		N	Int	REGISTRAR	
WILLIAM BARCLAY	N 828		N	Int	PLANNING	
WORK RM SENIOR	Y 741		N	Int		COMM CTR
WPCA	N		N		WPCA	
WPCA MAINTENANCE	N		N		WPCA	
YSB COUNSELOR	N 594		N	Int	YOUTH SER	
YSB COUNSELOR	N 546		N	Int	YOUTH SER	
YSB PROGRAM COORD	N 552		N	Int	YOUTH SER	
YVETTE PIER	N 590		N	Int	FINANCE	
[APY ROOM	N 187		N	Int		

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WATERFRD

FORMPRINT REQUEST COMPLETED

Waterford Town Hall

TDM to IP Migration

Enterprises considering migrating from TDM to IP-based systems are not making decisions based on technology platforms but rather on the wherewithal to achieve critical business imperatives: reduce the total cost of ownership and improve employee productivity and mobility. As such, organizations are investing in 'business solutions' – platforms, devices and applications – that will revolutionize the way they work and compete in the marketplace.

Business Motivations for Investing in IP

1. Reduce Total Cost of Ownership
2. Improve Business Communications and Customer Service
3. Improve Employee Productivity and Mobility
4. Disaster Recovery

Reduce Total Cost of Ownership

The ability to put voice over the LAN and/or WAN yields a list of hard and soft benefits that can be realized by the organization depending on the deployment scenario.

- a) Cabling Capital Cost Avoidance
- b) Minimize the Cost of Telephone Moves, Adds and Changes (MAC,s)
- c) Reduce Network Maintenance Expense

Teleworking benefits organizations in the following ways:

Improved employee productivity

Real estate savings

Reduced absenteeism costs

Environmental benefits

Network savings

Eliminate toll charges on InterLATA and IntraLATA calls

Equipment cost savings

Installation and configuration cost savings.

Improve Business Communications and Customer Service

By leveraging IP technology, organizations are able to seamlessly and cost-effectively extend corporate phone features and IP-based applications to employees regardless of location, thereby encouraging better communication and collaboration.

IP platforms can be used to extend applications irrespective of network or device type to users at their place of work (fixed or mobile). Organizations that run contact centers, for example, are realizing tremendous savings and improved operations by allowing contact center agents to work out of their homes or a designated remote location.

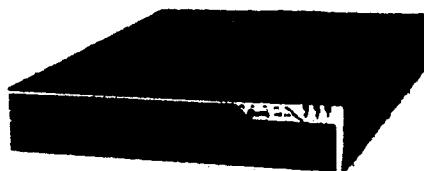
Improve Employee Productivity and Mobility

Technology investment cycles are ultimately driven by changes in the way we do business. Today's business climate is placing growing pressure on individuals, groups and organizations to work harder, longer and smarter to overcome the challenges brought by globalization, virtualization of the workplace and narrowing communication timeframes. The combination of these forces is driving us closer to the attainment of real-time business.

Disaster Recovery

With the deployment of IP technology, the organization can put a disaster recovery plan in place that allows an enterprise to continue business as usual seamlessly and cost effectively.

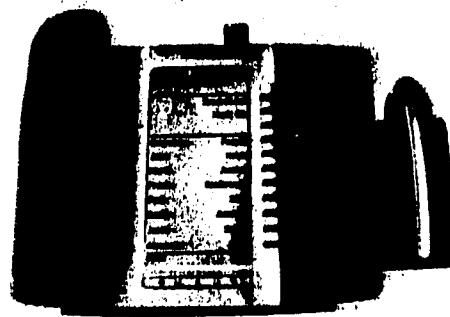
To evolve and ultimately to succeed in business, an organization's communications strategy must be aligned with its fundamental business goals. To be recognized as a viable contender, IP systems have had to prove their ability to consistently match the service quality and reliability of their TDM predecessors. There is no question that organizations (if they are not doing so already) routinely web-enable a business presentation, video-enable a business call, or seamlessly move from a wireline to a wireless network mid-conversation. It is by taking advantage of these innovations that organizations will streamline costs, and improve employee productivity, employee mobility, and business performance.



The 3300 ICP is a resilient network appliance that adds feature-rich IP communications and advanced user applications to the corporate LAN / WAN.

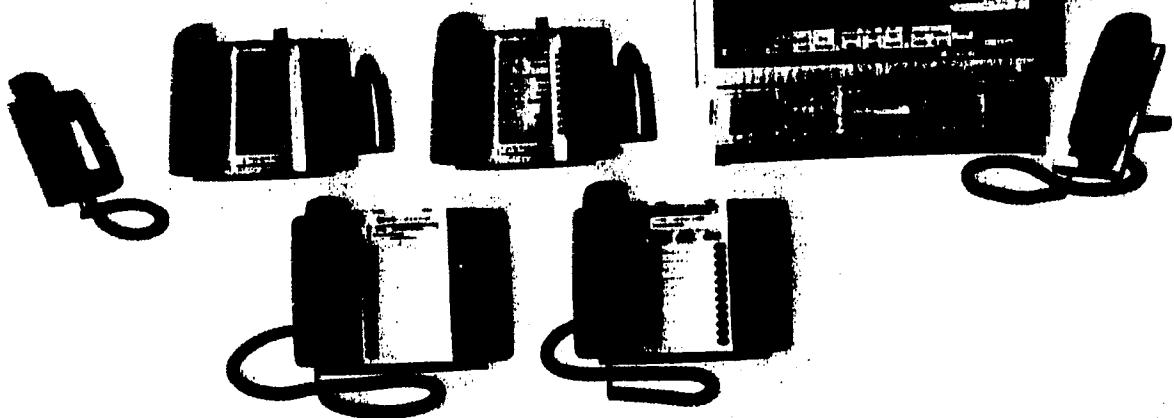
Add Feature-Rich IP Telephony and Advanced Applications to Your Corporate Network

The 3300 ICP is a proven, resilient converged communications platform that adds feature-rich IP communications and advanced user applications to corporate local and wide area networks (LAN / WAN). Readily scalable, the 3300 ICP can efficiently serve the needs of users in small, medium and large enterprises with single and multiple locations. It integrates rich call-handling, telephony features and networking built on Mitel's three decades as a leader in voice communications. In addition, the 3300 ICP supports many advanced applications including Mobility, Team Working, Customer Interaction, and Messaging designed to improve business processes. All are easily accessible via the industry's most extensive portfolio of desktop devices – and are readily managed via web-based embedded system management capabilities.



At Mitel, we believe the true promise of convergence is at the user desktop, and we've built the Mitel 3300 ICP to deliver.

Mitel offers the industry's broadest range of IP-based desktop devices, supporting diverse user needs and providing access to the 3300 ICP's telephony and network applications



Support Intuitive Access at the Desktop and Across the Enterprise

Mitel's industry-leading portfolio of desktop appliances, telephones, wireless phones, conference phones and consoles support intuitive user access to the telephony features and user applications supported by the 3300 ICP. From simple IP phones to web-enabled IP phones, only Mitel meets the diverse desktop needs of all users across the enterprise. And all support hot desking, allowing users to simply enter a password to automatically change any extension on the system into their own, complete with personal profiles and preferences.



The 3300 ICP can interoperate with traditional PBX systems to enable enterprises to evolve departments and facilities toward IP at a pace that makes sense for them

Manage it all, Simply and Cost-Effectively

The 3300 ICP provides simplified web-based management tools for efficient system configuration and maintenance. In addition, the inherent flexibility of Mitel desktop devices virtually eliminates the need and cost of onsite visits for moves, adds and changes (MACs), as do the intuitive personal programming interfaces supported by Mitel devices on the 3300 ICP. All of which combine with the efficiencies of operating a single integrated network to deliver significant cost of ownership savings.

Highly Scalable Architecture

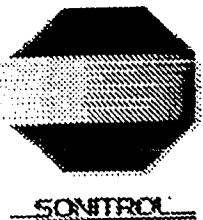
The 3300 ICP is built on the company's unique dual bus architecture: a switched IP core that is highly scalable and delivers resilient, robust switching, routing and call control, including support for traditional circuit-switched environments. This approach fully leverages the power of IP while recognizing the need to support traditional systems and devices as organizations evolve to a fully IP-based environment.

Proven IP Performance from the Leader in Convergence for the Enterprise

Already delivering on the promise of convergence to diverse organizations around the globe, the 3300 ICP is the proven solution that provides a strategic advantage through unparalleled user features, applications, simplified management and reduced system costs, while enabling companies to protect their investment in existing systems as they evolve toward a converged IP environment. The 3300 ICP is the result of Mitel's three decades as a leading innovator in user-focused business communications and delivers on the promise of convergence.

Mitel 3300 ICP's embedded wireless gateway provides 802.11 and IP-DECT functionality supporting mobility and location transparency within the enterprise





Expect The Best for Your Telecommunications Needs!

www.sonitrolhfd.com

Authorized Dealer

NEC

NEC Corporation of America

Buy Telecommunications Products off the State Contract # MA B-05-016

Sonitrol has been one of the top ranked vendors supplying telecommunications equipment, cabling, and paging systems to State and local government agencies. We can help you develop an accurate budget using fixed prices from a preapproved vendor.

How does the state contract make your job easier?

- ◆ No deposits are required. Simply provide a PO # and sign off on the equipment.
- ◆ Pre-negotiated prices offer a competitive solution on a proven product. (NEC)
- ◆ Single source for your phone equipment, paging and voice mail needs.
- ◆ Confidence purchasing from a financially tested organization.
- ◆ Avoid the complicated and time consuming open bid process.
- ◆ Fixed prices make it easier for you to budget and plan for the future.

What will we do for you?

- ◆ Review your phone bill to recommend cost saving alternatives.
- ◆ Explain how IP Technology will impact your organization.
- ◆ Show how call accounting software can save you money and improve safety.
- ◆ Describe how a service contract might or might not be the right thing to do.
- ◆ Demonstrate how similar organizations use a phone system as a safety tool.
- ◆ Design a voice mail system to improve your productivity.
- ◆ Improve your efficiency by routing calls into call groups or call centers.
- ◆ Provide a proven reliable resource for your communication needs.

Who else took advantage of working with Sonitrol and the State Contract?

East Hampton, Putnam, Montville, Redding, Shelton, UCONN, Hebron, Bridgeport, Granby, East Lyme, Montville, Govonor's Office, Rentschler Stadium, Norwich Technical, North West College, Windsor Locks, Judicial Court Houses, MCC and many more.

Marilyn Monaco (860) 616-7028

For a FREE budget proposal, contact Paul Sciano (Senior Sales Consultant)
Telephone: (860) 616-7015, email: psciano@sonitrol.net

CAPITAL IMPROVEMENT PLAN
PROJECT DESCRIPTION FORM

A) AGENCY: Information Technology Committee

B) PROJECT NAME: COP Logic – Web-based Reporting Software (Police)

C) CONTACT PERSON: Rudie Beers

D) DEPARTMENT'S PRIORITY: #6

E) DESCRIPTION:

- a) Describe the type, purpose, and anticipated accomplishments of the project.
- b) Indicate the progress to date on the project.
- c) List other projects it is coordinated with whether in your Department or with other Departments.
- d) Describe the affect the project will have on your annual Department expenditures:
- e) Attach plan, estimate, service area map and/or other support documentation.

This request is for the purchase of new software that will allow for on-line reporting of minor incidents and tips. The software will allow the Police Department to reallocate resources to productively focus on crime trends and decrease response time for priority calls. Citizens' statements may be completed on-line as well as property lists. The system will interface with the Records Management System. The cost is being presented for informational purposes only and will be bid in accordance with the Town's requirements when the project is ready to be moved forward.

Quote

copLogic

Coplogic, Inc.

231 Market Place, Suite 520
San Ramon, Ca. 94583
(800)734-9293
bmatuszko@coplogic.com
www.coplogic.com

November 25, 2008

Quote No.: 08-01Waterford,CT.

To: Marlena Montgomery
Waterford Police Department
41 Avery Lane
Waterford, CT 06385

Ship to (if different address):

Salesperson	Your P.O. No.	Date Shipped	Shipped Via	P.O. B. Point	Terms

Quantity	Description	Unit Price	Amount
1	Desk Officer Online Reporting System*	16,500.00	14,850.00
1	Yearly maintenance agreement (Incl. all upgrades/service)**	3,300.00	3,300.00
1	Train the Trainer Web Session (Incl.)	800.00	0.00
			0.00
	*Price reflects "First in State" discount		0.00
	**Includes Coplogic's hosting solution		0.00
			0.00
		Subtotal	18,150.00
		Sales Tax	0.00
		Shipping & Handling	0.00
		Total Due	\$ 19,250.00

Make all checks payable to:
Coplogic, Inc.

If you have any questions concerning this Quote, e-mail or call:
Brian Matuszko, bmatuszko@Coplogic.com

• 800-734-9293 ext. 747

THANK YOU FOR YOUR BUSINESS!

THIS QUOTE VALID FOR 90 DAYS FROM 11/25/08



WATERFORD POLICE DEPARTMENT
41 AVERY LANE
WATERFORD, CT 06385-2819



Murray J. Pendleton
Chief of Police

(860) 442-9451 TEL
(860) 442-2557 FAX

November 30, 2011

Mr. Daniel Steward, First Selectman
Town of Waterford
15 Rope Ferry Road
Waterford, CT 06385

RE: WATERFORD POLICE DEPARTMENT
CAPITAL IMPROVEMENT PLAN: FY 2013 – 2017

Dear Mr. Steward:

Attached please find the Waterford Police Department's Capital Improvement Plan request for FY 2013 – 2017, for your review and consideration.

Thank you.

Sincerely,

A handwritten signature in black ink.

Murray J. Pendleton
Chief of Police

CAPITAL IMPROVEMENT PLAN: PROJECT CONSOLIDATION FORM-FY2013-2017**DEPARTMENT/AGENCY: Waterford Police Department**

PROJECT NAME: IN ORDER OF DEPT.	S O U R C E						TOTAL FY 2013-2017
		FY-2013	FY-2014	FY-2015	FY-2016	FY-2017	
1 Parking Lot Resurfacing	4		\$170,000				\$170,000
2 Impound Storage Building	4				\$ 30,000		\$ 30,000
3							
4							
5							
6							
7							
8							
9							
TOTAL			\$170,000		\$30,000		\$200,000

INDEX TO FUNDING SOURCES,

1= CURRENT YEAR CAPITAL IMPROVEMENTS

2 = OPERATING BUDGETS,

3 = WASTE WATER BUDGET/SEWER CAPITAL MAINTENANCE FUND

4 = TRANSFER TO CAPITAL & NONRECURRING.

5 = SHORT AND LONG TERM DEBT FINANCING

6 = LOCAL CIP & OTHER GRANTS

7 = DESIGNATE FUNDS FROM UNDESIGNATED PROJECTS CNR;

8 = FUND FROM EXISTING DESIGNATIONS CNR

CAPITAL IMPROVEMENT PLAN
PROJECT DESCRIPTION FORM

- A) AGENCY: **Police Department**
- B) PROJECT NAME: **Parking Lot Resurfacing**
- C) CONTACT PERSON: **Murray J. Pendleton, Chief of Police**
- D) DEPARTMENT'S PRIORITY: # **1**
- E) DESCRIPTION:
- a) Describe the type, purpose, and anticipated accomplishments of the project.
Resurface the parking lot that serves the Police Department, the Public Safety Complex, the Youth Services Bureau, the Skateboard Park, and the Animal Control Facility
- b) Indicate the progress to date on the project.
None.
- c) List other projects it is coordinated with whether in your Department or with other Departments.
None.
- d) Describe the affect the project will have on your annual Department expenditures:
The longer we wait, the more it will cost, but once accomplished, there will be no impact on our annual Department expenditures.
- e) Attach plan, estimate, service area map and/or other support documentation.
The subject area is approximately 6,300 square yards. According the Assistant Public Works Director Kristin Zawacki, in 2007 it cost \$20.50 per square yard to do this type of work, and that price should be escalated between 3 & 5% per year. We would also need to include engineering and inspection costs. Allowing for a 4% increase per year, the 2014 price would be approximately \$26.98 per square yard, for a total amount of \$169,974.

Kathy Kent

From: Kristin Zawacki
Sent: Monday, November 01, 2010 8:35 AM
To: Kathy Kent
Cc: Murray Pendleton; Ron Cusano
Subject: RE: Chief Pendleton

Hi, Kathy,

The cost to do the parking lots for the Library and Town Hall was approximately \$20.50/sy. This included erosion and sedimentation control, the line striping, loaming and seeding and miscellaneous costs. I have deducted the sidewalk and ramp work from the cost. Also, please be aware that this cost should be escalated between 3 & 5% per year (this work was completed in 2007). You would also need to include engineering & inspection costs.

If you have any further questions, please let me know.

Thank you,

Kristin

From: Kathy Kent
Sent: Friday, October 29, 2010 2:20 PM
To: Kristin Zawacki
Cc: Murray Pendleton
Subject: Chief Pendleton

Hi, Kristin –

Chief Pendleton asked me to contact you to see if you could give us any information regarding the cost, per square foot, to resurface a parking lot. He said the Town Hall parking lot was recently done, so maybe you would know. Can you help us out?

Kathy Kent
Chief's Secretary
Waterford Police Department

Kathy Kent

From: Gerry Frechette
Sent: Wednesday, November 10, 2010 2:47 PM
To: Kathy Kent
Subject: parking lot square footage for the Chief
Attachments: police station.pdf

Kathy,

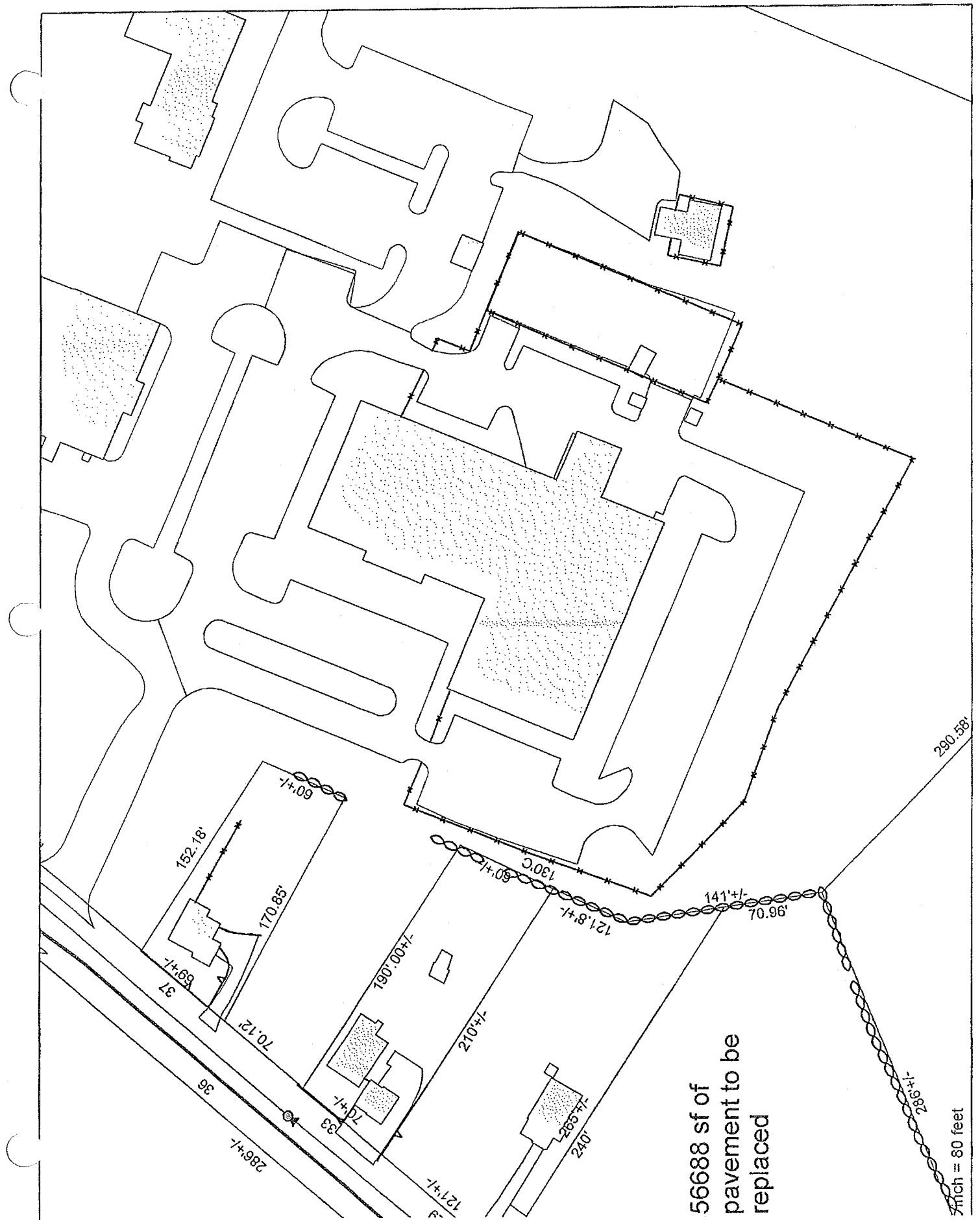
The chief asked Fred and I to determine the amount of area to be paved around the P.D.

The total square footage is 56,688 sq. ft. or 6,300 sq. yds

The attached pdf shows the area to be paved in yellow. I hope this helps out the Chief.

Please let me know if there is anything else he needs.

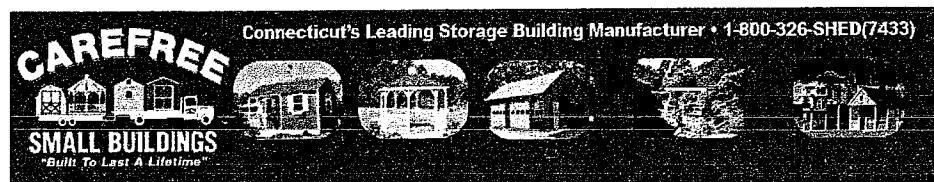
Jerry Frechette



CAPITAL IMPROVEMENT PLAN
PROJECT DESCRIPTION FORM

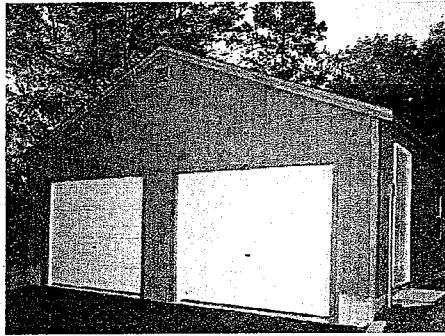
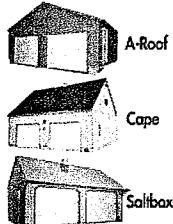
A) AGENCY: **Police Department**
B) PROJECT NAME: **Impound Storage Building**
C) CONTACT PERSON: **Murray J. Pendleton, Chief of Police**
D) DEPARTMENT'S PRIORITY: # **#2**
E) DESCRIPTION:

- a) Describe the type, purpose, and anticipated accomplishments of the project.
Recent demands from civil litigants have forced the Department to purchase temporary structures to preserve, for evidentiary purposes, materials and vehicles from major crime scenes. The accomplishment of this project will provide a permanent structure, thereby eliminating the need for ongoing expenditures for temporary structures.
- b) Indicate the progress to date on the project.
None.
- c) List other projects it is coordinated with whether in your Department or with other Departments.
None.
- d) Describe the affect the project will have on your annual Department expenditures:
It will eliminate the need to purchase temporary structures.
- e) Attach plan, estimate, service area map and/or other support documentation.



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The Carefree A-Roof Garage features primed Duratemp texture 1-11, nearly 8' high side walls, 2 or 3 9'x7' overhead doors and a 3' steel passage door. Available in sizes 24x20 through 24x48 in 7 different exterior sidings; Duratemp, Pine Novelty, Fairfax Ship-lap, Vinyl Clapboard, Cedar Novelty, Vinyl Shakes, and Cedar Clapboard.

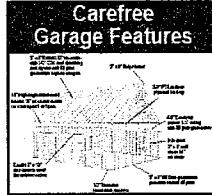
Starting Standard Price: 24'x20' \$13,430



Questions??

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TOWN OF WATERFORD
DEPARTMENT OF PUBLIC
WORKS



PROPOSED CIP

2013-2017

CAPITAL IMPROVEMENT PLAN PROJECT CONSOLIDATION FORM -FY2013-2017							
DEPARTMENT/AGENCY:	PUBLIC WORKS DEPARTMENT						
	SOURCE	FY2013	FY 2014	FY 2015	FY 2016	FY 2017	TOTAL
BUILDING IMPROVEMENTS							
MUNICIPAL COMPLEX WUC/DPW GARAGE/OFFICE RENOVATIONS/SITE CLEAN-UP	4			\$ 6,313,774			\$ 6,313,774
ROAD RECONSTRUCTION PROJECTS							
DOUGLAS LANE NO. 2	4	\$ 99,000	\$ 1,199,000				\$ 1,298,000
GARDINERS WOOD ROAD RECONSTRUCTION	4			\$ 169,000	\$ 1,973,000		\$ 2,142,000
ROAD RECLAMATION/MILL & OVERLAY							
DIMMOCK RD	4	\$ 271,619					\$ 271,619
DAYTON RD/FARGO RD	4		\$ 342,115				\$ 342,115
WILLETT'S AVE.	4			\$ 112,773			\$ 112,773
CROSS RD (RTE 85 TO BRIDGE)	4				\$ 348,995		\$ 348,995
BLOOMINGDALE ROAD	4					\$ 229,613	\$ 229,613
BRIDGE IMPROVEMENTS							
JORDAN COVE ROAD BRIDGE REPLACEMENT	4	\$ 380,000		\$ 2,292,000			\$ 2,672,000
SIDEWALKS & TRAILS							
BOSTON POST RD/GOSHEN RD	4	\$ 323,000					\$ 323,000
ROPE FERRY RD	4		291,000				\$ 291,000
PILGRAM RD/DAYTON RD	4			308,000			\$ 308,000
NORMAN, VIVIAN, CROSS, SPITHEAD GREENTREE	4				313,000		\$ 313,000
SHORE RD., JORDAN COVE RD.	4					345,000	\$ 345,000
NEW ROAD CONSTRUCTION PROJECTS							
PARKWAY NORTH CONNECTOR	4			150,700	2,292,000		\$ 2,442,700
CO-OP. RD. RECON. SEWER EXT.	4	0	\$0	\$0	\$0	\$0	\$0
(NONE SCHEDULED. WAITING FOR WASTEWATER FACILITIES PLAN)							
CONCRETE CURBING REPLACEMENT							
	4		\$ 69,554.00	70601	40978		\$ 181,133
TOTAL		\$ 1,073,619	\$ 1,832,115	\$ 9,346,247	\$ 4,926,995	\$ 574,613	\$ 17,934,722
INDEX OF FUNDING SOURCES							
(1) CURRENT YEAR CAPITAL IMP	(4) TRANSFER TO CAPITAL & NON-RECURRING			(7) TRANSFER FROM UNDESIGNATED CNR			
(2) OPERATING BUDGETS	(5) ALTERNATIVE FINANCING SOURCES(S)			(8) LEASE			
(3) WASTE WATER BUDGET	(6) LOCIP						

CAPITAL IMPROVEMENT PLAN
PROJECT DESCRIPTION FORM

A) AGENCY: PUBLIC WORKS DEPARTMENT
B) PROJECT NAME: MUNICIPAL COMPLEX RENOVATIONS/SITE REMEDIATION (FY 2013)
C) CONTACT PERSON: RONALD R. CUSANO, DIRECTOR, PUBLIC WORKS

D) DESCRIPTION:

- a) Describe the type, purpose, and anticipated accomplishments of the project.
- b) Indicate the Progress to date on the project.
- c) List other projects it is coordinated with whether in your Department or with other Departments.
- d) Describe the affect the project will have on your annual Department expenditures:
- e) (Attach Conceptual Plan)

E) PROJECT CATEGORY:

Facilities Improvements		Apparatus, Vehicle & Equipment Replacement	
Building Improvements	X	Highway Construction & Improvements	
Land Acquisition		Sidewalks & Trails	
Road Reconstruction-Sewer Extensions		Schools	
Water System		Sewerage System	
Community Campus			

The following is a brief history regarding the Municipal Complex Renovation Proposal:

HISTORY

In the FY02 Capital Improvement Program, the Waterford Utility Commission, WUC, had made a request for a Wastewater Maintenance Repair Facility. This project was denied at that time but recommended to be included in a new capital plan for the entire Municipal Complex. The project also included garage renovations and office modifications to the vacated Recreation and Parks Department, Outreach offices along with the DPW/WUC existing offices.

BUILDING COMMITTEE ESTABLISHED

A building committee had been established for the Renovation of the Municipal Complex under First Selectman Eccard with the following charges:

1. Develop a plan to move the Utility Commissions maintenance operation and equipment into the existing building at 1000 Hartford Road.
2. Determine the allocation of space within the Building for the First Selectman's consideration prior to funding.
3. Propose capital maintenance improvements to the existing offices as proposed in the Capital Improvement Plan.

4. Reestablish the firing range after site remediation.
5. Accommodate general equipment storage needs including the Board of Education.
6. Ability to accommodate Board of Education Maintenance.
7. Submit final scope and preliminary plans to the Board of Selectmen to initiate the appropriation process.
8. Feasibility Study to relocate Police Vehicle Impound Area

PHASE I ARCHITECT RETAINED

The firm of Lindsay, Liebig and Roche Architects was retained by the Building Committee. They had Mystic Air Quality conduct a Hazardous Material Report and Acorn Engineering perform a Building, Mechanical, Electrical, Plumbing, Fire Protection and Existing Systems Study in an attempt to find the overall condition of the building. The results of all of the studies found hazardous materials in the building and extremely old, inefficient and non-code compliant mechanical systems. (see enclosed)

BUILDING COMMITTEE CHARGES MODIFIED

A letter was received from First Selectman Steward, indicating that in reviewing the original charge for the committee, he is removing charge No. 5 and No. 6 which were related to the Board of Education equipment storage and maintenance needs. As noted in the Municipal Complex Building Committee Minutes of August 9, 2006, he indicated that when looking at the future plans for the High School, it seemed that this group of maintenance personnel would be better suited to have a more central location. The committee agreed.

As part of the initial charge to the committee, the firing range has been reestablished. The feasibility of relocating the Police Impound area has been explored with the Police Department and found not to be feasible.

After reviewing a conceptual design estimate prepared by Lindsay Liebig & Roche, the building committee agreed at their January 8, 2007 special meeting that the cost for Phase II shall be reduced to \$6,000,000, by reducing the scope of the project and retaining as much of the existing building as possible, renovating more of the garage rather than building new.

The committee agreed that the next step would be to get a preliminary design performed with various schemes and cost estimates utilizing the existing building to the maximum extent as possible through renovation and build whatever extra area is necessary to satisfy the DPW/WUC space requirements.

PHASE I – UTILITY MAINTNENACE GARAGE

To date, Phase I, the construction of the Utility Commission Maintenance Building that had been designed by Lindsay Liebig & Roche and constructed by Nosal Builders has been completed.

PHASE II – MUNCIPAL COMPLEX RENOVATION/SITE REMEDIATION

Schoenhardt Architects had been retained by the Town through the Municipal Complex Building Committee for the preliminary design of the building, Phase II, consistent with the charge by the First Selectman dated April 10, 2008 to Chairman Thomas F. Burns (enclosed.) Preliminary design plans have been completed with an associated cost estimate.

As directed in FY 09 CIP (enclosed), we were directed to include the site remediation or clean-up, except

the bus lot, with the Phase II building improvements. I have included correspondence from D. Scott Atkin of Anchor Engineering with the cost estimates related to Phase II remediation.

Prior to requesting design funding for the project, our committee was requested to appear before the Board of Selectmen, Board of Finance and RTM to provide them with a summary of the project status. This task was achieved through the following meetings as follows:

Board of Selectmen – July 21, 2009

Board of Finance – September 9, 2009

RTM – October 5, 2009

At the October 5, 2009 RTM meeting, a request was made to compare build new vs. renovate. This option was previously considered in January 2007 however to follow the RTM request, it was revisited. Base on this request, three options were prepared by our consultant Schoenhardt. The following are the associated construction costs without the cost of remediation or financing:

Option One : Renovation of Existing Building - \$4,917,000

Option Two: Renovation of Existing Building w/New Pre Engineered Garage – (Renovate the Office Area, Maintenance Garage Area and associated garage area between the two. Demolish and Rebuild the Recreation & Parks /Senior and portion of the garage area and construct butler style building) - **\$6,379,000**

Option Three: Demolish Existing Building Replace w/New Pre Engineered Building (Demolish and Rebuild the entire complex) - **\$7,103,000**.

Please note that the cost of remediating the site increases dramatically from option one to option three as we remove more portions of the building with the different options. This increased cost is due to the increased cost to remediate more of the soil under the building as required under DEP regulations.

The Municipal Complex Building Committee voted at their Special Meeting on December 2, 2009 to continue pursuing Option One.

BUS LOT REMEDIATION

The bus lot has been remediated and the First Student is fully operational again. The installation of the groundwater monitoring wells up and down gradient of the bus lot are schedule to be installed this fiscal year.

REMAINING PROJECT FUNDING FOR PHASE II/SITE REMEDIATION

The design development for the municipal complex Phase II has been completed and funding , \$375,525, has been approved and is preceding toward final design development and contract documents which is scheduled to be completed this fiscal year. The summary of project costs are as follows:

OPTION ONE

Site & Building Costs	\$4,917,000
Remediation Costs	\$ 790,000
Soft Costs	<u>\$ 545,000</u>
Cost	\$6,252,000

BID/NEGOTIATION & CONSTRUCTION	\$83,000
ADMINISTRATIVE PHASE COST	
TOTAL COST	\$6,710,525

Existing CIP funding:

LI#20530-57695(Designated) Municipal Complex Renovation	\$309,383
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LI# 20530-57696 (Designated) Municipal Complex Clean-up	<u>\$ 87,368</u>
Total Available Funding	\$396,751

Appropriation Requested	\$6,710,525 - \$396,751 = \$6,313,774
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CAPITAL IMPROVEMENT PLAN
PROJECT DESCRIPTION FORM

A) AGENCY: PUBLIC WORKS DEPARTMENT
B) PROJECT NAME: DOUGLAS LANE No. 2 – RECONSTRUCTION
C) CONTACT PERSON: RONALD R. CUSANO, DIRECTOR, PUBLIC WORKS

D) DESCRIPTION:

- a) Describe the type, purpose, and anticipated accomplishments of the project.
- b) Indicate the Progress to date on the project.
- c) List other projects it is coordinated with whether in your Department or with other Departments.
- d) Describe the affect the project will have on your annual Department expenditures:
- e) (Attach Conceptual Plan)

E) PROJECT CATEGORY:

Facilities Improvements	Apparatus, Vehicle & Equipment Replacement	
Building Improvements	Highway Construction & Improvements	X
Land Acquisition	Sidewalks & Trails	
Road Reconstruction-Sewer Extensions	Schools	
Water System	Sewerage System	

All of Douglas Lane is classified as a collector road between Route 85, Hartford Road and Vauxhall Street Extension. It is a roadway that has an approximate ADT (average daily traffic) of 2000.

Construction is essentially complete for the southerly portion of Douglas Lane from approximately 700 feet from the intersection of Rte 85 northerly 2000 feet to the vicinity of the power lines or approximately 300 feet south of house #42.

The northerly portion of Douglas Lane, which I am describing as Douglas Lane No. 2, is in very poor condition. The roadway has a pavement condition index of 32 and is recommended to be reconstructed. This portion of Douglas Lane is also one of the original oiled roads in Town, which was never slated for sewers. It was therefore never a candidate of the Cooperative Road Reconstruction Program. Minor drainage improvements had been installed periodically over the years and the road has received surface treatments such as chip seals. This surface treatment does nothing for the rideability of the road but seals it against damaging water. Estimated reconstruction is as follows:

Estimated reconstruction cost is as follows:

Engineering Services, (Survey, Design, Geotechnical Investigations, IW Permitting, Army Corps Permits) - \$99,000

**Bidding , Construction Admin., Resident Engineer Services - \$91,000
Construction - \$938,000**

SUB - TOTAL = \$1,128,000

15% Contingency - \$170,000

TOTAL = \$1,298,000

Town of Waterford CT

Douglas Lane Phase 2 Roadway Reconstruction

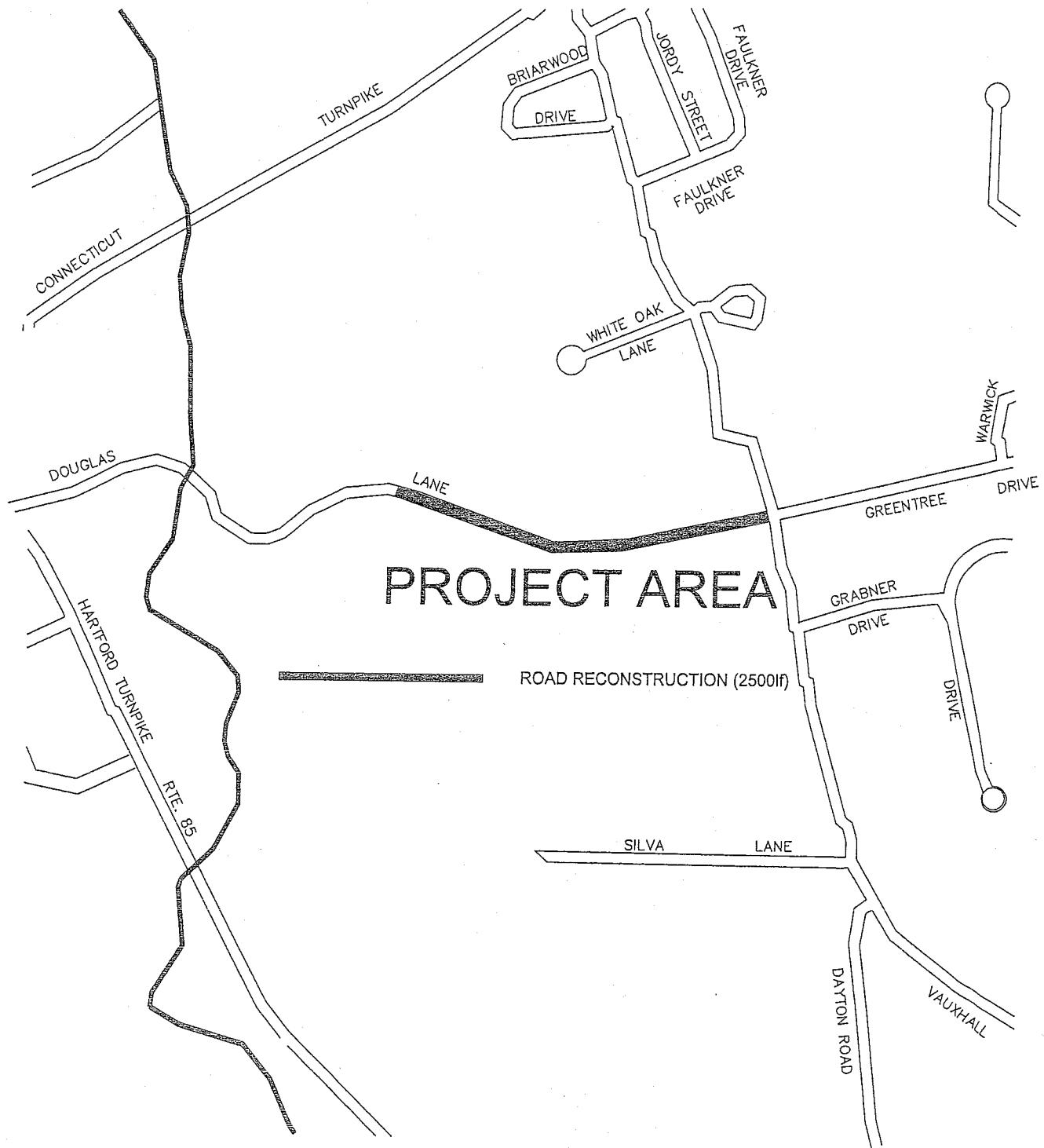
Fee Estimate

10/12/2010

Engineering Services

Task 1 Survey	\$18,000
Task 2 Roadway Preliminary Design	\$15,000
Task 3 Roadway Final Design	\$35,000
Task 4 Meetings	\$5,000
Task 5 Geotechnical Investigations	\$9,000
Task 6 IW Permitting	\$11,000
Task 7 USACE (Cat 1)	\$6,000
Task 8 Bidding	\$11,000
Task 9 Construction Admin	\$15,000
Task 10 Resident Engineer	\$65,000
2011 Engineering Services	\$190,000
2012 Construction (2500 LF @ \$375/LF)	\$938,000
Contingency (15%)	\$170,000

Estimated Project Budget **\$1,298,000**



**CONCEPTUAL PLAN
HIGHWAY CONSTRUCTION & IMPROVEMENTS
DOUGLAS LANE No. 2
TOWN OF WATERFORD
PUBLIC WORKS DEPARTMENT**

CAPITAL IMPROVEMENT PLAN
PROJECT DESCRIPTION FORM

A) AGENCY: PUBLIC WORKS DEPARTMENT
B) PROJECT NAME: GARDINERS WOOD ROAD – RECONSTRUCTION
C) CONTACT PERSON: RONALD R. CUSANO, DIRECTOR, PUBLIC WORKS

D) DESCRIPTION:

- a) Describe the type, purpose, and anticipated accomplishments of the project.
- b) Indicate the Progress to date on the project.
- c) List other projects it is coordinated with whether in your Department or with other Departments.
- d) Describe the affect the project will have on your annual Department expenditures:
- e) (Attach Conceptual Plan)

E) PROJECT CATEGORY:

Facilities Improvements		Apparatus, Vehicle & Equipment Replacement	
Building Improvements		Highway Construction & Improvements	X
Land Acquisition		Sidewalks & Trails	
Road Reconstruction-Sewer Extensions		Schools	
Water System		Sewerage System	

Gardiners Wood Road is classified as a collector road between Route 156, Rope Ferry Road and Jordan Cove Road. It is a roadway that has an approximate ADT (average daily traffic in 1999) of 2249. The road has no water or sewers and none are anticipated in the near future (see attached). The road also has a poor cross section and pavement surface with many patches. It is a road that has never been reconstructed, has no curbs or piped drainage except for two cross culverts that are severely undersized. Because the two cross culvert locations are undersized, the roadway floods many times during the year. The flooding becomes an issue when the days are warm and the cars track the water approximately 100 feet to each side of the culverts. When the temperature drops below 32 degrees at night, the road becomes black ice in which many cars experience hazardous driving conditions with the potential of accidents. In major storm events, the roadway has to be closed to traffic because of the extreme depths of flooding at the culverts. This becomes even a larger issue when an evacuation has to take place because Gardiners Wood Road is a major north-south route traveled by many in the Pleasure Beach and Millstone Point area.

The pavement has a PCI, Pavement Condition Index, of 40. More importantly, the two cross culverts have to be increased in size to accommodate all of the brook flow to prevent roadway flooding.

Estimated reconstruction cost is as follows:

Engineering Services, (Survey, Design, Geotechnical Investigations, IW Permitting, Army Corps Permits) - \$169,000

Bidding , Construction, Resident Engineer Services - \$93,000
Construction - \$1,600,000

SUB - TOTAL = \$1,600,000

15% Contingency - \$280,000

TOTAL = \$2,142,000

Town of Waterford CT
Gardiners Wood Road Reconstruction
Project Budget
10/12/2010

Engineering Services

Task 1 Survey (incl. wetlands ID and 2 prop. maps)	\$24,000
Task 2 Roadway Preliminary Design	\$22,000
Task 3 Roadway Final Design	\$57,000
Task 4 Meetings	\$5,000
Task 5 Geotechnical Investigations	\$10,000
Task 6 IW Permitting	\$15,000
Task 7 USACE (Cat 2 with hyd. model)	\$36,000
Task 8 Bidding	\$13,000
Task 9 Construction Admin	\$15,000
Task 10 Resident Engineer	\$65,000
2011 Engineering Services	\$262,000

2012 Construction (4000 LF @ \$400/LF)	\$1,600,000
Contingency (15%)	\$280,000

Estimated Project Budget	\$2,142,000
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CAPITAL IMPROVEMENT PLAN
PROJECT DESCRIPTION FORM

A) AGENCY: PUBLIC WORKS DEPARTMENT
B) PROJECT NAME: ROAD RECLAMATION / MILL & OVERLAY
C) CONTACT PERSON: RONALD R. CUSANO, DIRECTOR, PUBLIC WORKS

D) DESCRIPTION:

- a) Describe the type, purpose, and anticipated accomplishments of the project.
- b) Indicate the Progress to date on the project.
- c) List other projects it is coordinated with whether in your Department or with other Departments.
- d) Describe the affect the project will have on your annual Department expenditures:
- e) (Attach Conceptual Plan)

E) PROJECT CATEGORY:

Facilities Improvements		Apparatus, Vehicle & Equipment Replacement	
Building Improvements		Highway Construction & Improvements	X
Land Acquisition		Sidewalks & Trails	
Road Reconstruction-Sewer Extensions		Schools	
Water System		Sewerage System	

We have been making efforts to prevent the deterioration of the roadways through an aggressive program of pavement preservation. Pavement preservation is a planned system of treating pavements to maximize their useful life by treating problems early. The most cost effective means of treating roadway pavement in their early stages of deterioration are through the use of crack seals and surface treatments, such as chip seals and micro surfacing. The costs for these treatments are included in my operating budget. Unfortunately, there are some existing roads that have been reconstructed in the past that have gone beyond these treatments and require road reclamation or overlays.

If the roads are beyond crack sealing and micro surfacing, where the road is severely cracked but not alligatored, we can mill and overlay at a more substantial cost than the above methods. This method includes milling off the wearing surface and providing a new bituminous concrete surface. A road recently milled and overlaid is Clark Lane. If the pavements are alligatored and cannot be improved with the above methods, our option is road reclamation. Road Reclamation consists of pulverizing the asphalt and gravel base, grading and compacting to provide an improved subbase to receive a new wearing course. This reclaiming and paving operation has been used on the Town Hall parking lot, Library parking lot and Vet's Field parking, the Old Colchester Road, Braman Road, Pepperidge Drive, Spithead Road, Shore Rd, New Shore Rd. and most recently Bloomingdale Road. Funds are designated to reclaim Upper Bartlett Road, Lathrop Road, from Upper Bartlett to the Town line, and Gurley Road. This work will begin spring 2012.

Dimmock Road, from Rte 213 to Braman Road, has a PCI 41, has been identified as the next road to reclaim and pave, having a deteriorated pavement condition index as noted. It is 3,647 feet long and is 24 feet wide. In regard to utilities within the road, water exists but has no sewer. The WUC has indicated to me that the Wastewater Facilities Plan does not go through the exercise of prioritizing the sewer needs at

this time. The recommended process is to, as the need (petition, septic failures, economics, etc.) arrives; determine the justification for each individual project by assigning priority points. This is similar to what DEEP does for the assignment of Clean Water Funds projects. At this time those priorities have not been determined yet. At this time, all of the areas on previous CIP's are at the same priority level until it is demonstrated that the needs for sewers are justified.

I am including an "Analysis of Pavement Conditions and Budget Projections" report as prepared by our engineering consultants, VHB, /Vanesse Hangen Brustlin, Inc. Based on their report, they are recommending an appropriation of \$300,000 per year but based on budget restraints, I am requesting \$271,619 this fiscal year.

The estimated cost of this project is as follows:

Road Reclamation FY 2013

Survey, Design, Contract Admin., Inspection
Dimmock Road

9725 SY @ \$2.93/SY = \$28,494
9725 SY @ \$25.00/SY = \$243,125

Total

\$271,619

ROAD RECLAMATION 2013 FUNDING REQUEST = \$271,619

ROAD RECLAMATION/MILL OVERLAY
SCHEDULE

Name	From	To	Length (ft)	SY	Alternative	Cost	Year
<u>RECLAMING & PAVING FY 2013</u>							
DIMMOCK ROAD	ROUTE 213	BRAMAN RD	3647	9725	Reclaim & Pave	\$271,619	07/01/2013
TOTAL EST. CONST.						\$271,619	
COST							
<u>RECLAMING & PAVING FY 2014</u>							
DAYTON ROAD #1	928' NORTH OF DAYTON PL	HOUSE #123	3353	8196	Reclaim & Pave	\$228,914	07/01/2014
FARGO ROAD	846' S OF ROUTE 85	END	1520	4053	Reclaim & Pave	\$113,200	
TOTAL EST. CONST.						\$342,115	
COST							
<u>MILL & OVERLAY FY 2015</u>							
WILLETT'S AVENUE	ROUTE 1	TOWN LINE	2441	9221	MILL & OVERLAY	\$ 112,773	07/01/2015
TOTAL EST. CONST.						\$ 112,773	
COST							
<u>MILL & OVERLAY FY 2016</u>							
CROSS ROAD	FOSTER RD	ROUTE 85	3173	15512	MILL & OVERLAY	\$ 189,712	07/01/2016
CROSS ROAD	NOVA-CHIP	FOSTER RD	2664	13024	MILL & OVERLAY	\$ 159,284	
TOTAL EST. CONST.						\$ 348,995	
COST							
<u>RECLAMING & PAVING FY 2017</u>							
BLOOMINGDALE RD	GALLONS	APPLEWOOD	3083	8221	Reclaim & Pave	\$229,613	07/01/2017
TOTAL EST. CONST.						\$ 229,613	
COST							

Ron Cusano

From: Neil Kulikauskas [NKulikauskas@kleinfelder.com]
Sent: Wednesday, November 02, 2011 4:36 PM
To: Ron Cusano
Cc: Kristin Zawacki
Subject: RE: Dimmock Road Reclaim & Pave
Attachments: Amendment No. DPW-21 Dimmock Road Reclamation.pdf

Ron-

Here is the amendment for the Dimmock Road. Fee breaks down as follows:

Task 1 Survey \$2,100 (lump sum)

Task 2 Design \$6,300 (lump sum)

Task 3 Bidding \$5,700 (hourly rates)

Task 4 Construction Admin \$3,700 (hourly rates)

Task 5 Inspection \$10,700 (hourly rates)

Total \$28,500

Let me know if you have any comments or questions.

-Neil

From: Ron Cusano [mailto:rcusano@waterfordct.org]
Sent: Tuesday, November 01, 2011 10:54 AM
To: Neil Kulikauskas
Cc: Kristin Zawacki
Subject: Dimmock Road Reclaim & Pave

Neil,

Can you provide me with a proposal similar to the one you have done for the Upper Bartlett, Lathrop and Gurley Roads. Again, you will have to include survey and full time inspection. Dimmock Road is 3647 feet long, 24 feet wide with existing drainage.

Ronald R. Cusano P.E.,L.S.
Director of Public Works
15 Rope Ferry Road
Waterford, CT 06385
Ph 860-444-5864
Fax 860-442-9037

Ron Cusano

From: Neftali Soto
Sent: Monday, October 31, 2011 12:21 PM
To: Ron Cusano
Cc: Jim Bartelli
Subject: RE: FY 12 Road Reclamation

Hi Ron,

The Wastewater Facilities Plan does not go through the exercise of prioritizing the sewer needs at this time. The recommended process is to, as the need (petition, septic failures, economics, etc) arrives, determine the justification for each individual project by assigning priority points. This is similar to what DEEP does for the assignment of Clean Water Fund projects. At this time those priorities have not been determined, yet. At this time, all the areas on previous CIPs are at the same priority level until it is demonstrated that the needs for sewers is justified.

From: Ron Cusano
Sent: Monday, October 31, 2011 11:33 AM
To: Neftali Soto
Cc: Jim Bartelli
Subject: FY 12 Road Reclamation

Tali,

Last year you indicated to me in an e-mail that sewers would have a good chance of being installed in Gurley Road if it met a high priority in the Wastewater Facilities Plan. Now that the plan being is in a Draft form, does it look like that sewers will be proposed there? If not, I will consider proceeding to reclaim and pave that portion of road. Thanks

Ronald R. Cusano P.E.,L.S.
Director of Public Works
15 Rope Ferry Road
Waterford, CT 06385
Ph 860-444-5864
Fax 860-442-9037

Town of Waterford 5 Year Road Program



5 Year CIP Road Program

Year

- 2013
- 2014
- 2015
- 2016
- 2017



0 0.5 1 2 Miles

Analysis of Pavement Conditions and Budget Projections

Town of Waterford,
Connecticut

Prepared for **Ronald R. Cusano, P.E., L.S.**
Director, Department of Public Works
Town of Waterford
1000 Hartford Road
Waterford, CT 06385

Prepared by **████████/Vanasse Hangen Brustlin, Inc.**
Middletown, Connecticut

October 2010

Introduction

With the assistance of Vanasse Hangen Brustlin, Inc. (VHB) the Town of Waterford Public Works Department maintains a database of pavement conditions that can be used as a tool to help develop the Town's annual road program, and can also be used to project the future overall pavement conditions in Town under various roadway funding scenarios. The Town updates the database annually based on actual pavement maintenance and rehabilitation projects that have been completed and a field survey of pavement conditions. The following report summarizes the current pavement conditions in Waterford, and provides estimates of future overall pavement conditions, based on different funding assumptions. The purpose of this report is to provide the Town with the information it needs to make decisions with regard to its future roadway maintenance and improvement budget.

Summary of Current Conditions

The most recent pavement condition information for the Town of Waterford shows that the network average pavement condition index (PCI) is 83. The Town's pavement management software interprets the pavement distress information collected in the field to calculate a PCI for each pavement segment. The PCI is a rating between 0 and 100, where "100" represents a perfect pavement and "0" represents a completely failed and impassable pavement. The network average PCI found in Waterford is a very good average and is slightly higher than that of other New England towns of comparable size.

Treatment Bands

Treatment bands are classifications into which similar pavement repairs are grouped for the purposes of planning and summarizing budget scenarios. Treatment bands are defined by the pavement type and by the PCI range that applies to the repairs in the treatment band. The six treatment bands for the hot mix asphalt roadways in the Town of Waterford are listed below, along with their corresponding PCI range and example pavement repairs.

<u>Treatment Band</u>	<u>PCI Range</u>	<u>Repairs</u>
Reconstruction	0 - 39	Roadway reconstruction
Restoration	40 - 51	Reclaim and pave
Structural Improvement	52 - 58	Overlay or mill and overlay
Preventive Maintenance	59 - 72	Microsurface
Routine Maintenance	73 - 92	Crackseal and patch
Do Nothing	93 - 100	None

Rural roadways composed of years of penetration macadam, oil & sand, chip seals and other surface treatments are classified as "surface-treated" roadways. Different treatment bands apply to these roadways, because a different, more limited repair strategy is in effect for them. The treatment bands for surface-treated roadways are listed on the following page.

<u>Treatment Band</u>	<u>PCI Range</u>	<u>Repairs</u>
Restoration	0 - 50	Reclaim and pave
Preventive Maintenance	51 - 85	Chipseal
Do Nothing	86 - 100	None

Backlog of Pavement Improvement Needs

The term "backlog" is used to refer to the total amount of pavement improvement needs on the Town's road network at a given point in time. Calculating the current backlog of work provides the Town with a snapshot of current needs and gives the Town an idea of the type of repairs it should consider in its current and future plans. The backlog can also be used as a gauge of the effectiveness of the Town's repair strategy. One of the Town's goals should be to maintain or to reduce the total repair backlog over time. The current backlog of work required on all Town maintained roads is summarized below, broken out by the treatment bands described above.

<u>Treatment band</u>	<u>Miles</u>	<u>Dollar Backlog</u>
Do Nothing	17.3	\$0
Routine Maintenance (Crackseal & Patch)	53.3	\$1,036,512
Preventive Maintenance (Microsurface)	26.0	\$1,454,625
Structural Improvement (Overlay, Mill & Overlay) *	11.5	\$1,523,801
Restoration (Reclaim & Pave) *	8.7	\$3,461,047
Reconstruction *	2.8	\$3,528,061
Totals:	119.6	\$11,004,046

* Capital Projects

Budget Scenarios

The pavement management system uses pavement conditions and characteristics, repair alternative unit costs, and projected pavement deterioration trends to estimate the effects of various road budget scenarios on the overall pavement conditions over time. As part of the budget analysis process, the software "ages" the pavement using a deterioration curve to predict future pavement conditions.

Given the backlog of work described above, the estimated effects of the following three budget scenarios on the overall pavement conditions in Waterford over the next ten years were analyzed:

Planned funding

The "Planned" funding scenario projects the effects of completing the projects in the recommended 2011 to 2015 capital plan and then maintaining an average investment in capital pavement improvement projects of \$170,000 for the subsequent five years. In addition, this scenario provides for \$200,000 for Preventative Maintenance (microsurface applications) and \$50,000 for Routine Maintenance (Crack Seal & Patch) each year. This scenario does not include funding for specific projects classified as engineered road reconstruction, which may include costs for engineering design, drainage improvements, alignment changes, sidewalk improvements, etc.

Maintain PCI Funding

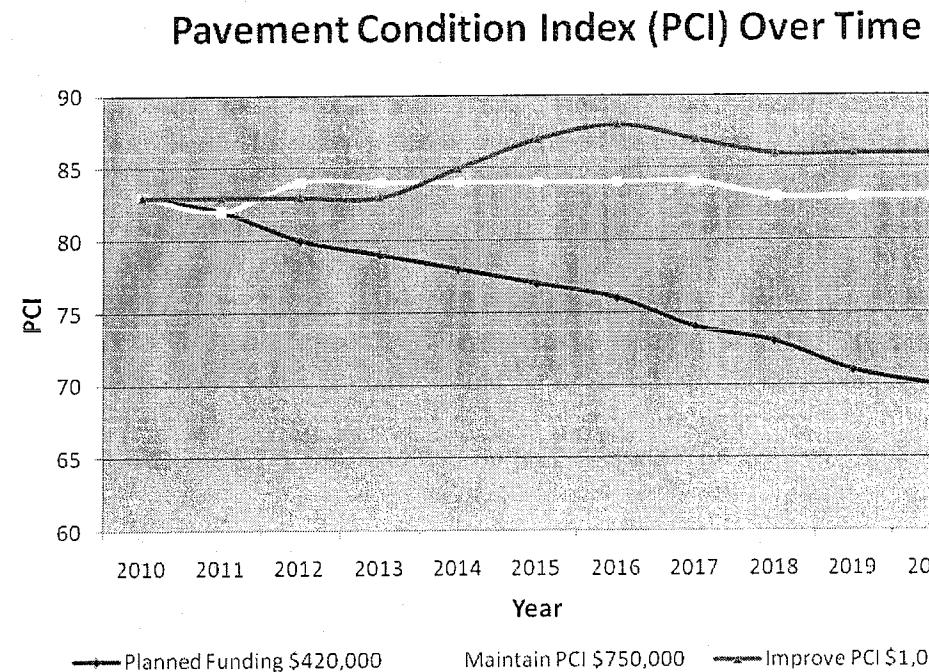
The "Maintain PCI" funding scenario increases the total annual budget for pavement maintenance and rehabilitation to \$750,000. This scenario is based on an optimum funding distribution between capital and maintenance each year. Again, this scenario does not include funding for specific projects classified as engineered road reconstruction.

Improve PCI Funding

The "Improve PCI" scenario predicts the effects of a budget that starts at \$1,000,000 per year in 2011 and increases to \$1.4 million per year by 2020. The additional funding provided in this scenario would be used to implement a more aggressive plan to rehabilitate those pavements in need of structural improvement, restoration, or reconstruction work, in addition to an increased preventative maintenance program.

Budget Analysis Results

The chart below illustrates the projected average PCI resulting from each of the three scenarios:



This chart shows that the average PCI will begin to decline significantly if the Town continues with the limited capital improvement funding amount outlined in the 2011 - 2015 plan. The "Maintain PCI" budget is estimated to be required to maintain the PCI at the current average of 83, while the higher funding provided in the "Improve PCI" scenario is projected to increase the average PCI to an 86 over the next 10 years. It should be noted that almost half of the funding in "Improve PCI" is allocated to maintenance work. The Town has a significant number of roadways that would benefit from a robust preventative maintenance program.

Conclusion

Based upon the results of these scenarios, it is recommended that when possible, the Town increase its current annual road funding and apply the additional funds evenly between an aggressive preventive maintenance program and the capital program.

The continued proper allocation of available funds is also important. The Town's approach has been to divide the total annual budget among maintenance work, preventative maintenance applications, and capital improvements. In doing so, the Town has developed a plan that reflects the definition of good pavement management: the Town is maintaining the roads in fair to good condition while it attempts to improve those roads in poor condition. This approach has contributed to the Town's above-average network PCI, and will be a key factor in the success of the Town's pavement program in the future.

CAPITAL IMPROVEMENT PLAN
PROJECT DESCRIPTION FORM

A) AGENCY: PUBLIC WORKS DEPARTMENT
B) PROJECT NAME: JORDAN COVE ROAD BRIDGE REPLACEMENT –
C) CONTACT PERSON: RONALD R. CUSANO, DIRECTOR, PUBLIC WORKS

D) DESCRIPTION:

- a) Describe the type, purpose, and anticipated accomplishments of the project.
- b) Indicate the Progress to date on the project.
- c) List other projects it is coordinated with whether in your Department or with other Departments.
- d) Describe the affect the project will have on your annual Department expenditures:
- e) (Attach Conceptual Plan)

E) PROJECT CATEGORY:

Facilities Improvements	Apparatus, Vehicle & Equipment Replacement	
Building Improvements	Highway Construction & Improvements	X
Land Acquisition	Sidewalks & Trails	
Road Reconstruction-Sewer Extensions	Schools	
Water System	Sewerage System	

We have received Bridge Inspection Reports for Local Structures from the Department of Transportation, specifically one for the Jordan Cove Road Bridge. In their reports they stated that the bridge deck was rated a 3 out of 10 (serious) due to numerous area of exhibiting serious spalling, hollow-sounding areas, cracking and other forms of deterioration. The superstructure of the support beams, were rated a 4 out of 10 (poor) due to the presence of hollow-sounding areas on several beams, spalling, delamination and visible rusting reinforcement. The substructure or abutments were rated a 5 out of 10 (fair) with various deficiencies such as cracking, loss of mortar at joints, exposed reinforcement and loose or cracked stones. Our Engineers, SEA Consultants Inc., has reviewed the bridge and reports and recommends its replacement. Their estimates are shown in the attached correspondence.

This total estimated cost of the project is \$2,971,000 which is eligible under the State of Connecticut Local Bridge Grant Program with an estimated reimbursement of 80%. Reimbursement would be \$2,377,000 leaving the Town with a net cost of \$594,000. Funds have to be appropriated and spent prior to federal reimbursement. A STEAP Grant, Small Town Economic Assistance Program Grant, was filed with the State of Connecticut Office of Policy and Management for the 20% balance of the project.

Based on the enclosed e-mail from Stanley Juber, Administrator of the Local Bridge Program for the Connecticut Department of Transportation dated October 7, 2010 indicating that the bridge is eligible for funding, I recommend the following:

Funding Request

FY2012	Engineering Design/Permitting Cost	\$330,000
FY2013	Engineering Const. Admin Services	<u>\$2,641,000</u>
	Construction Cost	
	Total	\$2,971,000

On March 11, 2011, we were notified by the State of Connecticut that the Jordan Cove Road Bridge project qualified for funding under the Local Bridge Program. On March 30, 2011, the town accepted the State of Connecticut's agreement to fund the project. A request for proposals for engineers were sent out with twenty three firms responding. Seven of the twenty three firms were short listed and interviews were held on September 6, 2011 by Rudie Beers, Finance Director, Kate Rotella, Purchasing Agent, Tom Wagner, Director of Planning, Kristin Zawacki, Assistant Director of Public Works and myself. CME Engineers from East Hartford, CT was chosen for the engineering services for the project.

An estimate of \$380,000 for design services was requested in the FY 12 CIP but deferred. I hereby request \$380,000 in the FY 13 CIP to initiate design.

MEMORANDUM

DATE: October 14, 2010
TO: Ron Cusano, Town of Waterford
FROM: Neil Kulikauskas *Neil K*
CC: file
SUBJECT: Jordan Cove Road Bridge over Jordan Cove (#04075)

We have reviewed the most recent CTDOT inspection report dated January 29, 2009 for the Jordan Cove Road Bridge over Jordan Cove (#04075). This bridge was built in 1937 making it 70 years old; and based on the recent inspection ratings, it appears that the entire deck superstructure, and substructure should be replaced.

- The deck was rated "3" (serious) due to numerous areas of exhibiting serious spalling, hollow-sounding areas, cracking, and other forms of deterioration.
- The superstructure was rated a "4" (poor) due to the presence of hollow-sounding areas on several beams, spalling, delaminations and visible rusting reinforcement.
- The substructure was rated a "5" (fair) with various deficiencies such as cracking, loss of mortar at joints, exposed reinforcement and loose or cracked stones. Although the substructure is rated fair, we are assuming that the entire substructure would also be replaced given the overall age structure.

The bridge dimensions are approximately 50 feet long by 35 feet wide. There is a walkway that extends from one side of the bridge. A water and sewer line are supported by the bridge and there are utility poles on one side of the roadway the presumably carry electric, telephone and cable utilities.

The approach roadway is in fair condition and will require full-depth reconstruction in the vicinity of the bridge and as well as resurfacing and safety improvements such as new guiderails along the approaches. We are assuming that there will be minor changes to the roadway horizontal and vertical geometry. We are assuming that the roadway will be closed to all traffic and detoured for the duration of the project, which we anticipate to be one full construction season.

We have prepared an order of magnitude estimate of probable cost for the replacement of the bridge. The total project cost for design and construction is estimated at \$2.971M. A detailed estimate for fiscal years 2011 and 2012 is attached.

It also appears that the project is eligible for Federal funding under the Local Bridge Program administered by the CTDOT. This program provides reimbursement of up to 80% of eligible project costs, for all phases of the project.

In support of this potential funding, we have prepared and attached the Preliminary Application with attachments. The First Selectman should sign the application and it can be submitted to the Local Bridge Program Manager. Federal projects are considered on an on-going basis and no specific timetable must be met for submission of the application.



MEMORANDUM

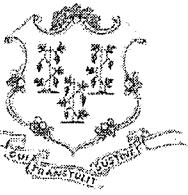
Please call should you have any questions.

Attachments:

- Estimated Project Budget
- Preliminary Application for the Local Bridge Program

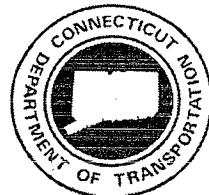
Town of Waterford CT
Jordan Cove Bridge Replacement
Estimated Project Budget
10/14/2010

Superstructure Replacement	\$900,000
Substructure Rehabilitation	\$600,000
Roadway	\$500,000
Subtotal Construction (2010)	\$2,000,000
Price Escalation to 2012 (2 years at 5%/year)	\$210,000
Subtotal Estimated Construction Cost (2012)	\$2,210,000
 Contingencies (10%)	 \$221,000
Subtotal Construction Cost (2012)	\$2,431,000
 Design (2011)	\$260,000
Permitting (2011)	\$70,000
Bidding (2012)	\$30,000
Construction Admin (2012)	\$60,000
Resident Engineer (2012)	\$120,000
Subtotal Engineering Services (2011-2012)	\$540,000
 Total Estimated Project Cost	 \$2,971,000
Eligible for Federal Reimbursement (80%)	\$2,377,000
 Total Net Project Cost to Waterford	 \$594,000



CONNECTICUT DEPARTMENT OF TRANSPORTATION

The Honorable Joseph F. Marie, Commissioner



PRELIMINARY APPLICATION FOR THE LOCAL BRIDGE PROGRAM

Preliminary application is hereby made by the Town/City/Borough of Waterford for possible inclusion in the Local Bridge Program for the following structure:

Bridge Location: Jordan Cove Road over Jordan Cove

Bridge Number: 04075 Length of Span: 50 feet

Sufficiency Rating: 49.52 Priority Rating: 48.50

Evaluation & Rating Performed by: X State Forces Others

If Others, Name of Professional Engineer: _____

Connecticut Professional Engineers License Number: _____

Engineering Firm: _____

Engineer's Address: _____

Engineer's E-mail Address: _____

Description of Existing Condition of Structure: *(attach description)* See Attachment A

Description of Project Scope: A *(note repair code; attach narrative/preliminary plans & specifications).* See Attachment B

Municipal Official to Contact *(name & title)*: Ronald R. Cusano, Director Public Works

Mailing Address: 15 Rope Ferry Road, Waterford, CT 06385

Telephone: (860)444-5864 FAX: (860)442-9037

E-mail: rcusano@waterfordct.org

Schedule: (Anticipated Dates – DD/MM/YYYY)

Public Hearing Conducted: 04/01/2011

Design Completion: 11/30/2011

Property Acquisition Completion: 01/30/2012

Utilities Coordination Completion: 01/30/2012

Construction Advertising: 04/01/2012

Start of Construction: 07/01/2012

Completion of Construction: 12/30/2012

Preliminary Application
Local Bridge Program – Federal

Page #2

Preliminary Cost Figures:

Preliminary Engineering Fees (Include Breakdown of Fees)	\$ 330,000
Rights-of-Way Cost (If applicable)	\$ --
Municipally Owned Utility Relocation Cost	\$ --
Estimated Construction Costs (Include Detailed Estimate)	\$ 2,210,000
Construction Engineering (Inspection, Materials Testing)	\$ 210,000
Contingencies (<i>10% of Construction Costs Only</i>)	\$ 221,000
Total Estimated Project Cost	\$ 2,971,000

Financial Aid Data:

Federal Reimbursement:

Total Estimated Project Cost multiplied by 80%:

Federal Aid Request \$ 2,377,000

I hereby certify that the above is accurate and true, to the best of my knowledge and belief. I also certify that this form has not been modified in any way from that distributed by the Department of Transportation.

Signature: _____
(Chief Elected Official, Town Manager, or other Officer Duly Authorized)

Date: _____

Return completed applications to: Mr. Stanley C. Juber
Administrator of the Local Bridge Program
Connecticut Department of Transportation
2800 Berlin Turnpike, P.O. Box 317546
Newington, Connecticut 06131-7546

PRELIMINARY APPLICATION FOR THE LOCAL BRIDGE PROGRAM
WATERFORD
JORDAN COVE ROAD BRIDGE OVER JORDAN COVE (#04075)

ATTACHMENT A
DESCRIPTION OF EXISTING CONDITION OF STRUCTURE

Summary

Overall, the existing bridge is in poor condition. According to the latest inspection report, the deck was rated "3" (serious), the superstructure a "4" (poor) and the substructure a "5" (fair).

Deck

The exposed reinforced concrete deck exhibits two dull sounding areas approximately 8' wide for the length of the bridge or approximately 45% of the deck surface. There are also numerous hollow areas, map cracking, spalling and scaling. There are also spalls with and without exposed reinforcement along the approach curbs.

Superstructure

The superstructure consists of reinforced concrete T-beams which have hollow-sounding areas on several beams, spalling, delaminations, map cracking (up to 1/16") and visible rusting of steel reinforcement. The end diaphragms have hairline cracks and map cracking with efflorescence. The bearings are heavily rusted and have missing or damaged anchor bolts.

Substructure

The substructure has various deficiencies such as cracking, loss of mortar at joints, exposed reinforcement and loose or cracked stones. Cracking ranges from small map cracking to 3/8" in size. The wingwall caps exhibit severe spalling and areas of major cracking and exposed deteriorated reinforcement.

Channel

The channel is in satisfactory condition with no serious signs of scour. No footings were exposed according to the last underwater inspection report. There is some areas of heavy embankment erosion on the northwest and northeast corners.

The last underwater and routine bridge inspection report dated January 29,2009 is attached to provide further details and information.

**PRELIMINARY APPLICATION FOR THE LOCAL BRIDGE PROGRAM
WATERFORD
JORDAN COVE ROAD BRIDGE OVER JORDAN COVE (#04075)**

**ATTACHMENT B
DESCRIPTION OF PROJECT SCOPE**

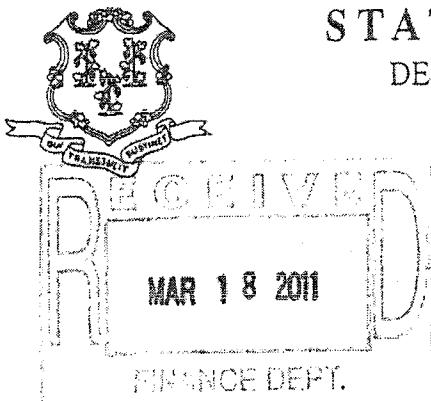
Given the condition of the bridge, we anticipate that the bridge will be replaced in its entirety. During reconstruction, Jordan Cove Road will be closed to vehicular and pedestrian traffic. A detour route will be established.

At this time, a bridge type study has not been performed. Consequently, the type of bridge has not been determined. At this time, we assume that the general bridge dimensions will be similar to those of the existing bridge. However, this may change depending on results of hydraulic analysis and further engineering considerations such as conflicts with the existing foundations.

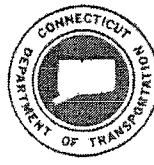
It is reasonable to assume that the foundation will consist of some type of deep foundation (e.g. piles) since the existing bridge is founded on timber piles. Abutments will be most likely constructed of reinforced concrete. Superstructure types that would be considered for a bridge of this span length would include precast prestressed concrete box beams (either butted or with a reinforced concrete deck) or steel beams with a reinforced concrete deck. A strong consideration will be given to resistance to corrosion in the marine environment.

Roadway work would most likely consist of full depth reconstruction in the vicinity of the bridge and some overlay work as necessary depending on pavement conditions. Replacement of guiderails and other related work would be included as well. We do not anticipate that major adjustment to the horizontal or vertical alignment will be necessary.

No engineering has been performed for this project at this time. Plans, specifications, estimates or hydraulic data are not available.



STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION



2800 BERLIN TURNPIKE, P.O. BOX 317546
NEWINGTON, CONNECTICUT 06131-7546

Phone:

MAR 18 2011

FINANCE DEPT.

March 11, 2011

The Honorable Daniel M. Steward
First Selectman
Town of Waterford
Town Hall
15 Rope Ferry Road
Waterford, CT 63852886

Dear Mr. Steward:

Subject: Local Bridge Program, Fiscal Year 2011
Commitment to Fund
Jordan Cove Road over Jordan Cove, Bridge No. 04075
Town of Waterford
Federal Funds: \$2,376,800.00

RECEIVED
CITY OF NEW HAVEN
CITY HALL'S OFFICE
11 MAR 17 PM 1:06

The Department has reviewed your Preliminary Application for the replacement or rehabilitation of the bridge at the subject location. I am pleased to inform you that the project qualifies for funding under the Local Bridge Program.

The State of Connecticut hereby commits to fund up to 80% of the eligible project costs under the Federal HBRR Program. This commitment is subject to the Program Regulations, in particular as follows:

1. The amount is based upon the information in your Preliminary Application and is subject to later adjustments.
2. This Commitment to Fund will lapse if your final cost estimate exceeds your Preliminary Application and sufficient monies are not available.
3. The Commitment to Fund does not constitute a binding agreement, and the State's obligation is further contingent upon your execution and delivery of a Project Agreement, and your compliance with its terms.

The Honorable Daniel M. Steward

- 2 -

March 11, 2011

The next step in the grant process, if this Commitment to Fund is acceptable to you, is to sign below and return a copy of this letter within 30 days. A Department representative will contact you to schedule a concept meeting to review the program's procedural requirements. So that the project's costs may be accurately tracked, it is recommended that you set up a separate budget line item for this project and make your auditor aware of the project. Also, the Department now relies heavily on electronic communications. Please ensure that we have an up-to-date e-mail address for your designated contact person at all times.

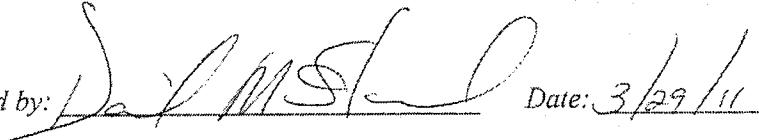
If you have any questions, or need any assistance, please contact Mr. Joseph Scalise, Liaison Engineer, at (860) 594-3389.

Sincerely,



Timothy M. Wilson, P.E.
Manager of Consultant Design
Bureau of Engineering and Construction

Accepted by:



Date: 3/29/11

Title: First Selectman

cc: Mr. Ronald R. Cusano, Director of Public Works

FIFTEEN ROPE FERRY ROAD



WATERFORD, CT 06385-2886

March 30, 2011

Mr. Timothy M. Wilson, P.E.
Manager of Consultant Design
Bureau of Engineering and Construction
Department of Transportation
2800 Berlin Turnpike
P.O. Box 317546
Newington, CT 06131-7546

Dear Mr. Wilson:

Re: Local Bridge Program, Fiscal Year 2011
Commitment to Fund
Jordan Cove Road over Jordan Cove, Bridge No. 04075
Town of Waterford, CT
Federal Funds: \$2,376,800.00

In accordance with your correspondence dated March 11, 2011, I am returning acceptance of the Department of Transportation's commitment to fund the above-referenced project under the Local Bridge Program.

On behalf of the Town of Waterford, I extend my gratitude for your consideration of our funding request.

Sincerely,

Daniel M. Steward
First Selectman

DMS:rab

Enclosure

Cc: Ronald R. Cusano, Director of Public Work
Ruth A. Beers, Director of Finance
Ginny Bielucki, Accountant

CAPITAL IMPROVEMENT PLAN
PROJECT DESCRIPTION FORM

A) AGENCY: **PUBLIC WORKS DEPARTMENT**

B) PROJECT NAME: **SIDEWALKS AND TRAILS**

C) CONTACT PERSON: **RONALD R. CUSANO, DIRECTOR, PUBLIC WORKS**

D) DESCRIPTION:

- a) Describe the type, purpose, and anticipated accomplishments of the project.
- b) Indicate the Progress to date on the project.
- c) List other projects it is coordinated with whether in your Department or with other Departments.
- d) Describe the affect the project will have on your annual Department expenditures:
- e) (Attach Conceptual Plan)

E) PROJECT CATEGORY:

Facilities Improvements		Apparatus, Vehicle & Equipment Replacement	
Building Improvements		Highway Construction & Improvements	
Land Acquisition		Sidewalks & Trails	X
Road Reconstruction-Sewer Extensions		Schools	
Water System		Sewerage System	

This project includes installation of sidewalks in accordance with the master plan for sidewalk construction and includes the renovation of all existing Town sidewalks. In the past, sidewalk construction had been coordinated with roadway reconstruction work to take advantage of drainage, curbing and grading available thus providing an improved facility at a limited sidewalk cost. As the major sewer projects were completed this economic advantage was reduced although a significant amount of sidewalks have been completed. Currently there are 33 miles of sidewalks maintained by the Town.

Sidewalks that have recently been renovated are on Fog Plain Road, Fitzgerald Avenue, Daniels Avenue, Willets Avenue, Rope Ferry Road and the Boston Post Road. No other new sidewalks are being installed until the Sidewalk Master Plan is updated by the Public Works, Planning & Zoning Development Sub Committee of the RTM outside of the ones approved by the Planning & Zoning Commission as part of a new subdivision or development.

As indicated above, a comprehensive Sidewalk Master Plan updating the 1978 version and adding trails is currently being reviewed and updated by the Public Works, Planning & Development subcommittee of the RTM. This update will address priorities, selection criteria and design characteristics. New projects are to be coordinated with the Plan of Conservation and Development. The revised Sidewalk Master Plan will take into consideration the recommendations of the Community Campus Plan and Civic Triangle traffic study that are currently being performed.

Sidewalk renovations include removal and placement of new bituminous concrete. In the past we have tried to cut and patch portions of the sidewalk that were broken, extremely cracked or had potholes, followed by a tack coat and a 1" overlay. This method proved to be labor intensive and actually more costly.

Where sidewalks conditions have deteriorated to a point where there is an extremely large amount of patching required, sidewalk replacement is recommended. The following would be the estimated cost of performing the work for our network:

Annual Maintenance Cost (Bituminous Concrete) – 33 miles of sidewalks

Reevaluated to 25 year life

33 miles/25 years = 1.32 miles/yr

1.32 miles x \$38.50/lf = \$268,330 say \$268,330/yr

This amount will vary year to year based on the length of existing sidewalk and in an effort not to replace the sidewalk in pieces.

There are three sidewalks that I am recommending for funding FY12. The first is the Boston Post Road sidewalk from Tiffany Court to Clark Lane. This sidewalk is one of the oldest in Town being 46 years old. I am recommending that it be removed and replaced for the reasons as stated above. This cost of replacement of this sidewalk is approximately \$38.50/lf because of the additional cost of curbing replacement located adjacent to the sidewalk which is in poor condition and an integral part of the sidewalk. There is also an additional cost for maintenance and protection of traffic due to the high speed/volume roadway.

The second sidewalk is also on the Boston Post Road from Willets Avenue to the New London Town Line and is also 46 years old. The third is on Goshen Road from Shore Road to Great Neck Road and is 22 years old but adjacent to the Great Neck School and in poor condition.

BOSTON POST ROAD SIDEWALK:

Boston Post Rd, Tiffany to Clark Lane

Estimated Cost 2900'@\$40.84/LF = \$118,436

Boston Post Rd, Willets Ave to NL Town Line

2100' @40.84/LF = \$85,764

Goshen Rd, Shore Rd to Great Neck Rd

2900'@\$40.84/LF = \$118,436

Estimated Construction Cost

\$322,636 say

Design, Inspection Bidding and Contract Administration will be handled in house.

Funding Request = \$323,000

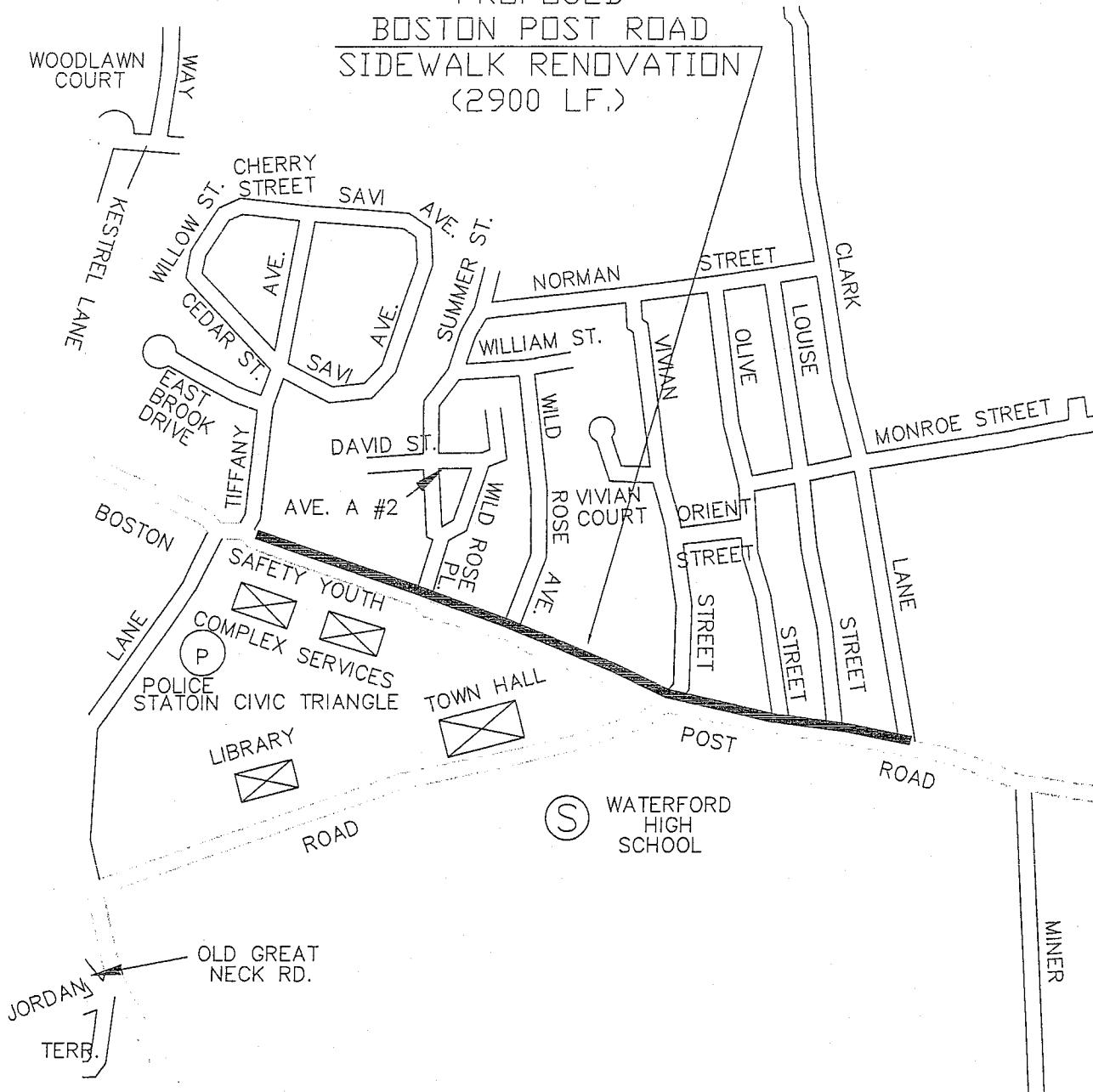
PROJECTED 10 YEAR CIP SIDEWALK PLAN
FY13

<u>SIDEWALK LOCATION</u>	<u>FROM</u>	<u>TO</u>	<u>LENGTH (FT)</u>	<u>PROPOSED CONSTRUCTION YEAR (FY)</u>	<u>COST</u>	<u>FY COST</u>
Boston Post Road	Tiffany Court	Clark Lane	2900	2013	\$ 118,436.00	
Boston Post Road	Willetts Avenue	Town Line	2100	2013	\$ 85,764.00	
Goshen Road	Shore Road	Great Neck Road	2900	2013	\$ 118,436.00	\$ 322,636.00
Rope Ferry Road	B Lane	Boston Post Road	4300	2014	\$ 180,858.00	
Rope Ferry Road	Avery Lane	Boston Post Rd	2600	2014	\$ 109,356.00	\$ 290,214.00
Pilgrim Road	Bloomingdale	Chapman Ave.	1200	2015	\$ 51,984.00	
Dayton Road	Route 85	House #126	5900	2015	\$ 255,588.00	\$ 307,572.00
Norman Street	House #35	Clark Lane	900	2016	\$ 40,158.00	
Vivian Street	Boston Post Road	Norman Street	1800	2016	\$ 80,316.00	
Cross Road	Boston Post Road	Marilyn	1500	2016	\$ 66,930.00	
Spithead Road	Boston Post Road	Mullen Hill	2000	2016	\$ 89,240.00	
Green Tree Drive	Vauxhall St. Ext.	Power Lines	800	2016	\$ 35,696.00	\$ 312,340.00
Shore Road	New Shore	Quarry	1600	2017	\$ 73,536.00	
Shore Road	Jordan Cove Road	New Shore Rd.	3500	2017	\$ 160,860.00	
Jordan Cove Road	Gardiners Wood Rd	Shore Road	2400	2017	\$ 110,304.00	\$ 344,700.00
Niantic River Road	House #151	Rope Ferry Road	5400	2018	\$ 255,636.00	\$ 255,636.00
Vauxhall St. Ext.	Old Barry Rd.	Greentree Drive	3200	2019	\$ 156,032.00	
Cross Road	Parkway North	Foster Road	3100	2019	\$ 151,156.00	\$ 307,188.00
Quarry Road	Shore Road	Great Neck Road	3100	2020	\$ 155,682.00	
Old Norwich Road	Town Line	Senkow Drive	2100	2020	\$ 105,462.00	
Sound View Dr.	Willetts Ave.	Longview Dr	550	2020	\$ 27,621.00	
Longview St.	Sound View Drive	Clement St.	1000	2020	\$ 50,220.00	\$ 338,985.00
Old Colchester Rd.	Old Norwich Rd.	House #88	3700	2021	\$ 191,401.00	
Scotch Cap West	Old Norwich Rd.	Route 32	1500	2021	\$ 77,595.00	
Scotch Cap East	Route 32	House #55	950	2021	\$ 49,143.50	\$ 318,139.50
Old Norwich Road	Hunts Brook	Roxwood Rd.	5500	2022	\$ 293,040.00	
Old Colchester Rd.	House #88	House #106	900	2022	\$ 47,952.00	\$ 340,992.00

*Assume a 3% increase per year in construction costs.

Year	3% annual increase per DOT
2013	40.84
2014	42.06
2015	43.32
2016	44.62
2017	45.96
2018	47.34
2019	48.76
2020	50.22
2021	51.73
2022	53.28

PROPOSED
BOSTON POST ROAD
SIDEWALK RENOVATION
(2900 LF.)



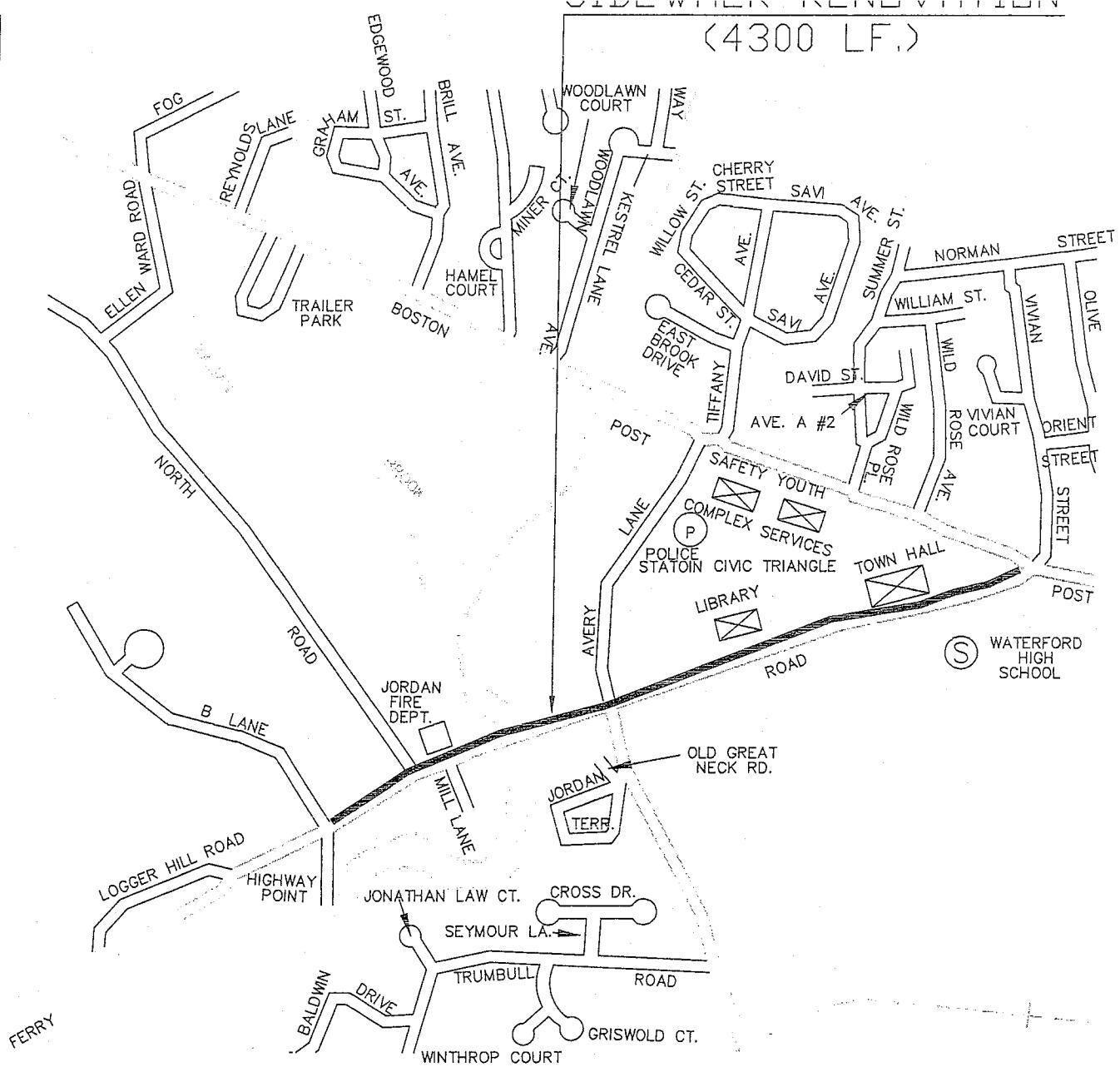
**SIDEWALK RENOVATION
BOSTON POST ROAD
TIFFANY AVENUE TO CLARK LANE
TOWN OF WATERFORD
PUBLIC WORKS DEPARTMENT**

PROPOSED
BOSTON POST ROAD
SIDEWALK RENOVATION
(2100 LF.)



SIDEWALK RENOVATION
BOSTON POST ROAD
WILLETS AVENUE TO TOWN LINE
TOWN OF WATERFORD
PUBLIC WORKS DEPARTMENT

PROPOSED
ROPE FERRY ROAD
SIDEWALK RENOVATION
(4300 LF.)



SIDEWALK RENOVATION
ROPE FERRY ROAD
B-LANE TO BOSTON POST ROAD
TOWN OF WATERFORD
PUBLIC WORKS DEPARTMENT

PROPOSED
PILGRIM ROAD
SIDEWALK RENOVATION
(1200 LF.)

CHAPMAN AVENUE

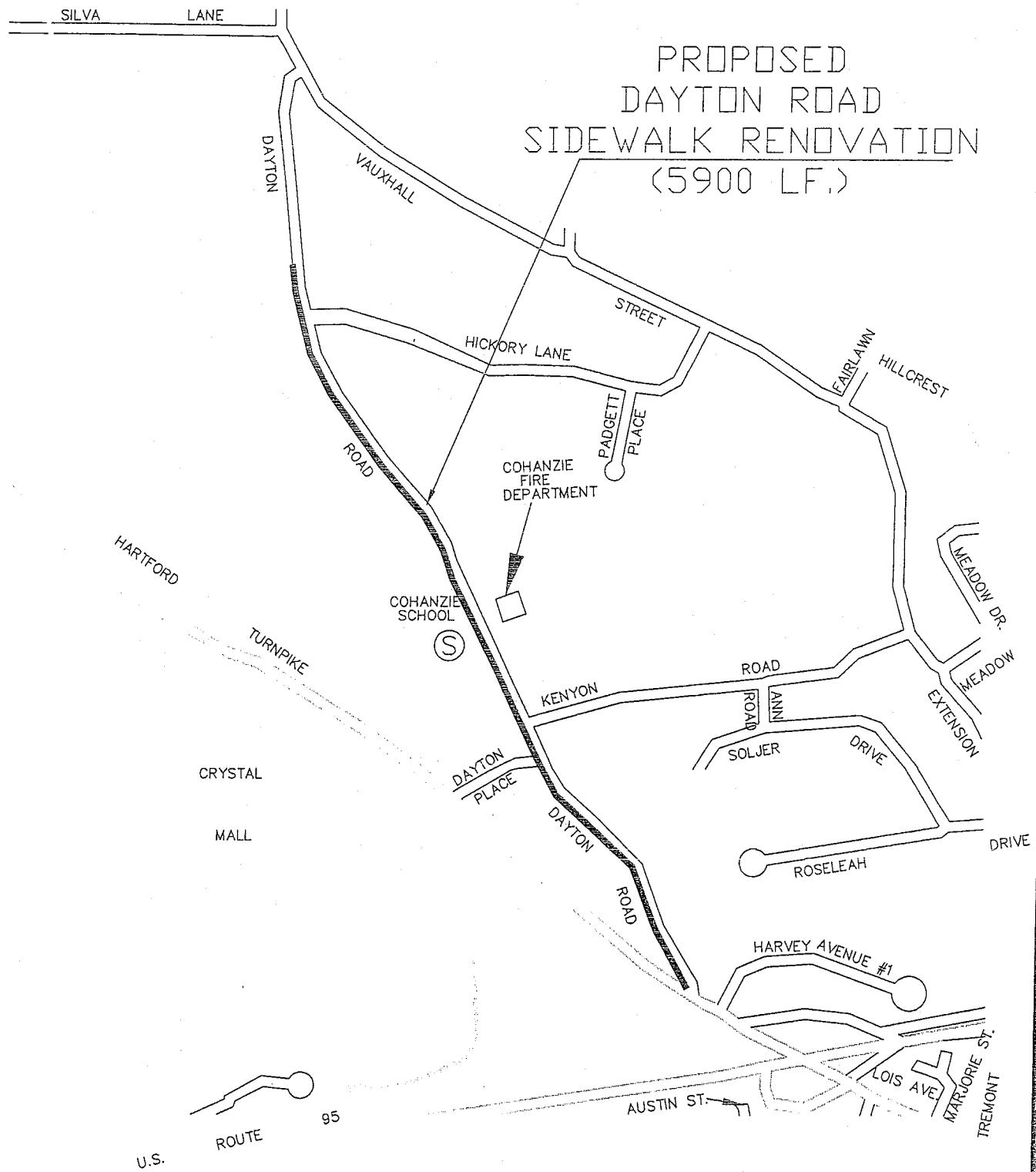
PILGRIM ROAD
EAST LAKE DRIVE

ARBORETUM

NEW LONDON

**SIDEWALK RENOVATION
PILGRIM ROAD
BLOOMINGDALE ROAD TO CHAPMAN AVENUE
TOWN OF WATERFORD
PUBLIC WORKS DEPARTMENT**

PROPOSED
DAYTON ROAD
SIDEWALK RENOVATION
(5900 LF.)



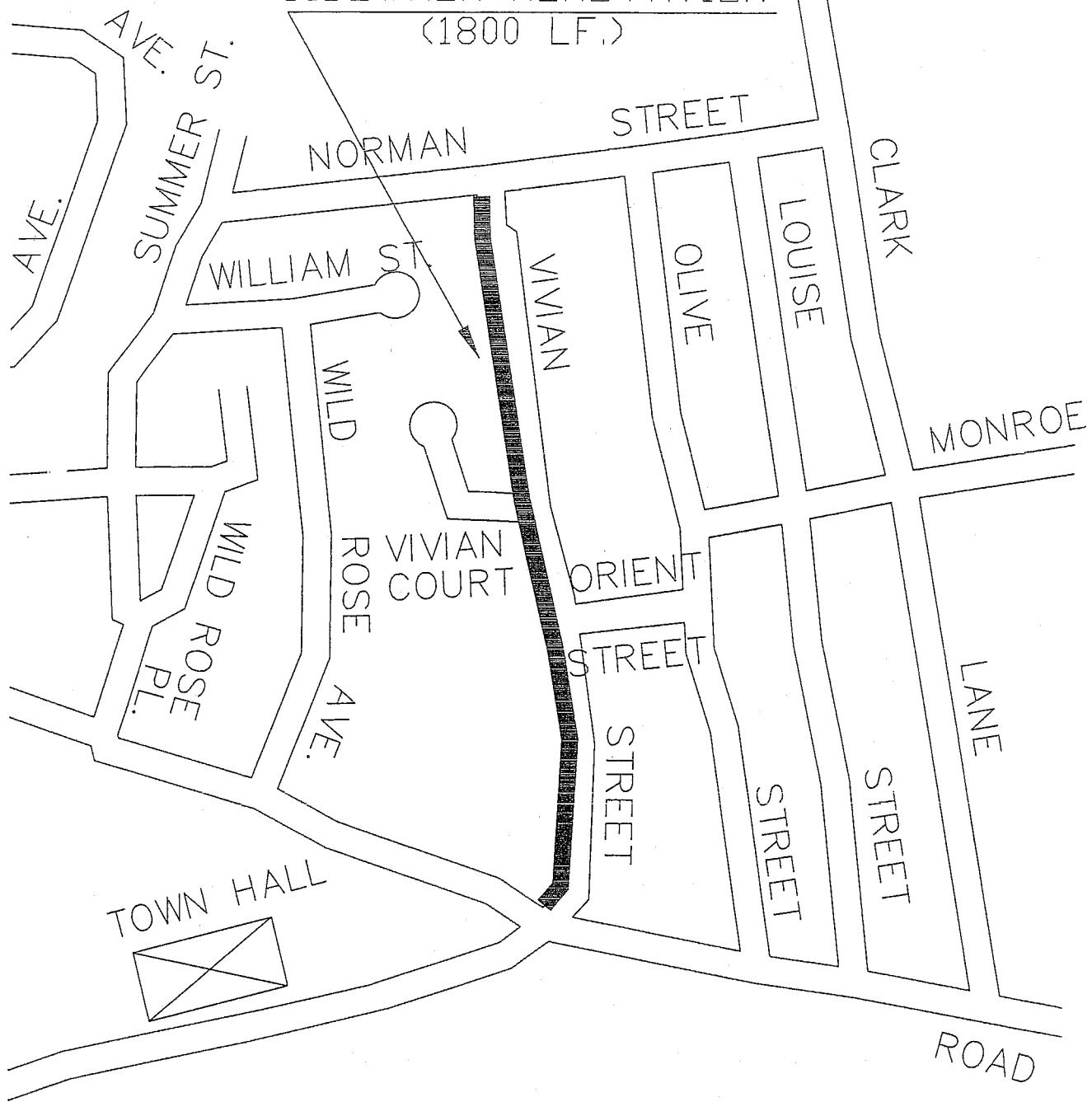
**SIDEWALK RENOVATION
DAYTON ROAD
ROUTE 85 TO HOUSE # 126
TOWN OF WATERFORD
PUBLIC WORKS DEPARTMENT**

PROPOSED
NORMAN STREET
SIDEWALK RENOVATION
900 LF.



**SIDEWALK RENOVATION
NORMAN STREET
FROM HOUSE # 35 TO CLARK LANE
TOWN OF WATERFORD
PUBLIC WORKS DEPARTMENT**

PROPOSED
VIVIAN STREET
SIDEWALK RENOVATION
(1800 LF.)



SIDEWALK RENOVATION
VIVIAN STREET
BOSTON POST ROAD TO NORMAN STREET
TOWN OF WATERFORD
PUBLIC WORKS DEPARTMENT

Town of Waterford CT
Sidewalk improvements Design
Project Budget
10/15/2010

Engineering Services

Task 1 Design	\$12,800
Task 2 Bidding	\$4,200
Task 3 - Construction Admin.	\$3,800
Task 4 - Resident Engineering	\$15,500
2011 Engineering Services	\$36,300

SECONDARY SIDEWALKS ARE MAINTAINED BY THE RESIDENT NOTE: PRIMARY SIDEWALKS ARE MAINTAINED BY THE TOWN										SIDEWALK MASTER PLAN SUMMARY DEPARTMENT OF PUBLIC WORKS				
LOCATION	FROM	TO	ROAD	WIDTH	RAMPS	LENGTH	CONTRACT	COMPLETED		DATE Original or Reconstructed	MAINTENANCE LIFE			
								(FT)	(FT)		(FT)	25 YEAR		
BOSTON POST ROAD ✓	Tiffany Court	Clark Lane	N	4.5'	21	2900		1965	6900	2	1990			
BOSTON POST ROAD L	Willerts Avenue	Town Line	N	4.5'	4	2100		1965	2100	2	1990		= FY 2011	
ROPE FERRY RD ✓	B Lane	Avery Lane	N	4.5'	3	1700		1965	1700	1	1990			
DAYTON ROAD (One-way) ✓	Route 85	Dayton Place	W	4.5'	3	1500		1965	2600	2	1990			
ROPE FERRY ROAD ✓	Avery Lane	Boston Post Road	N	4.5'	5	2600		1965	1500	2	1990			
DAYTON ROAD-PLACE ✓	House #1	House #126	W	5'	5	4400	14a	1979	4400	2	2004			
NORMAN STREET ✓	House #35	Clark Lane	S	4.5'	6	900		1979	900	2	2004			
VIVIAN STREET ✓	Boston Post Road	Norman Street	W	4.5'	2	1800		1979	1800	2	2004			
NORMAN STREET-(both side-concrete) ✓	Summer Street	House #35	S/N	4.5'	0	900		1979		900	2	2004		
CROSS ROAD ✓	Boston Post Road	Marilyn Road	E	4.5'	5	1500		1981	1500	1	2004			
SPITHEAD ROAD ✓	Boston Post Road	Mullen Hill Road	W	4.5'	6	2000	17d	1982	2000	1	2006			
PILGRIM ROAD ✓	Bloomingdale Road	Chapman Avenue	W	5'	0	1200		1983	1200	3	2008			
NIANTIC RIVER ROAD ✓	House #151	Rope Ferry Road	E	4.5'	31	5400		1984	5400	1	2009			
SHORE ROAD ✓	New Shore Road	Quarry Road	W	4.5'	4	1600	23b	1984	1600	2	2009			
JORDAN COVE ROAD ✓	Gardiners Wood Rd	Shore Road	N	4.5'	2	2400	23b	1984	2400	4	2009			
NORTH ROAD (concrete) ✓	House #10	Rope Ferry Road	E	4.5'	0	300		1984	300	5	2009			
SHORE ROAD ✓	Jordan Cove Road	New Shore Road	W	4.5'	10	3500	23/23b	1984	3500	3	2009			
DANIELS AVENUE ✓	South West School	Spithead Road	S	4.5'	8	1700		1985	1700	1	2010			
VAUXHALL ST. EXT. ✓	Old Barry Road	Greentree Drive	E	4.5'	7	3200	32a/33	1985	3200	3	2010			
CROSS ROAD ✓	Parkway North	Foster Road	E	4.5'	4	3100		1987	3100	3	2012			
GREENTREE DRIVE ✓	Vauxhall St. Ext.	Power Lines	S	4.5'	0	800	32	1987	800	3	2012			
COSHEN ROAD ✓	Shore Road	Great Neck Road	S	4.5'	1	2900		1989	2900	2	2014			
BUARRY ROAD ✓	Shore Road	Great Neck Road	N	4.5'	4	3100		1989	3100	3	2014			
OLD NORWICH ROAD ✓	Town Line	Senkow Drive	E	5'	3	2100	PW-24	1991	2100	3	2016			
LONGVIEW STREET ✓	Sound View Drive	Clement Street	S	4.5'	3	1000	Long. Condo	1991	1000	4	2016			
SOUND VIEW DRIVE ✓	Willets Avenue	Longview Avenue	W	4.5'	0	550	Long. Condo	1991	550	4	2016			
OLD COLCHESTER ROAD ✓	Old Norwich Road	House #88	W	5'	6	3700	40	1992	3700	3	2017			
OLD NORWICH ROAD ✓	Hunts Brook	Roxwood Road	E	5'	11	5500	40	1992	5500	3	2017			
SCOTCH CAP EAST ✓	Route 32	House #55	S	4.5'	5	950	40	1992	950	3	2017			
SCOTCH CAP WEST ✓	Old Norwich Road	Route 32	S	4.5'	5	1500	40	1992	1500	3	2017			
BOSTON POST ROAD ✓	Oswegachie Road	Cross Road	S	4.5'	9	9150	42a	1992	9150	4	2017			
CLARK PLACE ✓	House #4	Cul De Sac	E	4.5'	0	400	40	1992		400	3	2017	\$602,000.00	
STONEHEIGHTS DRIVE ✓	Boston Post Road	Mullen Hill Road	E	5'	0	2100	Stone Condo	1992		2100	3	2017		
OLD COLCHESTER ROAD ✓	House #88	House #106	W	5'	0	900	59	1993	900	4	2018			
CHAPMAN AVENUE ✓	Vauxhall St. Ext.	Pilgrim Road	S	5'	1	1800	37	1994	1800	3	2019			
GREAT NECK ROAD ✓	Waterford Beach	Glenwood Ave. Ext.	S	4.5'	6	2600	51	1994	2600	4	2019			
NIANTIC RIVER ROAD ✓	Boston Post Road	House #199	E	4.5'	8	4500	42	1994	4500	4	2019			
SHORE ROAD ✓	Magon Point Road	Great Neck Road	S	4.5'	5	2500	55	1994	2500	4	2019			
AVENUE B ✓	Niantic River Road	Fourth Street	E	4.5'	3	430	PW-26	1995		730	4	2020		
RIVER STREET ✓	Second Street	Dock	N	4.5'	1	260	PW-26	1995		260	4	2020		
SECOND STREET ✓	River Street	Avenue B	S	4.5'	1	180	PW-26	1995		180	4	2020		
GREAT NECK ROAD ✓	Trumbull Road	Shore Road	W	4.5'	11	11800	PW-27	1996	11800	4	2021			
PENNINSULAR AVENUE ✓	Ridgewood Avenue	Town Line	S	4.5'	1	900	51a	1996	900	4	2021			
RIDGEWOOD AVENUE ✓	Great Neck Road	Greenfield Street	E	4.5'	12	2800	51a	1996	2800	4	2021			
BIRCH STREET ✓	Forest Street	Shore Drive	S	4.5'	3	650	51a	1996		650	4	2021		
GREENFIELD STREET ✓	Perinslilar Avenue	Ridgewood Avenue	W	4.5'	2	550	51a	1996		550	4	2021		
LAUREL STREET ✓	Ridgewood Avenue	Shore Drive	S	4.5'	1	650	51a	1996		650	4	2021		
WOODBINE STREET ✓	Ridgewood Avenue	Shore Drive	N	4.5'	1	600	51a	1996		600	4	2021		
ROPE FERRY RD ✓	Gardiners Wood Rd	Gallup Lane	N	4.5'	3	2900	58	1997	2900	4	2022		\$556,000.00	

CAPITAL IMPROVEMENT PLAN
PROJECT DESCRIPTION FORM

A) AGENCY: PUBLIC WORKS DEPARTMENT
B) PROJECT NAME: PARKWAY NORTH CONNECTOR- HIGHWAY CONSTRUCTION & IMPROVEMENTS
C) CONTACT PERSON: RONALD R. CUSANO, DIRECTOR, PUBLIC WORKS

D) DESCRIPTION:

- a) Describe the type, purpose, and anticipated accomplishments of the project.
- b) Indicate the Progress to date on the project.
- c) List other projects it is coordinated with whether in your Department or with other Departments.
- d) Describe the affect the project will have on your annual Department expenditures:
- e) (Attach Conceptual Plan)

E) PROJECT CATEGORY:

Facilities Improvements	Apparatus, Vehicle & Equipment Replacement	
Building Improvements	Highway Construction & Improvements	X
Land Acquisition	Sidewalks & Trails	
Road Reconstruction-Sewer Extensions	Schools	
Water System	Sewerage System	

Funds in the amount of \$300,000 have been placed into an escrow account entitled the "Parkway North Extension Escrow Fund" from Wal-Mart Real Estate Business Trust for the funding of the connecting road extending from the cul-de-sac on Parkway North to a point to the west of the Crystal Mall access Drive. The current funds are being used for the design of the proposed road. This account is currently being administered by the Planning Director.

The proposed road will be approximately 3,300 lf long with a paved width of 40 feet. An engineering consultant has been retained for the design of the road. The Town has received correspondence from the State and indicated that they would be willing to deed a corridor to the Town at no cost (see attached). I am therefore not including funds for this.

Currently preliminary plans are being drawn and a traffic study is being completed. Efforts are being made to increase turning movements at new Rte 85/Parkwy North Connector. The total anticipated project cost is as follows without utilities:

PROJECT COSTS

Design & Const. Admin.	\$ 150,700
ROW	<u>NA</u>
Total	\$ 150,700
Construction	\$ 2,650,000
Inspection	3,500 lf @ \$ 12/lf = \$ 42,000
Total	\$ 2,692,000

FUNDS REQUIRED

Total Project Cost	\$2,696,200
Funds Available through escrow	<u>300,000</u>
Funds Required	\$2,392,000

It is hereby requested that \$2,392,000 be designated for the above referenced project for roadwork only.

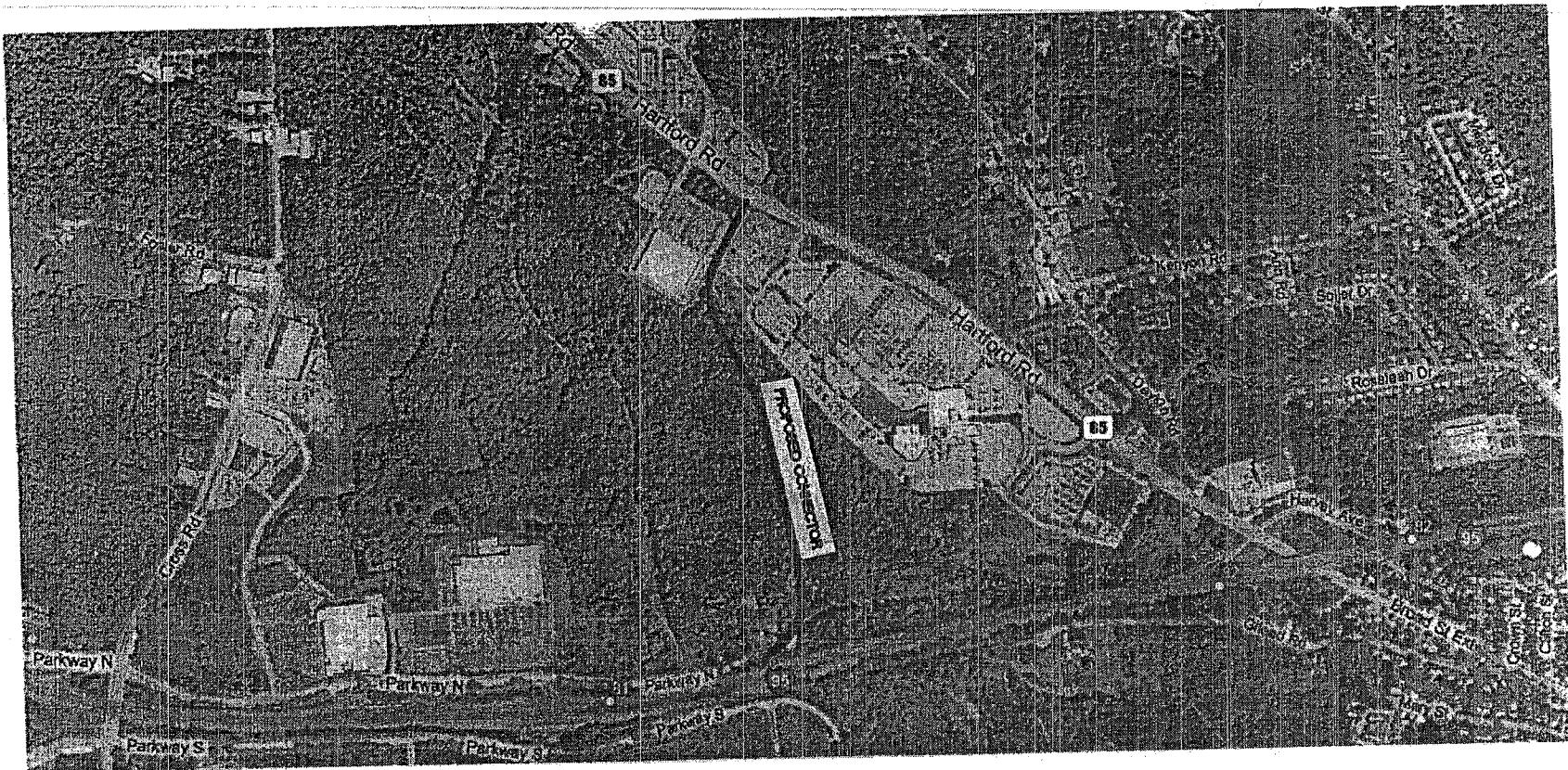
Close, Jensen and Miller, P.C.
Wethersfield, Connecticut

DATE: 11/20/08

CONSTRUCTION COST ESTIMATE
PRELIMINARY DESIGN
PARKWAY NORTH CONNECTOR ROAD
WATERFORD, CONNECTICUT

ITEM DESCRIPTION	UNIT	QUANTITY	PRICE	AMOUNT
Earth Excavation	C.Y.	51,000	\$ 15.00	\$ 765,000.00
Rock Excavation (10%)	C.Y.	5,000	\$ 40.00	\$ 200,000.00
Processed Aggregate	C.Y.	2,500	\$ 35.00	\$ 87,500.00
Trench Excavation	C.Y.	600	\$ 25.00	\$ 15,000.00
Formation of Subgrade	S.Y.	12,800	\$ 1.00	\$ 12,800.00
Sedimentation Control	L.F.	2,400	\$ 4.00	\$ 9,600.00
Bituminous Concrete	Ton	2,600	\$ 125.00	\$ 325,000.00
Catch Basins	Ea.	8	\$ 2,200.00	\$ 17,600.00
Catch Basins Double Grate	Ea.	2	\$ 4,500.00	\$ 9,000.00
12" Pipe	L.F.	220	\$ 60.00	\$ 13,200.00
15" Pipe	L.F.	260	\$ 70.00	\$ 18,200.00
18" Pipe	L.F.	120	\$ 75.00	\$ 9,000.00
24" Pipe	L.F.	140	\$ 85.00	\$ 11,900.00
Culvert Ends	Ea.	6	\$ 400.00	\$ 2,400.00
Metal Beam Rail	L.F.	2,400	\$ 28.00	\$ 67,200.00
Rail Anchorages	Ea.	6	\$ 1,200.00	\$ 7,200.00
Turf Establishment	S.Y.	26,000	\$ 2.00	\$ 52,000.00
Erosion Control Lining	S.Y.	1,600	\$ 8.00	\$ 12,800.00
Pavement Markings	L.F.	6,600	\$ 2.00	\$ 13,200.00

SUBTOTAL = \$ 1,648,600.00
+ 20% **CONTINGENCY** = \$ 329,720.00
+5% **CLEARING & GRUBBING** = \$ 82,430.00
MOBILIZATION = \$ 120,000.00
CONSTRUCTION STAKING = \$ 15,000.00
SUBTOTAL = \$ 2,195,750.00
+20% **INCIDENTALS** = \$ 439,150.00
GRAND TOTAL = \$ 2,634,900.00
SAY \$ 2,650,000.00

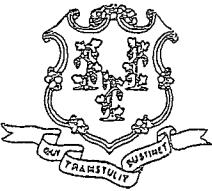


LOCATION MAP
FOR PROPOSED
PARKWAY NORTH CONNECTOR
WATERFORD, CONNECTICUT

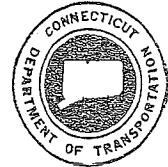
C.
J.
M.

Close, Jensen & Miller, P.C.
1137 Silas Deane Highway, Wethersfield, Ct.06109
Tel. 860.563.9375 Fax 860.721.1802

FIGURE -1



STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION



2800 BERLIN TURNPIKE, P.O. BOX 317546
NEWINGTON, CONNECTICUT 06131-7546

Phone:
(860) 594-2463

June 27, 2007

Ronald R. Cusano, P.E., L.S.
Director of Public Works
15 Rope Ferry Road
Waterford, Connecticut 06385-2886

Dear Mr. Cusano:

Subject: State Land – Waterford
File No. 152-55-33G

This will inform you that the Department of Transportation (Department) concurs with the conceptual design for the proposed roadway in the Town of Waterford (Town) connecting Route 85 and Parkway North.

The subject property has been identified as a potential wetland mitigation site associated with the planned widening and interchange improvements to Interstate 95 (I-95) as identified in the final report of the "I-95 Corridor Branford to Rhode Island Feasibility Study," dated December 2004. Therefore, the specific alignment of the proposed connector road must be determined in coordination with the Department's Office of Intermodal and Environmental Planning. In addition, no development or access would be allowed off of the connector roadway as part of any future agreement. The Department shall also reserve the right to realign the southern portion of the proposed connector roadway to accommodate the future widening and interchange improvements of I-95.

The Office of Rights of Way will table any agreements or conveyance of property rights until preliminary, semi-final, and final design plans showing the final roadway configuration are reviewed by the Department. The concept plan shows the eastern end of the proposed roadway will intersect with Route 85 approximately 450 feet north of the existing Crystal Mall Middle Drive. A revision to the State Traffic Commission (STC) Certificate for Crystal Mall may be required due to the proposed revision to the existing approved site drive location. In addition, it should be noted that a traffic signal may be needed at this location.

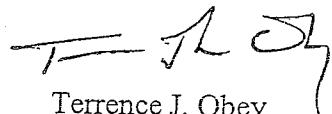
Ronald R. Cusano, P.E., L.S.
File No. 152-55-33G

-2-

June 27, 2007

If you have any questions in the interim, please do not hesitate to contact
Ms. Amy N. Martinez, Supervising Property Agent, at (860) 594-2391.

Sincerely,



Terrence J. Obey
Division Chief
Property Management Division
Office of Rights of Way

C

**CAPITAL IMPROVEMENT PLAN
PROJECT DESCRIPTION FORM**

A) AGENCY: **PUBLIC WORKS DEPARTMENT**
B) PROJECT NAME: **CURBING REPLACEMENT**
C) CONTACT PERSON: **RONALD R. CUSANO, DIRECTOR, PUBLIC WORKS**

D) DESCRIPTION:

- a) Describe the type, purpose, and anticipated accomplishments of the project.
- b) Indicate the Progress to date on the project.
- c) List other projects it is coordinated with whether in your Department or with other Departments.
- d) Describe the affect the project will have on your annual Department expenditures:
- e) (Attach Conceptual Plan)

E) PROJECT CATEGORY:

Facilities Improvements	Apparatus, Vehicle & Equipment Replacement	
Building Improvements	Highway Construction & Improvements	X
Land Acquisition	Sidewalks & Trails	
Road Reconstruction-Sewer Extensions	Schools	
Water System	Sewerage System	

This project includes the removal and replacement of deteriorated concrete curb that currently exists along many of our roadways. Concrete curb had been a roadway standard and was installed as part of our Cooperative Road Reconstruction Program which began in the early 70's. I am suggesting that concrete curbing be replaced on Old Barry Road, which was installed under Cooperative Sewer/Roadway Reconstruction Project 33 in 1987. This would make this curbing 24 years old. The remainder of that subdivision would be replaced in the next fiscal year.

Rather than replace the full depth of curbing, it will be more cost-effective to break the curbing off at road level and install slip-formed 4" park curbing.

The cost for this replacement:

Old Barry Road: 3320 LF x \$20.95/LF = \$69,554.00

Other curbing that has been deteriorated and in need of replacement is in the Pleasure Beach and Millstone Point area, which may be proposed in the future.

CONCRETE CURBING REPLACEMENT PROGRAM

<u>LOCATION</u>	<u>LENGTH</u>	<u>x2 TOTAL LENGTH</u>	<u>UNIT PRICE</u>	<u>TOTAL COST</u>
Old Barry Road	1660	3320	\$20.95	\$69,554.00
Faulkner Drive	1685	3370	\$20.95	\$70,601.50
Jodry	978	1956	\$20.95	\$40,978.20

Ron Cusano

From: Kimberly [kim@curb-ne.com]
Sent: Thursday, October 27, 2011 4:50 PM
To: Ron Cusano
Subject: Budget numbers for curbing

Hi Ron,

This should cover everything we discussed.

For following information is for budgetary purposes only.

500' minimum per scheduled visit.

6" standard curb \$6.25 per LF

4" park curb \$5.95 per LF

Removal and disposal of curb \$15.00 per LF

Add an additional .25 cents if prevailing wage rates apply. This addition may increase on all curbing installed after 6/30/2012 based on annual adjusted 2012 prevailing rates published by CTDOL.

The removal of existing curb will be hammered/broken off flush with the pavement.

Any patching or repairs are the responsibility of the municipality.

Backfill will be the responsibility of the municipality.

Traffic control will be the responsibility of the municipality.

Protection of the curb after our crew leaves is the responsibility of the municipality. Curbing should be protected for 24 to 48 hours.

Any damage due to negligent drivers or vandalism during that time will not be covered under our warranty.

Call me a call if you need additional information or pricing.

Kimberly Crumb
C&C Concrete Curb, Inc
PO Box 2873
New Britain, CT 06050
Tel: 860.827.8637
Fax: 860.827.0351
Mobile: 860.970.2234

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CAPITAL IMPROVEMENT PLAN: PROJECT CONSOLIDATION FORM-FY2013-2017

DEPARTMENT/AGENCY: Waterford Recreation & Parks Commission

PROJECT NAME: IN ORDER OF DEPT. PRIORITY	SOURCE	FY-2013	FY-2014	FY-2015	FY-2016	FY-2017	TOTAL FY 2013-2017
1 Waterford Beach Park Accessible Restroom	18	\$23,817					\$23,817
2 Replacement of Causeway Bathroom at Waterford Beach Park	46		\$61,000				\$61,000
3 Leary Park Irrigation System	4	\$26,000	\$24,800	\$11,500			\$62,300
4 Stenger Farm Park Restrooms	46			\$30,000			\$30,000
5							
6							
7							
8							
9							
TOTAL		\$49,817	\$85,800	\$41,500			\$177,117

INDEX TO FUNDING SOURCES,

1= CURRENT YEAR CAPITAL IMPROVEMENTS

2 = OPERATING BUDGETS,

3 = WASTE WATER BUDGET/SEWER CAPITAL MAINTENANCE FUND

4 = TRANSFER TO CAPITAL & NONRECURRING.

5 = SHORT AND LONG TERM DEBT FINANCING

6 = LOCAL CIP & OTHER GRANTS

7 = DESIGNATE FUNDS FROM UNDESIGNATED PROJECTS/CNR;

8 = FUND FROM EXISTING DESIGNATIONS CNR

FUND BALANCE

CAPITAL IMPROVEMENT PLAN
PROJECT DESCRIPTION FORM

- A) AGENCY: Waterford Recreation & Parks Commission**
- B) PROJECT NAME: Waterford Beach Park Accessible Restroom**
- C) CONTACT PERSON: Brian W. Flaherty**
- D) DEPARTMENT'S PRIORITY: # 1**

E) DESCRIPTION:

- a) Describe the type, purpose, and anticipated accomplishments of the project.**

The restroom building at the pavilion area is not handicap accessible. Each year, we are required to rent an accessible porta-john to provide access for those that need it. For the 2011 season, the rental cost totaled approximately \$ 1,000. Our intent is to provide accessibility for those park users that have disabilities. (Requirement of the Americans with Disabilities Act, Title II).

In order to be compliant, we plan on adding an accessible restroom addition to the existing building. We will better serve our residents and visitors with this project, as well as be in compliance with federal regulations.

Waterford Beach Park Statistics for the summer of 2011 (June through September 9am-5pm): Attendance: 30,000 Revenue: \$71,000

b) Indicate the progress to date on the project.

Funds in the amount of \$11,813 (20537-57342) currently are designated for this project. Plans have been completed by The Winthrop Group. Our request of \$23,187 will make the total project cost total \$35,000.

As stated below, a sewage connection to the O'Neill Theater is planned in the near future. This would be an ideal time to renovate our existing restroom facilities and connect to the town's sewage system. The existing septic tanks and leach fields reliability are in question as well, and are located in an environmentally sensitive coastal area.

c) List other projects it is coordinated with whether in your Department or with other Departments.

The Eugene O'Neill Theater Center plans on updating to a sewage connection to their facility as part of their proposed improvements to the Theater Center. They requested \$170,000 for sewage connection at the O'Neill & Waterford Beach Park. This should allow for the connection of the park wastewater systems to the O'Neill's new connection in the future. Those costs are not included here. The feasibility of this connection has been reviewed in the fall of 2011 with the O'Neill staff, their Architect, Recreation & Parks staff, Utility Commission staff, and town Planning & Building officials.

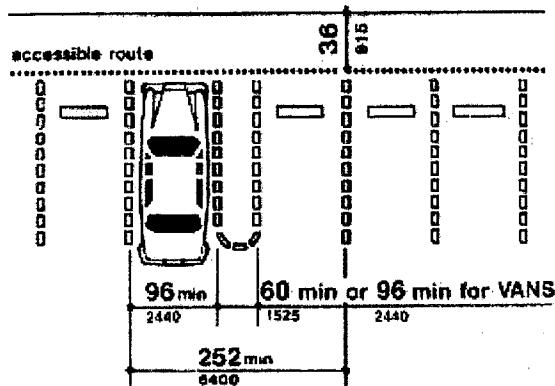
d) Describe the affect the project will have on your annual Department expenditures:

Estimated utility costs: \$250 Janitorial/paper supplies: \$100
Miscellaneous repairs: \$50 TOTAL: \$400

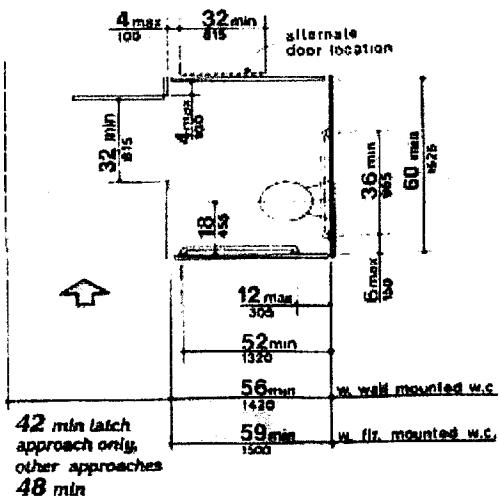
- e) Attach plan, estimate, service area map and/or other support documentation.

Complete blueprint available upon request. Contractor quote of project estimated at \$35,000. Recreation & Parks Maintenance staff will be involved in parking lot installation, so labor savings will occur.

WBP Accessible Restroom page 2 of 2



Current Restroom 1



CAPITAL IMPROVEMENT PLAN
PROJECT DESCRIPTION FORM

A) AGENCY: Waterford Recreation & Parks Commission

**B) PROJECT NAME: Replacement of Causeway Bathroom at
Waterford Beach Park**

C) CONTACT PERSON: Brian W. Flaherty

D) DEPARTMENT'S PRIORITY: # 2

E) DESCRIPTION:

a) Describe the type, purpose, and anticipated accomplishments of the project.

The causeway bathroom building is showing signs of its age (40 + years) such as structural issues. It does not have electricity and although repairs have been made over the years, it is in need of replacement. It also lacks handicap access. Each year, we are required to rent an accessible porta-john to provide access for those that need it. For the 2011 season, the rental cost totaled approximately \$ 1,000. Our intent is to provide accessibility for those park users that have disabilities. (Requirement of the Americans with Disabilities Act, Title II).

We will better serve our residents and visitors with this project, as well as be in compliance with federal regulations.

Waterford Beach Park Statistics for the summer of 2011 (June through September 9am-5pm): Attendance: 30,000 Revenue: \$71,000

Building Cost \$51,000. Crane rental/setting: \$4,200 Site work/utilities: \$4,800 TL Cost \$61,000 (2% contingency).

b) Indicate the progress to date on the project.

As stated below, a sewage connection to the O'Neill Theater is planned in the near future. This would be an ideal time to renovate our existing restroom facilities and connect to the town's sewage system. The existing septic tanks and leach fields reliability are in question as well, and are located in an environmentally sensitive coastal area.

Our staff has been researching precast concrete restroom buildings designed for park settings. They are durable, long lasting, and less expensive than traditional restroom building construction. Traditional construction options have been reviewed as well, but the cost escalates with that option. We plan on applying for grant funds through the state of Connecticut's Small Town Economic Assistance program (STEAP) in the summer of 2012. If grant funds are obtained, we will consider the traditional construction option as well.

c) List other projects it is coordinated with whether in your Department or with other Departments.

The Eugene O'Neill Theater Center plans on updating to a sewage connection to their facility as part of their proposed improvements to the Theater Center. They requested \$170,000 for sewage connection at the O'Neill & Waterford Beach Park. This should allow for the connection of the park wastewater systems to the O'Neill's new connection in the future. Those costs are not included here. The feasibility of this connection has been reviewed in the fall of 2011 with the O'Neill staff, their Architect, Recreation & Parks staff, Utility Commission staff, and town Planning & Building officials.

d) Describe the affect the project will have on your annual Department expenditures:

Estimated utility costs: \$ 600 Janitorial/paper supplies: \$200
Miscellaneous repairs: \$ 100 TOTAL: \$900

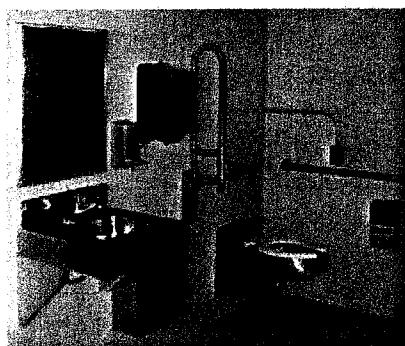
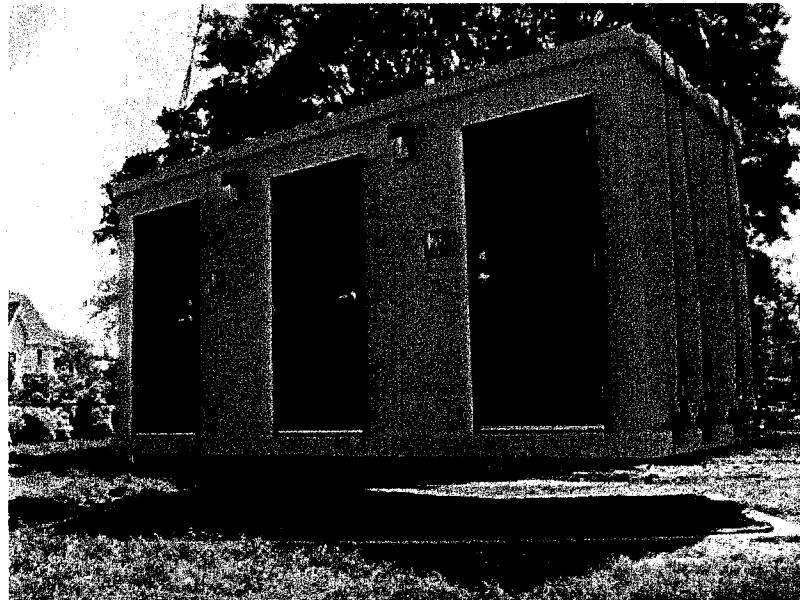
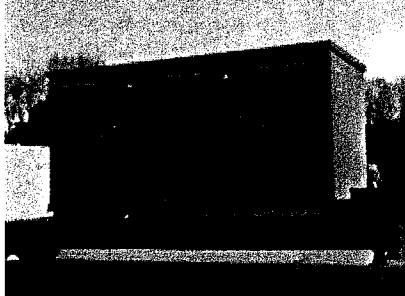
e) Attach plan, estimate, service area map and/or other support documentation.

Precast Concrete Restroom Buildings



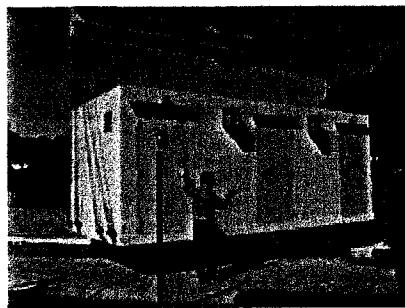
Unicon restroom buildings and concession stand buildings are manufactured by United Concrete Products Inc. in Yalesville, Connecticut and delivered to New England and Northeast.

These buildings offer the best long-range solution for parks and recreation departments, because they offer the permanence and durability of precast concrete, while retaining the flexibility for future relocation to accommodate any site changes that might arise.



Unicon Precast Concrete Restroom Buildings

- DESIGN: International Building Code (2000 Edition)
- CONSTRUCTION: 5000 psi steel-reinforced precast concrete. 4" walls, 6" floor and 4 1/2 " roof.
- DOORS: 18 gauge galvanized steel doors, 16 gauge galvanized steel frames, stainless steel hinges, ADA lockset, indicator dead-bolt lock, door closer, threshold.



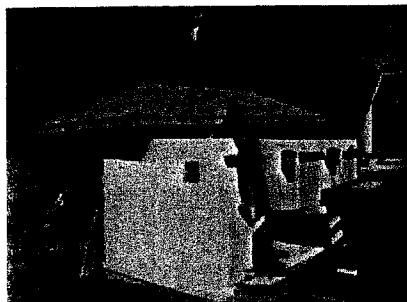
Every building is manufactured according to meet customers' specific needs. Buildings are designed to meet A.D.A. requirements



and building codes.

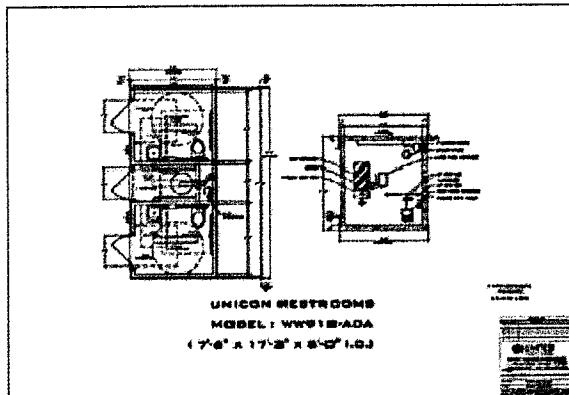
Unicon prefabricated restrooms offer a variety of options in terms of design, fixtures and finishes. Stainless steel fixtures are preferred by the customers seeking for maximum vandal-resistance.

Concession buildings and storage buildings are also available to combine with restroom buildings offering a multi-use building solution.



[Precast Buildings Main Page](#)
[Hazmat Storage](#)
[Telecommunication Shelters](#)

Please, call 1-800-234-3119 Ext.17 Mr. Tolga Oztoprak for detailed information, specifications and drawings.



Standard and Custom Designs Available!



Causeway bathroom



173 Church Street
Yalesville, CT 06492
Toll Free: (800) 234-3119
Tel: (203) 269-3119
Fax: (203) 265-4941

QUOTATION

Nov 11, 2011

**Town of Waterford
Parks And Recreation Department
Attn Brian Flaherty**

**Re: Waterford Park Department
Waterford CT
Proposal # 6093**

I am pleased to provide you with a Quotation for the Pre-Cast Concrete Restroom structure for the above listed project.

- The Pre-cast building structure will be shipped as complete one unit and will be installed in the field with our personnel on a customer supplied compacted 10" stone base with 4" of pea stone gravel.
- Penetrations to be provided as indicated on drawings for incoming water supply, sewer and main electrical connections.

Restroom Structure Will Consist Of

Interior Building Dimensions: 7'-6" x 17'-2" x 8'-0"

Building will include:

Interior Finish of building:

- Walls and Ceiling: Epoxy Paint.
- Floor: Epoxy paint (slate gray floor with non skid sand additive).

Exterior of building:

- Exterior to be coated with a Thorocoat finish (final color to be determined by owner).

Doors:

- (3) Three CECO 3'-0" x 7'-0" 18 gauge galvanized steel doors w/ SS hinges & contacts. Color to be determined by owner.
- (3) Three CECO 3'-0" x 7'-0" 16 gauge galvanized steel door frames.
- Cylindrical Lockset for each door.
- Weatherstrip & threshold at each door.
- (2) Two closures (@ restroom doors) & (1) One hold open chain (@ utility chase)
- (2) Two deadbolts w/ occupancy indicator.

Electrical:

- (1) One 100 Amp electrical panel.
 - (3) Three vapor tight 4' interior light fixtures w/ motion sensor on/off.
-

November 10, 2011

- (1) One 120V 20A Receptacle.
- Surface mounted EMT.
- (2) Two light switches.
- (2) Two call for aid devices.
- (1) One exterior lights w/ photocell & time clock.
- (3) Three Combination LED Exit Sign / Emergency Lighting Unit
- (2) Two exhaust fans & vandal proof wall vent.
- Door contact and strikes wired to common junction box each building.

Plumbing:

- (1) One tankless waterheater.
- (2) Two stainless steel sinks w/ mechanical metering auto shut-off hot & cold faucets.
- (2) Two stainless steel toilets w/ integral seats and concealed push button flushometer.

Accessories:

- (2) Two center pull paper towel dispensers.
- (2) Two stainless steel soap dispensers.
- (2) Two stainless steel waste receptacles.
- (2) Two stainless steel toilet seat covers.
- (2) Two stainless steel toilet paper dispensers.
- Restroom door signage (handicap Male & Female).

EXCLUSIONS:

- Site preparation by others.
- ALL items specifically not listed above.
- Site electrical & wiring to restroom building.
- 3rd party testing.
- Disconnect Switch.
- Electric meter.
- ALL FIELD Connections of all plumbing, mechanical & electrical to main supplies by others.

Total Price:\$ 51,000.00 (Excludes Tax)

ADDER: Add for ceiling mount 3kw electric heater\$1986.00

ADDER: Add for 1-day crane rental service to set Pre-cast building.....\$4200.00

ADDER: Add for Sargent mortise lock sets with electric strikes and timer option.. \$2300.00

CAPITAL IMPROVEMENT PLAN
PROJECT DESCRIPTION FORM

A) AGENCY: Waterford Recreation & Parks Commission

B) PROJECT NAME: Leary Park Irrigation System

C) CONTACT PERSON: Brian W. Flaherty, Director

D) DEPARTMENT'S PRIORITY: #3

E) DESCRIPTION:

a) Describe the type, purpose, and anticipated accomplishments of the project.

The scope of this project will be to install a basic irrigation system for the baseball, softball, and soccer fields in stages. The first stage would be establishing the required connection to the existing water system by a pit tap and installation of a back flow preventer. It would also include running a main line to the soccer field and installation of the irrigation lines, sprinkler heads, control unit and water sensors. All parts, labor, and training included. The soccer field is located at the far end of the park and is exposed to full sun. No water is currently available. It is difficult for turf to grow here. **Irrigation is vital for healthy turf. It allows measured amounts of water to sustain the turf.** Accomplishments to include:

- **Our current amount of practice and sport fields in town is not meeting demand. This can be shown by new sports requiring fields such as the emergence of lacrosse in recent years, as well as the loss of some field usage at Oswegatchie School.**
- **It will greatly enhance the playability and quality of these fields. This will relieve some of the pressure for field demands in town since the soccer field will be able to be used more frequently as a multi-purpose field.**
- **Healthy turf requires less fertilizer and weed/pest control.**
- **Infield surfaces such as clay and stone dust also benefit by way of dust control.**

Several studies have shown quality parks improve our health, economy, property values, and create strong communities. That in itself creates a large return on investment.

Currently, These fields host WHS Freshman & JV Baseball, Babe Ruth Baseball, American Legion Baseball, Senior League Girls Softball, Little League Baseball, Youth Soccer, general public use, and various public rentals. (1000+ uses, 8050 hours, 2010.)

Updated project cost as discussed with Maxum enterprises 11/2011:

Irrigation system parts & labor: \$26,000 (1st stage Soccer Field)

Second Stage Baseball Field: \$ 24,800 (2 % contingency)

Third Stage Softball Field: \$11,500 (2% contingency)

b) Indicate the Progress to date on the project.

Designs of the irrigation systems have already been received and reviewed, and are appropriate for public turf fields (Long term dependability and ease of maintenance). Flow tests have been completed, and the pressure at the feed is adequate.

c) List other projects it is coordinated with whether in your Department or other departments.

d) Describe the impact the project will have on your annual department expenditures.

Additional annual utilities estimated at \$1,500. A large amount of labor costs will be realized since currently, our maintainers only have the option of manually watering the turf and infields at this time. That savings alone will pay for the utilities. These systems will be automated and have rain/weather sensors, so utility cost savings will occur, and water resources will not be wasted. Less fertilizer and weed control will be required which will provide cost savings.

e) Attach plan, estimate, service area map and/or other support documentation

Complete blueprints available upon request.

Maxum

November 9, 2011

Brian Flaherty, Director
Recreation and Parks
Town of Waterford
15 Rope Ferry Road
Waterford, CT 06385-2886

Dear Brian,

After reviewing the ball fields that we visited, we are pleased to offer you the following quotations for irrigation installation at the different sites.

Vauxhill Road Park

1. Large baseball field - \$24,300.00
2. Softball Field - \$11,200.00
3. Lower Soccer Field - \$15,600.00
4. Mainline, controller, and cabling - \$7,800.00
5. Pit tap and backflow - \$2,850.00

Small Ball Field by Elementary School - \$6,850.00

Ball Field by Town Hall - \$10,600.00

Please feel free to contact us with any questions you may have. Thanks.

Regards,

Douglas R Philipp, President

Maxum Enterprises LLC --- 137 Parks Road, Preston, CT 06365 --- CT License #208812
Phone 860-376-4630 --- Fax 860-376-3603 --- www.maxumenterprises@hotmail.com

CAPITAL IMPROVEMENT PLAN **PROJECT DESCRIPTION FORM**

- A) AGENCY: Waterford Recreation & Parks Commission**
B) PROJECT NAME: Stenger Farm Park Restroom Facility
C) CONTACT PERSON: Brian W. Flaherty, Director
D) DEPARTMENT'S PRIORITY: #4
E) DESCRIPTION:

- a) Describe the type, purpose, and anticipated accomplishments of the project.**

Over two decades ago, residents on the Stenger Farm Advisory Committee recommended a restroom facility for this property. From a park planning perspective, a park this size (100+ acres) and the amount of visitors justifies having a restroom facility.

The attached plans illustrate a pre-cast concrete restroom facility which will meet all applicable building codes, as well as ADA accessibility requirements. The structure is installed on a compacted stone base which eliminates the need for a concrete foundation & slab. It is designed to be vandal resistant and requires minimal maintenance. Accomplishments would include:

- Stenger would be consistent with our other large parks that have bathroom facilities.
- Usage (hiking, fishing, skiing, dog walking) increases at this park each year. Requests for basic bathroom facilities are expected to grow, so residents will be pleased with this addition.
- Annual events will benefit such as the Stenger Cross Country Invitational, Fishing Derby, and Easter Egg Hunt.
- Clark Lane Middle School athletic, physical education, and outdoor education programs will benefit.
- Scout groups in town use Stenger extensively. They would benefit.
- Revenue could be generated by renting the upper field area for group picnics and special events. Estimated annual revenues could total \$4,000.

Stenger was acquired with the assistance of federal funds from the *Land and Water Conservation Program*. They have indicated support for this addition and indicated federal funds may be available for this project. We will also be applying for the Small Town Economic Assistance Program of the State of Connecticut for possible funds to support this project.

Building Cost \$51,000. Crane rental/setting: \$4,200 Site work/utilities: \$4,800

TL Cost \$61,000 (2% contingency)

Not only will we be better serving users of this park, several studies have shown quality parks improve our health, economy, property values, and create strong communities (National Recreation & Parks Association, Centers for Disease Control).

- b) Indicate the Progress to date on the project.**

In the planning/budget stage. A local electrician and plumber have offered to donate their services to hook-up the restroom utilities. A location in the upper park area across from Clark Lane Middle School has been chosen. The Stenger Farm Advisory Committee has formally supported this project. The budget figure has been reduced to \$30,000. It was reduced to reflect cost savings by moving the bathroom closer to the existing parking area which will reduce site preparation costs such as parking spaces and sidewalks. Funding from private

donations as well as government grants will require \$31,000, for a total project cost of \$61,000. It has been confirmed utilities are located close to the site.

- c) List other projects it is coordinated with whether in your Department or or other departments.

Utility Commission & staff have assisted us in site planning/location/utility connections.

- d) Describe the impact the project will have on your annual department expenditures.

Approximate estimated costs: Water: \$300, sewage \$250, electricity: \$800, custodial supplies at \$200. Solar powered equipment and water saver toilets are expected to provide cost savings. Labor will be provided by our maintenance crew under normal hours, so no additional cost will be incurred.

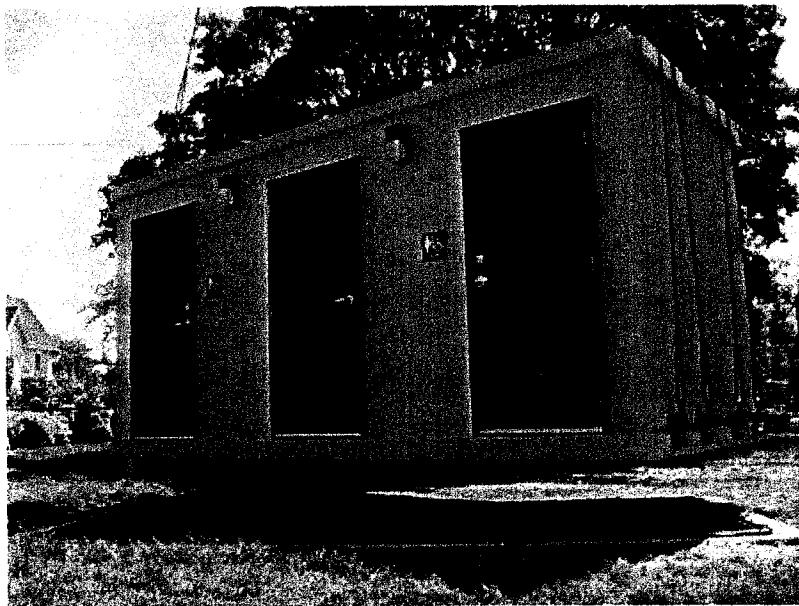
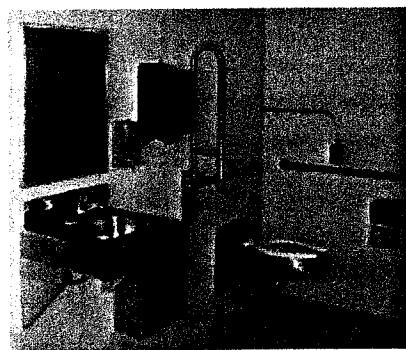
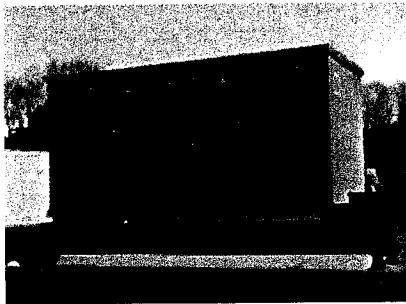
- e) Attach plan, estimate, service area map and/or other support documentation

Precast Concrete Restroom Buildings



Unicon restroom buildings and concession stand buildings are manufactured by United Concrete Products Inc. in Yalesville, Connecticut and delivered to New England and Northeast.

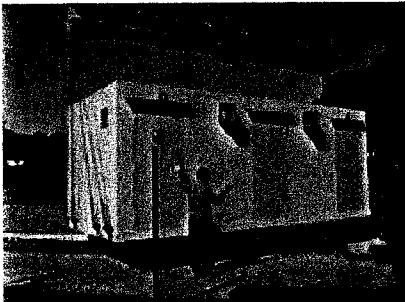
These buildings offer the best long-range solution for parks and recreation departments, because they offer the permanence and durability of precast concrete, while retaining the flexibility for future relocation to accommodate any site changes that might arise.



Unicon Precast Concrete Restroom Buildings

•DESIGN: International Building Code (2000 Edition)

•CONSTRUCTION: 5000 psi steel-reinforced precast concrete. 4"



walls, 6" floor and 4 1/2 " roof.

DOORS: 18 gauge galvanized steel doors, 16 gauge galvanized steel frames, stainless steel hinges, ADA lockset, indicator dead-bolt lock, door closer, threshold.

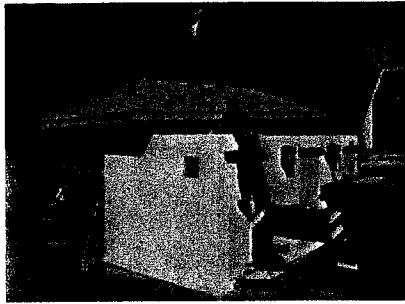
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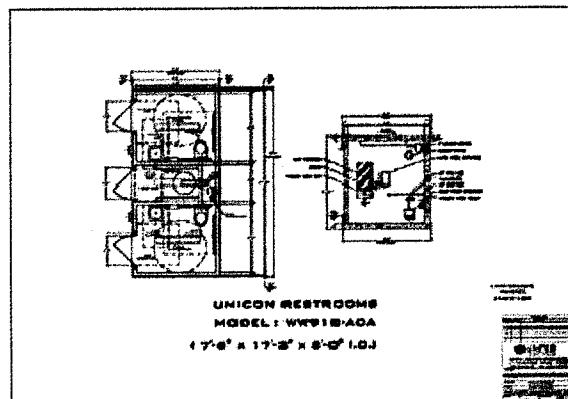
Unicon prefabricated restrooms offer a variety of options in terms of design, fixtures and finishes. Stainless steel fixtures are preferred by the customers seeking for maximum vandal-resistance.

Concession buildings and storage buildings are also available to combine with restroom buildings offering a multi-use building solution.

Please, call **1-800-234-3119** Ext.17 Mr. Tolga Oztoprak for detailed information, specifications and drawings.



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Standard and Custom Designs Available!



173 Church Street
Yalesville, CT 06492
Toll Free: (800) 234-3119
Tel: (203) 269-3119
Fax: (203) 265-4941

QUOTATION

Nov 11, 2011

**Town of Waterford
Parks And Recreation Department
Attn Brian Flaherty**

Re: Waterford Park Department
Waterford CT
Proposal # 6093

I am pleased to provide you with a Quotation for the Pre-Cast Concrete Restroom structure for the above listed project.

- The Pre-cast building structure will be shipped as complete one unit and will be installed in the field with our personnel on a customer supplied compacted 10" stone base with 4" of pea stone gravel.
- Penetrations to be provided as indicated on drawings for incoming water supply, sewer and main electrical connections.

Restroom Structure Will Consist Off

Interior Building Dimensions: 7'-6" x 17'-2" x 8'-0"

Building will Include:

Interior Finish of building:

- Walls and Ceiling: Epoxy Paint.
- Floor: Epoxy paint (slate gray floor with non skid sand additive).

Exterior of building:

- Exterior to be coated with a Thorocoat finish (final color to be determined by owner).

Doors:

- (3) Three CECO 3'-0" x 7'-0" 18 gauge galvanized steel doors w/ SS hinges & contacts. Color to be determined by owner.
- (3) Three CECO 3'-0" x 7'-0" 16 gauge galvanized steel door frames.
- Cylindrical Lockset for each door.
- Weatherstrip & threshold at each door.
- (2) Two closures (@ restroom doors) & (1) One hold open chain (@ utility chase)
- (2) Two deadbolts w/ occupancy indicator.

Electrical:

- (1) One 100 Amp electrical panel.
- (3) Three vapor tight 4' interior light fixtures w/ motion sensor on/off.

November 10, 2011

- (1) One 120V 20A Receptacle.
- Surface mounted EMT.
- (2) Two light switches.
- (2) Two call for aid devices.
- (1) One exterior lights w/ photocell & time clock.
- (3) Three Combination LED Exit Sign / Emergency Lighting Unit
- (2) Two exhaust fans & vandal proof wall vent.
- Door contact and strikes wired to common junction box each building.

Plumbing:

- (1) One tankless waterheater.
- (2) Two stainless steel sinks w/ mechanical metering auto shut-off hot & cold faucets.
- (2) Two stainless steel toilets w/ integral seats and concealed push button flushometer.

Accessories:

- (2) Two center pull paper towel dispensers.
- (2) Two stainless steel soap dispensers.
- (2) Two stainless steel waste receptacles.
- (2) Two stainless steel toilet seat covers.
- (2) Two stainless steel toilet paper dispensers.
- Restroom door signage (handicap Male & Female).

EXCLUSIONS:

- Site preparation by others.
- ALL items specifically not listed above.
- Site electrical & wiring to restroom building.
- 3rd party testing.
- Disconnect Switch.
- Electric meter.
- ALL FIELD Connections of all plumbing, mechanical & electrical to main supplies by others.

Total Price:\$ 51,000.00 (Excludes Tax)

ADDER: Add for ceiling mount 3kw electric heater\$1986.00
ADDER: Add for 1-day crane rental service to set Pre-cast building.....\$4200.00
ADDER: Add for Sargent mortise lock sets with electric strikes and timer option.. \$2300.00

Town of Waterford
TRANSFERS OUT TO CAPITAL IMPROVEMENTS

Report # 34917

Statement Code: GF10638SUM

Account Number / Description	Adopted Budget 7/1/2011 - 6/30/2012	Revised Budget 7/1/2011 - 6/30/2012	Current Period 3/1/2012 - 3/31/2012	Reporting Period 7/1/2011 - 3/31/2012	Encumbrances 7/1/2011 - 3/31/2012	Amt Remaining 7/1/2011 - 3/31/2012	% Remaining 7/1/2011 - 3/31/2012
10638-55738-101-060-38-00-00 FLEET MANAGEMENT PLAN	1,095,000.00	1,095,000.00	0.00	1,095,000.00	0.00	0.00	0.00 %
10638-55739-101-060-38-00-00 IT NETWORK UPGRADE	0.00	0.00	(23,000.00)	0.00	0.00	0.00	---
10638-55777-101-060-38-00-00 COMPUTER TO PLATE-PRINT SHOP	23,000.00	23,000.00	23,000.00	23,000.00	0.00	0.00	0.00 %
10638-55778-101-060-38-00-00 SEWER SYSTEM UPGRADE	18,000.00	21,951.00	0.00	21,951.00	0.00	0.00	0.00 %
GRAND TOTAL	\$1,136,000.00	\$1,139,951.00	\$0.00	\$1,139,951.00	\$0.00	\$0.00	0.00 %

Town of Waterford
TRANSFERS TO CAPITAL AND NON-RECURRING FUND

Statement Code: GF10640SUM

Account Number / Description	Adopted Budget	Revised Budget	Current Period	Reporting Period	Encumbrances	Amt Remaining	% Remaining
	7/1/2011 - 6/30/2012	7/1/2011 - 6/30/2012	3/1/2012 - 3/31/2012	7/1/2011 - 3/31/2012	7/1/2011 - 3/31/2012	7/1/2011 - 3/31/2012	7/1/2011 - 3/31/2012
10640-57495-101-060-40-00-00 ROAD RECLAMATION	240,747.00	240,747.00	0.00	240,747.00	0.00	0.00	0.00 %
10640-57639-101-060-40-00-00 REVALUATION	75,000.00	75,000.00	0.00	75,000.00	0.00	0.00	0.00 %
10640-57706-101-060-40-00-00 SOCCER LIGHTS & SPERA FIELD	34,500.00	34,500.00	0.00	34,500.00	0.00	0.00	0.00 %
10640-57731-101-060-40-00-00 POLICE ROOF & GUTTER REPLACEM	135,000.00	135,000.00	0.00	135,000.00	0.00	0.00	0.00 %
10640-57733-101-060-40-00-00 OSWEGATCHIE FIRE BLDG IMPROVE	20,000.00	20,000.00	0.00	20,000.00	0.00	0.00	0.00 %
10640-57734-101-060-40-00-00 FIRE COMM. UNDERGROUND TANK	45,000.00	45,000.00	0.00	45,000.00	0.00	0.00	0.00 %
10640-57735-101-060-40-00-00 LEARY PARK RD/PARKING IMPROVE	20,000.00	20,000.00	0.00	20,000.00	0.00	0.00	0.00 %
GRAND TOTAL	\$570,247.00	\$570,247.00	\$0.00	\$570,247.00	\$0.00	\$0.00	0.00 %

ANNUAL BUDGET		DEPT/AGENCY:		10638 CURRENT YEAR CAPITAL IMPROVEMENTS					FISCAL YEAR 2012/2013		
		COLUMN 1	COLUMN 2	COLUMN 3	COLUMN 4	COLUMN 5	COLUMN 6	COLUMN 7	COLUMN 8	COLUMN 9	
LINE	ITEM	2010/11	168	2011/12	ACTUAL	2012/2013	2012/2013	2012/2013	2012/2013	2012/2013	
	DESCRIPTION	EXPENDED	APPROP.	ADDITIONAL	TO 1/1/12	REQUEST	FIRST SELECTMAN	BD OF SELECTMEN	BD OF FINANCE	APPROVED	
CAPITAL IMPROVEMENTS											
BOARD OF SELECTMEN:											
55738	FLEET MANAGEMENT PLAN	820,000	1,095,000		1,095,000	1,095,000	1,095,000	1,095,000	1,095,000	1,095,000	
	SUBTOTAL BD. OF SELECTMEN	820,000	1,095,000	0	1,095,000	1,095,000	1,095,000	1,095,000	1,095,000	1,095,000	
INFORMATION TECHNOLOGY											
55739	IT NETWORK UPGRADE-RMS POLICE CARS	19,752	0		0	0	0	0	0	0	
55777	COMPUTER TO PLATE-PRINT SHOP	0	23,000		23,000	8,500	8,500	8,500	8,500	8,500	
	POLICE TELESTAFF SCHEDULING PROGRAM	0	0		0	28,750	0	0	0	0	
	SUBTOTAL INFORMATION TECHNOLOGY:	19,752	23,000	0	23,000	37,250	8,500	8,500	8,500	8,500	
EMERGENCY MANAGEMENT											
	UPS SYSTEM FOR COMMUNICATIONS CENTER	0	0		0	32,000	0	0	0	0	
	SUBTOTAL FIRE COMMISSION	0	0	0	0	32,000	0	0	0	0	
RECREATION & PARKS:											
55774	WFD BEACH PARK PAVILION REPAIRS	11,000	0		0	0	0	0	0	0	
	WBP ACCESSIBLE BATHROOM	0	0		0	23,817	0	0	0	0	
	SUBTOTAL RECREATION & PARKS	11,000	0	0	0	23,817	0	0	0	0	
PUBLIC WORKS:											
55775	FUEL RECORDING SYSTEM	30,000	0		0	0	0	0	0	0	
55776	RECYCLING ROLL OUT CONTAINERS	82,672	0		0	0	0	0	0	0	
	SUBTOTAL PUBLIC WORKS	112,672	0	0	0	0	0	0	0	0	
LIBRARY:											
55760	ENG. STUDY-RETROFIT ROOFTOP HVAC	20,000	0		0	0	0	0	0	0	
55778	SEWER SYSTEM UPGRADE	0	18,000		18,000	0	0	0	0	0	
	SUBTOTAL LIBRARY	20,000	18,000	0	18,000	0	0	0	0	0	
BOARD OF EDUCATION:											
	SOUNDFIELD SYSTEM - CLMS	0	0	0	0	71,000	0	0	0	0	
	SUBTOTAL BOARD OF EDUCATION:	0	0	0	0	71,000	0	0	0	0	
YOUTH SERVICES:											
	LEAD ABATEMENT/EXT. PAINTING/WINDOW REP	0	0	0	0	86,500	0	0	0	0	
	SUBTOTAL YOUTH SERVICES:	0	0	0	0	86,500	0	0	0	0	
SENIOR SERVICES:											
	REPLACEMENT CHAIRS @ COMMUNITY CENTER	0	0	0	0	0	12,155	12,155	12,155	12,155	
	SUBTOTAL SENIOR SERVICES:	0	0	0	0	0	12,155	12,155	12,155	12,155	
	DEPARTMENT TOTAL	983,424	1,136,000	0	1,136,000	1,345,567	1,115,655	1,115,655	1,115,655	1,115,655	

ANNUAL BUDGET		DEPT/AGENCY:		10640 TRANSFERS TO CAPITAL AND NON-RECURRING EXPENDITURE FUND					FISCAL YEAR 2012/2013		
LINE	ITEM	COLUMN 1	COLUMN 2	COLUMN 3	COLUMN 4	COLUMN 5	COLUMN 6	COLUMN 7	COLUMN 8	COLUMN 9	
		2010/11	2011/12	2011/12	ACTUAL	2012/2013	2012/2013	2012/2013	2012/2013	2012/2013	
		ACTUAL	R.T.M.	(TRANSFER)	EXPENDED/ENC	DEP/AGENCY	FIRST SELECTMAN	RECOMMENDED	RECOMMENDED	R.T.M.	
	DESCRIPTION	EXPENDED	APPROP.	ADDITIONAL	TO 1/1/12	REQUEST	RECOMMENDED	BD OF SELECTMEN	BD OF FINANCE	APPROVED	
	SERVICES										
	BOARD OF SELECTMEN:										
57639	REVALUATION	75,000	75,000		75,000	75,000	75,000	75,000	75,000	75,000	
	COHANZIE SCHOOL REMEDIATION & DEMO	0	0		0	463,100	463,100	463,100	463,100	463,100	
	AUDIO VISUAL UPGRADE T.H. AUDITORIUM	0	0		0	0	27,374	27,374	27,374	27,374	
	SUBTOTAL BD. OF SELECTMEN	75,000	75,000	0	75,000	538,100	565,474	565,474	565,474	565,474	
	POLICE										
57731	ROOF & GUTTER REPLACEMENT	0	135,000		135,000	0	0	0	0	0	
	SUBTOTAL POLICE:	0	135,000	0	135,000	0	0	0	0	0	
	FIRE COMMISSION:										
57733	OSWEGATCHIE BUILDING IMPROVEMENTS	0	20,000		20,000	500,000	0	0	0	0	
57734	UNDERGROUND TANK REPLACEMENTS	0	45,000		45,000	45,000	45,000	45,000	45,000	45,000	
	JORDAN PARKING LOT IMPROVEMENTS	0	0		0	80,000	80,000	80,000	80,000	80,000	
	JORDAN BLDG. DOOR REPLACEMENT	0	0		0	20,000	0	0	0	0	
	SUBTOTAL FIRE COMM.	0	65,000	0	65,000	645,000	125,000	125,000	125,000	125,000	
	RECREATION & PARKS:										
57718	WTFD BEACH PARK MAINT. BLDG. REPAIR	27,000	0		0	0	0	0	0	0	
57706	SOCER LIGHTS & SPERA FIELD	45,000	34,500		34,500	0	0	0	0	0	
57735	LEARY PARK ROAD/PARKING IMPROVE.	0	20,000		20,000	0	0	0	0	0	
	LEARY PARK IRRIGATION SYSTEM	0	0		0	26,000	0	0	0	0	
	LL SOUTH - NEW LITTLE LEAGUE FIELD	0	0		0	50,000	0	0	0	0	
	SEWER CONNECTION O'NEILL/WBP	0	0		0	170,000	0	0	0	0	
	SUBTOTAL REC & PARKS	72,000	54,500	0	54,500	246,000	0	0	0	0	
	SENIOR SERVICES:										
	REPLACEMENT CHAIRS-COMM. CENTER	0	0		0	12,155	0	0	0	0	
	SUBTOTAL SENION SERVICES:	0	0	0	0	12,155	0	0	0	0	
	PUBLIC WORKS:										
57651	DOUGLAS LANE RECONSTRUCTION #2	0	0		0	99,000	99,000	99,000	99,000	99,000	
57090	SIDEWALKS & TRAILS	0	0		0	323,000	0	0	0	0	
57495	ROAD RECLAMATION/OVERLAY-DIMMOCK	255,000	240,747		240,747	271,619	271,619	271,619	271,619	271,619	
	JORDAN COVE RD. BRIDGE REPLACEMENT	0	0		0	380,000	380,000	380,000	380,000	380,000	
	SUBTOTAL PUBLIC WORKS	255,000	240,747	0	240,747	1,073,619	750,619	750,619	750,619	750,619	
	UTILITIES COMMISSION:										
57605	MAGO POINT PS UPGRADE	390,000	0		0	0	0	0	0	0	
57719	NLWWP CTDEP Draft Order	35,000	0		0	0	0	0	0	0	
	RICHARDS GROVE PUMP STATION UPGRADE	0	0		0	422,000	0	0	0	0	
	LOGGERS HILL SEWER LINE REHABILITATION	0	0		0	1,200,000	0	0	0	0	
	SUBTOTAL UTILITIES COMM	425,000	0	0	0	1,622,000	0	0	0	0	
	DEPARTMENT TOTAL	827,000	570,247	0	570,247	4,136,874	1,441,093	1,441,093	1,441,093	1,441,093	



January 30, 2012

Paul A. Suprin, Selectman
Paul Konstantakis Selectman

RE: FIVE YEAR CAPITAL IMPROVEMENT PLAN RECOMMENDATIONS

As part of my responsibilities to prepare a Capital Improvement Plan, I received \$5,579,541 in capital improvement project requests for the fiscal year beginning July 1, 2012 and \$38,027,216 in requests for the 5 year planning period of FY 13 through FY17. Funds for capital improvements are appropriated or designated during the budget process primarily into two accounts. First, into the Current Year Capital (CYC) Improvements budget 10638; and second, Transfers to Capital and Non-Recurring Expenditure Fund (CNR) 10640. \$1,706,247 was funded in FY 12 into these accounts. I have recommended that the funding level for these two budgets for the FY 13 Capital Improvement Plan should be increased to \$2,556,748. The net increase considering revenue from 80% reimbursement on the Jordan Cove Road Bridge replacement grant and 100% on the Cohanzie School demolition from LoCIP funding, results in a net request of \$1,789,648.

The information contained in the binder includes the CIP Guidelines, Capital Improvement Fund balances and all the capital improvement requests by department. In addition my recommended plan for the upcoming fiscal year as well as the 5 year planning period are attached with my comments along with the summary of the budgets for the CNR & CYC for the FY 13 budget. This document and binder constitute my recommendations for the Capital Improvement Plan(CIP) for Fiscal Years 2013-2017. The Town Charter requires these recommendations be ratified by the Board of Selectmen.

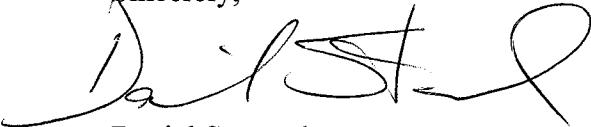
The following are my comments on how the majority of the funds are anticipated to be used:

- Continued funding of the Fleet Management Plan in the amount of \$1,095,000. At this level of appropriation over the next several years the plan will be adequately funded.
- I have moved up the funding for upgrading the audio system in the Auditorium as a high priority project. Current estimates and bids will be provided prior to requesting the appropriation of funds.
- Information technology & hardware upgrades are an ongoing program. \$8,500 is recommended to provide the second phase of the replacement of the Computer to Print Plate for the print shop.
- Revaluation is an ongoing process and this \$75,000 allows us to fund the program over the next five years as we have done in the past.
- Funding for road improvements includes \$271,619 for road reclamation and overlay of Dimmock Road.

- \$99,000 is recommended to be designated in order to start the design of the second phase of reconstruction of Douglas Lane.
- 80% funding is available for the engineering of repairs to the Jordan Cove Road bridge under the State's Local Bridge Program. Total cost for design is estimated at \$380,000.
- Funding for replacement chairs for the Community Center is recommended at \$12,155.
- Some partial funding from existing sources to assist with the evaluation of the proposed improvements to the Oswegatchie Fire House is recommended. A building committee needs to be established.
- Repaving of the Jordan Fire House parking lot needs to use DPW to assist with getting best price prior to requesting appropriation.
- A project to change over the existing fuel storage tanks at each fire house is entering its second year of funding. Funding this year or next is dependent on the status of progress made on the first phase.
- Sewer and Water projects will come from funds already on account or through re-designation of existing funds in the CYC and CNR budgets. Although there is currently no money appropriated or designated in the CNR for sewer projects, there is a great deal available for water projects. I endorse the Utility Commission's program to upgrade the pump stations and would recommend that the Utility Commission might consider releasing water funds that have been available to them in some cases for at least a decade and ask that they be applied to continue with their pump station upgrade program in light of the current budget situation.

I anticipate reviewing these recommendations with the Board at the upcoming budget meeting and ultimately expect the Board to ratify the plan. Please feel free to contact Rudie Beers, Finance Director, Tom Wagner, Planning Director or myself with any questions you may have.

Sincerely,



Daniel Steward
First Selectman

Enclosure

Distribution List :

Representative Town Meeting: Moderator, Majority and Minority Leaders
Director of Finance & Chairman, Board of Finance
Superintendent of Schools & Chairperson, Board of Education
Director of Buildings & Grounds, Board of Education
Director of Finance and Operations, Board of Education
Planning Director
Administrative Officer, Emergency Management
Director of Fire Services
Treasurer

Tax Assessor
Town Clerk
Tax Collector
Police Chief & Chair, Board of Police Commissioners
Director of Public Works
Chief Engineer & Chair, Utility Commission
Senior Services Director & Chair, Senior Citizens Commission
Director of Recreation and Parks & Chair, Recreation & Parks Commission
Chair, Flood & Erosion Control Board
Library Director & President of the Library Board
Director of Human Resources
Purchasing Agent

CAPITAL IMPROVEMENT PLAN FOR FY 2013-17
FIRST SELECTMAN'S RECOMMENDED PLAN

RATIFIED 3/29/2012 BOARD OF SELECTMEN

CURRENT YEAR CAPITAL IMPROVEMENTS (CYC)		Proposed FY 2013	Recommended FY 2013	First Selectman's Comments & Recommendations
DESCRIPTION	DEPARTMENT			
FLEET MANAGEMENT	FIRST SELECTMAN	\$1,095,000	\$ 1,095,000	Minimum funding needed
BOARD OF EDUCATION				
SOUNDFIELD SYSTEM-CLARK LANE MIDDLE SCHOOL		\$71,000	\$ -	Insufficient funding. No back up provided
INFORMATION TECHNOLOGY COMMITTEE				
COMPUTER TO PLATE SYSTEM-PRINT SHOP		\$8,500	\$ 8,500	Second year of funding to complete project
POLICE TELESTAFF AUTOMATED SCHEDULING		\$28,750		Move out
SENIOR SERVICES				
REPLACEMENT CHAIRS COMMUNITY CENTER		\$0	\$ 12,155	Project ready for funding moved from CNR
EMERGENCY MANAGEMENT				
UPS SYSTEM FOR COMMUNICATIONS CENTER		\$32,000		Use funds from Radio Project
RECREATION & PARKS COMMISSION				
WATERFORD BEACH PARK: ACCESSIBLE BATHROOM		\$23,817		Move out, sewers planned with O'Neill project. Detailed review of Beach Park facilities needed to determine degree of modernization including flood hazard risks.
YOUTH SERVICES				
LEAD ABATEMENT, EXTERIOR PAINTING & WINDOW REPLACEMENT-SECTION 2		\$86,500		Move out, More detailed cost estimate needed
TOTAL		\$1,345,567	\$1,115,655	
TRANSFER TO CAPITAL & NON-REC. (CNR)				
DESCRIPTION	DEPARTMENT	Proposed	Recommended	
FLEET MANAGEMENT	FIRST SELECTMAN	FY 2013	FY 2013	
COHANZIE SCHOOL REMEDIATION AND DEMOLITION		\$463,100	\$ 463,100	Designation needed in anticipation of grant funding or LoCIP
AUDIO/VISUAL UPGRADE-TOWN HALL AUDITORIUM			\$ 27,374	Move up from FY2014 Priority
FIRE SERVICE				
JORDAN PARKING LOT IMPROVEMENTS		\$80,000	\$ 80,000	Work with DPW to get pricing through State Bid
JORDAN BUILDING DOOR REPLACEMENT		\$20,000		
OSWEGATCHIE-BUILDING IMPROVEMENTS		\$500,000		Establish Building Committee fund in part from remaining Building Improvement funds
FIRE STATIONS - UNDERGROUND TANK REPLACEMENT		\$45,000	\$ 45,000	Continuing project
ASSESSOR/FINANCE				
REVALUATION		\$75,000	\$ 75,000	Continuing project
SENIOR SERVICES				
REPLACEMENT CHAIRS COMMUNITY CENTER		\$12,155	\$ -	Fund from CYC
RECREATION & PARKS COMMISSION				
LEARY PARK IRRIGATION SYSTEM		\$26,000	\$ -	Move out
SEWAGE CONNECTION ONEILL & WATERFORD BEACH		\$170,000		Project represents new capital asset/improvement to Town property. Cost offset against future revenue based on recent lease amendment to allow building expansion
COSPONSORED LEAGUES				
LLSOUTH-NEW LITTLE LEAGUE FIELD		\$50,000	\$ -	Coordinate needs with LL Football-move out no detail
DEPARTMENT OF PUBLIC WORKS				
RECONSTRUCTION: DOUGLAS LANE NO 2		\$99,000	\$ 99,000	
ROAD RECLAMATION/MILL & OVERLAY: DIMMOCK RD		\$271,619	\$ 271,619	
JORDAN COVE RD BRIDGE REPLACEMENT		\$380,000	\$ 380,000	80% funded from local bridge program DOT
SIDEWALK & TRAILS: BOSTON POST RD & GOSHEN ROAD		\$323,000	\$ -	Insufficient funding available
UTILITY COMMISSION				
LOGGER HILL ROAD SEWER LINE REHAB OR REPLACEMENT		\$1,200,000	\$ -	Request pending for funding in FY12
RICHARDS GROVE ROAD PUMP STATION PARTIAL UPGRADE		\$422,000		Current level of funding for sewer projects has depleted available funding
TOTAL		\$4,136,874	\$1,441,093	

CAPITAL IMPROVEMENT PLAN FOR FY 2013-17

RATIFIED 3/29/2012 BOARD OF SELECTMEN

FIRST SELECTMAN'S RECOMMENDED PLAN

DESCRIPTION	DEPARTMENT	FUNDING SOURCE	FY 2013	FY 2014	FY 2015	FY 2016	FY 2017	FY 2013-17
FIRST SELECTMAN								
FLEET MANAGEMENT		1	\$1,095,000	\$1,095,000	\$1,095,000	\$1,095,000	\$1,095,000	\$5,475,000
COHANZIE SCHOOL REMEDIATION AND DEMOLITION		4	\$463,100					\$463,100
ASSESSOR/FINANCE								
REVALUATION		4	\$75,000	\$75,000	\$75,000	\$75,000	\$75,000	\$375,000
BOARD OF EDUCATION								
SOUNDFIELD SYSTEM-CLARK LANE MIDDLE SCHOOL		1		\$71,000				
TIME & ATTENDANCE RECORDING SYSTEM-BOE EMPLOYEES		1		\$40,000				
VOICE OVER IP CLARK LANE MIDDLE SCHOOL		1		\$45,000				
INFORMATION TECHNOLOGY COMMITTEE								
COMPUTER TO PLATE SYSTEM-PRINT SHOP		1	\$8,500					\$8,500
POLICE TELESTAFF AUTOMATED SCHEDULING		1		\$28,750				\$28,750
PRINT SHOP FOLDER INSERTER		1		\$11,900				\$11,900
AUDIOVISUAL UPGRADE-TOWN HALL AUDITORIUM		4	\$27,374					\$27,374
TOWN-WIDE TELECOMMUNICATION UPGRADE		4			\$144,348			\$144,348
POLICE COP LOGIC (WEB-BASED COMPLAINT)		4				\$19,250		\$19,250
FIRE SERVICE								
COMMUNITY FIRE PROTECTION IMPROVEMENTS		4		\$25,000	\$25,000	\$25,000		\$75,000
JORDAN PARKING LOT IMPROVEMENTS		4	\$80,000					\$80,000
JORDAN BUILDING DOOR REPLACEMENT		4		\$20,000				\$20,000
QUAKER HILL-BUILDING ROOF PROJECT		4		\$80,000				\$80,000
GOSHEN-BUNKROOM RENOVATIONS		4		\$20,000				\$20,000
OSWEGATCHIE-BUILDING EMERGENCY GENERATOR		4		\$30,000				\$30,000
OSWEGATCHIE-BUILDING IMPROVEMENTS		8	\$100,000					\$100,000
COHANZIE-BUILDING RENOVATIONS		4			\$100,000			\$100,000
PUBLIC SAFETY COMPLEX-CARPET REPLACEMENT		4		\$50,000				\$50,000
THERMAL IMAGING CAMER REPLACEMENT		2	\$16,000	\$16,000				\$32,000
FIRE STATIONS - UNDERGROUND TANK REPLACEMENT		4	\$45,000					\$45,000
POLICE								
PARKING LOT RESURFACING		4		\$170,000				\$170,000
IMPOUND STORAGE BUILDING #2		4				\$30,000		\$30,000
EMERGENCY MANAGEMENT								
UPS SYSTEM FOR COMMUNICATIONS CENTER		8	\$32,000					\$32,000
FLOOD AND EROSION CONTROL BOARD								
ALEWIFE COVE		4			\$100,000			\$100,000
SENIOR SERVICES								
REPLACEMENT CHAIRS COMMUNITY CENTER		1	\$12,155					\$12,155
YOUTH SERVICES								
LEAD ABATEMENT, EXTERIOR PAINTING & WINDOW REPLACEMENT-SECTION 2		1		\$86,500				\$86,500
HEATING & COOLING SYSTEM-SECTION 2		1			\$34,000			\$34,000
RECREATION & PARKS COMMISSION								
WATERFORD BEACH PARK: ACCESSIBLE BATHROOM		1		\$23,817				\$23,817
REPLACEMENT OF CAUSEWAY RESTROOM WFD. BEACH PARK		4		\$61,000				\$61,000
LEARY PARK IRRIGATION SYSTEM		4		\$26,000	\$24,800	\$11,500		\$62,300
RESTROOMS AT STENGER FARM PARK		4			\$30,000			\$30,000
COSPONSORED LEAGUES								
LLSOUTH-NEW LITTLE LEAGUE FIELD		4		\$50,000				\$50,000
YOUTH FOOTBALL LIGHTING IMPROVEMENTS		4		\$22,000				\$22,000
GARDINERS WOOD COMPLEX, PARKING LOT IMPROVEMENTS & LIGHTING		4			\$80,000			\$80,000
FENCING IMPROVEMENTS- LL SOUTH		4		\$50,000				\$50,000
FIELD LIGHTS - SENIOR SOFTBALL		4				\$80,000		\$80,000
PRESS BOX W/STORAGE-SENIOR SOFTBALL		4					\$20,000	\$20,000
EUGENE O'NEILL THEATRE CENTER								
SEWAGE CONNECTION ONEILL & WATERFORD BEACH		6	\$170,000					\$170,000

CAPITAL IMPROVEMENT PLAN FOR FY 2013-17

FIRST SELECTMAN'S RECOMMENDED PLAN FUNDING

DESCRIPTION	DEPARTMENT	SOURCE	FY 2013	FY 2014	FY 2015	FY2016	FY 2017	FY 2013-17
DEPARTMENT OF PUBLIC WORKS								
MUNICIPAL COMPLEX RENOVATIONS & CLEAN UP		4			\$6,313,774			\$6,313,774
RECONSTRUCTION: DOUGLAS LANE NO 2		4	\$99,000	\$1,199,000				\$1,298,000
RECONSTRUCTION: GARDINERS WOOD ROAD		4			\$169,000	\$1,973,000		\$2,142,000
ROAD RECLAMATION/MILL & OVERLAY: DIMMOCK RD		4	\$271,619					\$271,619
ROAD RECLAMATION/MILL & OVERLAY: DAYTON RD & FARGO RD		4		\$342,115				\$342,115
ROAD RECLAMATION/MILL & OVERLAY: WILLETS AVE		4			\$112,773			\$112,773
ROAD RECLAMATION/MILL & OVERLAY: CROSS ROAD-85 TO I-95		4				\$348,995		\$348,995
ROAD RECLAMATION/MILL & OVERLAY: BLOOMINGDALE RD		4					\$229,613	\$229,613
JORDAN COVE RD BRIDGE REPLACEMENT		4	\$380,000		\$2,292,000			\$2,672,000
SIDEWALK & TRAILS: BOSTON POST RD & GOSHEN ROAD		4		\$323,000				\$323,000
SIDEWALK & TRAILS: ROPE FERRY ROAD		4			\$291,000			\$291,000
SIDEWALK & TRAILS: PILGRIM & DAYTON RDS		4				\$308,000		\$308,000
SIDEWALK & TRAILS: NORMAN, VIVIAN, CROSS, SPITHEAD, GREENTREE		4					\$313,000	\$313,000
SIDEWALK & TRAILS: SHORE RD, JORDAN COVE RD		4					\$345,000	\$345,000
PARKWAY NORTH CONNECTOR		4			\$150,700	\$2,292,000		\$2,442,700
CONCRETE CURB REPLACEMENT		2		\$69,554	\$70,601	\$40,978		\$181,133
UTILITY COMMISSION								
LOGGER HILL ROAD SEWER LINE REHAB OR REPLACEMENT		4	\$0					\$0
RICHARDS GROVE ROAD PUMP STATION PARTIAL UPGRADE		4		\$422,000				\$422,000
FUTURE SSES/CMOM PROGRAM		5	\$33,000	\$33,000	\$33,000	\$33,000	\$33,000	\$165,000
LI SOUND NITROGEN REDUCTION		3	\$48,100	\$47,400	\$46,700	\$46,000	\$45,300	\$233,500
CROSS RD PUMP STATION PARTIAL UPGRADE		4		\$491,000				\$491,000
REPLACE MARILYN RD, WIEMES CT, EJECTOR STATIONS		4				\$975,000		\$975,000
STONEY BROOK PUMP STATION PARTIAL UPGRADE		4					\$511,000	\$511,000
BOLLES COURT PUMP STATION PARTIAL UPGRADE		4				\$468,000		\$468,000
HARVEY AVE PUMP STATION PARTIAL UPGRADE		4					\$446,000	\$446,000
REMAINING 17 PUMP STATIONS		4				\$672,000	\$675,000	\$1,347,000
NLWWTP CTDEP DRAFT ORDER		4					\$300,000	\$300,000
INFLOW AND INFILTRATION MITIGATION & CONTROL		4		\$319,000	\$335,000	\$352,000	\$300,000	\$1,306,000
CO-OP SEWER LATERAL EXTENSIONS		7		\$375,000	\$375,000	\$375,000	\$375,000	\$1,500,000
WASTEWATER SCADA SYSTEM UPGRADE		4		\$241,000	\$241,000	\$241,000		\$723,000
PORTABLE EMERGENCY GENERATORS, SWITCHES, ETC		5		\$150,000				\$150,000
GRAND TOTALS			\$2,955,848	\$6,109,036	\$13,113,696	\$8,485,723	\$4,762,913	\$35,427,216

FIVE YEAR CAPITAL PLAN-SUMMARY

FIRST SELECTMAN'S RECOMMENDED PLAN

SUBTOTALS: FUNDING SOURCE	FUNDING SOURCE	FY 2013	FY 2014	FY 2015	FY2016	FY 2017	FY 2013-17
CURRENT YEAR CAPITAL IMPROVEMENTS (CYC)	1	\$1,115,655	\$1,401,967	\$1,129,000	\$1,095,000	\$1,095,000	\$5,836,622
OPERATING BUDGETS	2	\$16,000	\$85,554	\$70,601	\$40,978	\$0	\$213,133
WASTE WATER BUDGET/OR SEWER CAP. MAINT. FUND	3	\$48,100	\$47,400	\$46,700	\$46,000	\$45,300	\$233,500
TRANSFER TO CAPITAL & NON-REC. (CNR)	4	\$1,441,093	\$4,016,115	\$11,459,395	\$6,895,745	\$3,214,613	\$27,026,961
SHORT AND LONG TERM DEBT FINANCING	5	\$33,000	\$183,000	\$33,000	\$33,000	\$33,000	\$315,000
LOCAL CAPITAL IMPROVEMENT PROGRAM (LoCIP) OR GRANTS	6	\$170,000	\$0	\$0	\$0	\$0	\$170,000
DESIGNATE FUNDS FROM UNDESIGNATED PROJECTS & FUND BALANCE CNR	7	\$0	\$375,000	\$375,000	\$375,000	\$375,000	\$1,500,000
EXISTING FUNDS AVAILABLE IN CNR EXPENDITURE FUND	8	\$132,000	\$0	\$0	\$0	\$0	\$132,000
GRAND TOTALS		\$2,955,848	\$6,109,036	\$13,113,696	\$8,485,723	\$4,762,913	\$35,427,216
SUBTOTALS: DEPARTMENTS							
SELECTMEN		\$1,558,100	\$1,095,000	\$1,095,000	\$1,095,000	\$1,095,000	\$5,938,100
BOARD OF EDUCATION		\$0	\$156,000	\$0	\$0	\$0	\$156,000
INFORMATION TECHNOLOGY COMMITTEE		\$35,874	\$40,650	\$144,348	\$19,250	\$0	\$240,122
ASSESSOR/FINANCE DEPT		\$75,000	\$75,000	\$75,000	\$75,000	\$75,000	\$375,000
LIBRARY		\$0	\$0	\$0	\$0	\$0	\$0
FIRE		\$241,000	\$241,000	\$125,000	\$25,000	\$0	\$632,000
POLICE DEPARTMENT		\$0	\$170,000	\$0	\$30,000	\$0	\$200,000
RECREATION AND PARKS		\$0	\$232,817	\$134,800	\$91,500	\$20,000	\$479,117
SENIOR SERVICES		\$12,155	\$0	\$0	\$0	\$0	\$12,155
YOUTH SERVICES		\$0	\$86,500	\$34,000	\$0	\$0	\$120,500
FLOOD AND EROSION CONTROL BOARD		\$0	\$0	\$100,000	\$0	\$0	\$100,000
DEPARTMENT OF PUBLIC WORKS		\$750,619	\$1,933,669	\$9,399,848	\$4,962,973	\$887,613	\$17,934,722
UTILITY COMMISSION		\$81,100	\$2,078,400	\$2,005,700	\$2,187,000	\$2,685,300	\$9,037,500
EMERGENCY MANAGEMENT		\$32,000	\$0	\$0	\$0	\$0	\$32,000
EUGENE O'NEILL THEATRE CENTER		\$170,000	\$0	\$0	\$0	\$0	\$170,000
GRAND TOTALS		\$2,955,848	\$6,109,036	\$13,113,696	\$8,485,723	\$4,762,913	\$35,427,216
		\$0	\$0	\$0	\$0	\$0	\$0

CAPITAL IMPROVEMENT PLAN FOR FY 2013-17

DEPARTMENT REQUESTS

LISTED IN DEPARTMENTAL PRIORITY ORDER

DESCRIPTION	DEPARTMENT	FUNDING SOURCE	FY 2013	FY 2014	FY 2015	FY 2016	FY 2017	FY 2013-17
FLEET MANAGEMENT	FIRST SELECTMAN	1	\$1,095,000	\$1,095,000	\$1,095,000	\$1,095,000	\$1,095,000	\$5,475,000
COHANZIE SCHOOL REMEDIATION AND DEMOLITION	ASSESSOR/FINANCE	4	\$463,100					\$463,100
REVALUATION	BOARD OF EDUCATION	4	\$75,000	\$75,000	\$75,000	\$75,000	\$75,000	\$375,000
SOUNDFIELD SYSTEM-CLARK LANE MIDDLE SCHOOL		1	\$71,000					
TIME & ATTENDANCE RECORDING SYSTEM-BOE EMPLOYEES		1		\$40,000				
VOICE OVER IP CLARK LANE MIDDLE SCHOOL		1		\$45,000				
INFORMATION TECHNOLOGY COMMITTEE								
COMPUTER TO PLATE SYSTEM-PRINT SHOP		1	\$8,500					\$8,500
POLICE TELESTAFF AUTOMATED SCHEDULING		1	\$28,750					\$28,750
PRINT SHOP FOLDER INSERTER		1		\$11,900				\$11,900
AUDIO/VISUAL UPGRADE-TOWN HALL AUDITORIUM		4		\$27,374				\$27,374
TOWN-WIDE TELECOMMUNICATION UPGRADE		4			\$144,348			\$144,348
POLICE COP LOGIC (WEB-BASED COMPLAINT)		4				\$19,250		\$19,250
FIRE SERVICE								
COMMUNITY FIRE PROTECTION IMPROVEMENTS		4		\$25,000	\$25,000	\$25,000		\$75,000
JORDAN PARKING LOT IMPROVEMENTS		4	\$80,000					\$80,000
JORDAN BUILDING DOOR REPLACEMENT		4	\$20,000					\$20,000
QUAKER HILL-BUILDING ROOF PROJECT		4		\$80,000				\$80,000
GOSHEN-BUNKROOM RENOVATIONS		4		\$20,000				\$20,000
OSWEGATCHIE-BUILDING EMERGENCY GENERATOR		4		\$30,000				\$30,000
OSWEGATCHIE-BUILDING IMPROVEMENTS		4	\$500,000	\$500,000	\$500,000			\$1,500,000
COHANZIE- BUILDING RENOVATIONS		4			\$100,000			\$100,000
PUBLIC SAFETY COMPLEX-CARPET REPLACEMENT		4		\$50,000				\$50,000
THERMAL IMAGING CAMER REPLACEMENT		2	\$16,000	\$16,000				\$32,000
FIRE STATIONS - UNDERGROUND TANK REPLACEMENT		4	\$45,000					\$45,000
POLICE								
PARKING LOT RESURFACING		4		\$170,000				\$170,000
IMPOUND STORAGE BUILDING #2		4				\$30,000		\$30,000
EMERGENCY MANAGEMENT								
UPS SYSTEM FOR COMMUNICATIONS CENTER		1	\$32,000					\$32,000
FLOOD AND EROSION CONTROL BOARD								
ALEWIFE COVE		4		\$100,000				\$100,000
SENIOR SERVICES								
REPLACEMENT CHAIRS COMMUNITY CENTER		4	\$12,155					\$12,155
YOUTH SERVICES								
LEAD ABATEMENT, EXTERIOR PAINTING & WINDOW REPLACEMENT-SECTION 2		1	\$86,500					\$86,500
HEATING & COOLING SYSTEM-SECTION 2		1			\$34,000			\$34,000
RECREATION & PARKS COMMISSION								
WATERFORD BEACH PARK: ACCESSIBLE BATHROOM		1	\$23,817					\$23,817
REPLACEMENT OF CAUSEWAY RESTROOM WFD. BEACH PARK		4		\$61,000				\$61,000
LEARY PARK IRRIGATION SYSTEM		4	\$26,000	\$24,800	\$11,500			\$62,300
RESTROOMS AT STENGER FARM PARK		4			\$30,000			\$30,000
COSPONSORED LEAGUES								
LLSOUTH-NEW LITTLE LEAGUE FIELD		4	\$50,000					\$50,000
YOUTH FOOTBALL LIGHTING IMPROVEMENTS		4		\$22,000				\$22,000
GARDINERS WOOD COMPLEX, PARKING LOT IMPROVEMENTS & LIGHTING		4			\$80,000			\$80,000
FENCING IMPROVEMENTS- LL SOUTH		4		\$50,000				\$50,000
FIELD LIGHTS - SENIOR SOFTBALL		4				\$80,000		\$80,000
PRESS BOX W/STORAGE-SENIOR SOFTBALL		4					\$20,000	\$20,000
EUGENE O'NEILL THEATRE CENTER								
SEWAGE CONNECTION ONEILL & WATERFORD BEACH		4	\$170,000					\$170,000

CAPITAL IMPROVEMENT PLAN FOR FY 2013-17

DEPARTMENT REQUESTS

FUNDING

LISTED IN DEPARTMENTAL PRIORITY ORDER

DESCRIPTION	DEPARTMENT	SOURCE	FY 2013	FY 2014	FY 2015	FY 2016	FY 2017	FY 2013-17
DEPARTMENT OF PUBLIC WORKS								
MUNICIPAL COMPLEX RENOVATIONS & CLEAN UP		4			\$6,313,774			\$6,313,774
RECONSTRUCTION: DOUGLAS LANE NO 2		4	\$99,000	\$1,199,000				\$1,298,000
RECONSTRUCTION: GARDINERS WOOD ROAD		4			\$169,000	\$1,973,000		\$2,142,000
ROAD RECLAMATION/MILL & OVERLAY: DIMMOCK RD		4	\$271,619					\$271,619
ROAD RECLAMATION/MILL & OVERLAY: DAYTON RD & FARGO RD		4		\$342,115				\$342,115
ROAD RECLAMATION/MILL & OVERLAY: WILLETS AVE		4			\$112,773			\$112,773
ROAD RECLAMATION/MILL & OVERLAY: CROSS ROAD-85 TO I-95		4				\$348,995		\$348,995
ROAD RECLAMATION/MILL & OVERLAY: BLOOMINGDALE RD		4					\$229,613	\$229,613
JORDAN COVE RD BRIDGE REPLACEMENT		4	\$380,000		\$2,292,000			\$2,672,000
SIDEWALK & TRAILS: BOSTON POST RD & GOSHEN ROAD		4	\$323,000					\$323,000
SIDEWALK & TRAILS: ROPE FERRY ROAD		4		\$291,000				\$291,000
SIDEWALK & TRAILS: PILGRIM & DAYTON RDS		4			\$308,000			\$308,000
SIDEWALK & TRAILS: NORMAN, VIVIAN, CROSS, SPITHEAD, GREENTREE		4				\$313,000		\$313,000
SIDEWALK & TRAILS: SHORE RD, JORDAN COVE RD		4					\$345,000	\$345,000
PARKWAY NORTH CONNECTOR		4			\$150,700	\$2,292,000		\$2,442,700
CONCRETE CURB REPLACEMENT		4		\$69,554	\$70,601	\$40,978		\$181,133
UTILITY COMMISSION								
LOGGER HILL ROAD SEWER LINE REHAB OR REPLACEMENT		4	\$1,200,000					\$1,200,000
RICHARDS GROVE ROAD PUMP STATION PARTIAL UPGRADE		4	\$422,000					\$422,000
FUTURE SSES/CMOM PROGRAM		5	\$33,000	\$33,000	\$33,000	\$33,000	\$33,000	\$165,000
LI SOUND NITROGEN REDUCTION		3	\$48,100	\$47,400	\$46,700	\$46,000	\$45,300	\$233,500
CROSS RD PUMP STATION PARTIAL UPGRADE		4		\$491,000				\$491,000
REPLACE MARILYN RD, WIEMES CT, EJECTOR STATIONS		4			\$975,000			\$975,000
STONEY BROOK PUMP STATION PARTIAL UPGRADE		4					\$511,000	\$511,000
BOLLES COURT PUMP STATION PARTIAL UPGRADE		4				\$468,000		\$468,000
HARVEY AVE PUMP STATION PARTIAL UPGRADE		4					\$446,000	\$446,000
REMAINING 17 PUMP STATIONS		4				\$672,000	\$675,000	\$1,347,000
NLWWTP CTDEP DRAFT ORDER		4					\$300,000	\$300,000
INFLOW AND INFILTRATION MITIGATION & CONTROL		4		\$319,000	\$335,000	\$352,000	\$300,000	\$1,306,000
CO-OP SEWER LATERAL EXTENSIONS		7		\$375,000	\$375,000	\$375,000	\$375,000	\$1,500,000
WASTEWATER SCADA SYSTEM UPGRADE		4		\$241,000	\$241,000	\$241,000		\$723,000
PORTABLE EMERGENCY GENERATORS, SWITCHES, ETC		5		\$150,000				\$150,000
GRAND TOTALS			\$5,579,541	\$6,001,143	\$13,517,396	\$8,479,223	\$4,449,913	\$38,027,216

FIVE YEAR CAPITAL PLAN-SUMMARY

DEPARTMENT REQUESTS

SUBTOTALS: FUNDING SOURCE

	FUNDING SOURCE	FY 2013	FY 2014	FY 2015	FY 2016	FY 2017	FY 2013-17
CURRENT YEAR CAPITAL IMPROVEMENTS (CYC)	1	\$1,345,567	\$1,191,900	\$1,129,000	\$1,095,000	\$1,095,000	\$5,856,467
OPERATING BUDGETS	2	\$16,000	\$16,000	\$0	\$0	\$0	\$32,000
WASTE WATER BUDGET/OR SEWER CAP. MAINT. FUND	3	\$48,100	\$47,400	\$46,700	\$46,000	\$45,300	\$233,500
TRANSFER TO CAPITAL & NON-REC. (CNR)	4	\$4,136,874	\$4,187,843	\$11,933,696	\$6,930,223	\$2,901,613	\$30,090,249
SHORT AND LONG TERM DEBT FINANCING	5	\$33,000	\$183,000	\$33,000	\$33,000	\$33,000	\$315,000
LOCAL CAPITAL IMPROVEMENT PROGRAM (LoCIP) OR GRANTS	6	\$0	\$0	\$0	\$0	\$0	\$0
DESIGNATE FUNDS FROM UNDESIGNATED PROJECTS & FUND BALANCE CNR	7	\$0	\$375,000	\$375,000	\$375,000	\$375,000	\$1,500,000
EXISTING FUNDS AVAILABLE IN CNR EXPENDITURE FUND	8	\$0	\$0	\$0	\$0	\$0	\$0
GRAND TOTALS		\$5,579,541	\$6,001,143	\$13,517,396	\$8,479,223	\$4,449,913	\$38,027,216

SUBTOTALS: DEPARTMENTS

SELECTMEN		\$1,558,100	\$1,095,000	\$1,095,000	\$1,095,000	\$1,095,000	\$5,938,100
BOARD OF EDUCATION		\$71,000	\$85,000	\$0	\$0	\$0	\$156,000
INFORMATION TECHNOLOGY COMMITTEE		\$37,250	\$39,274	\$144,348	\$19,250	\$0	\$240,122
ASSESSOR/FINANCE DEPT		\$75,000	\$75,000	\$75,000	\$75,000	\$75,000	\$375,000
LIBRARY		\$0	\$0	\$0	\$0	\$0	\$0
FIRE		\$661,000	\$721,000	\$625,000	\$25,000	\$0	\$2,032,000
POLICE DEPARTMENT		\$0	\$170,000	\$0	\$30,000	\$0	\$200,000
RECREATION AND PARKS		\$99,817	\$157,800	\$121,500	\$80,000	\$20,000	\$479,117
SENIOR SERVICES		\$12,155	\$0	\$0	\$0	\$0	\$12,155
YOUTH SERVICES		\$86,500	\$0	\$34,000	\$0	\$0	\$120,500
FLOOD AND EROSION CONTROL BOARD		\$0	\$100,000	\$0	\$0	\$0	\$100,000
DEPARTMENT OF PUBLIC WORKS		\$1,073,619	\$1,901,669	\$9,416,848	\$4,967,973	\$574,613	\$17,934,722
UTILITY COMMISSION		\$1,703,100	\$1,656,400	\$2,005,700	\$2,187,000	\$2,685,300	\$10,237,500
EMERGENCY MANAGEMENT		\$32,000	\$0	\$0	\$0	\$0	\$32,000
EUGENE O'NEILL THEATRE CENTER		\$170,000	\$0	\$0	\$0	\$0	\$170,000
GRAND TOTALS		\$5,579,541	\$6,001,143	\$13,517,396	\$8,479,223	\$4,449,913	\$38,027,216
		\$0	\$0	\$0	\$0	\$0	\$0



To: Boards, Agencies, Commissions and Administrative Staff

Date: October 17, 2011

RE: 5-YEAR CAPITAL IMPROVEMENT PLAN

FISCAL YEARS 2013-2017

Once again it is time to start the preparation of our 5-year Capital Improvement Plan. The purpose of this program is to provide the Board of Selectmen, Board of Finance, and RTM with a comprehensive multi-year plan on anticipated appropriations for Capital Improvements. This process begins now so that the plan can be completed in time for the Fiscal Year 2013 budget review.

Two factors will affect your preparation of this year's plan. First is the increased cost of healthcare on the operational budget and second is the increasing amount of the budget being dedicated to debt service associated with bonding the school construction projects. As a result we have seen significant reductions in the amount of money set aside for capital improvement funding. Over the years the Town has invested wisely in infrastructure improvements when the revenue was there. It is now time to focus our attention on maintaining these improvements so that our investment achieves or exceeds its useful life. To that end, I do not expect new initiatives unless there is a clear and measurable benefit and savings to operations.

PLAN COMPONENTS

Attached you will find the CIP for FY 2012-2016 with the actual approved funding levels for fiscal year 2012. This document shows the First Selectman's recommended plan as ratified by the Board of Selectmen. A similar document will be prepared for FY 2013-2017 for ratification by the Board of Selectmen and use by the Board of Finance and RTM. Projects to be funded in Fiscal Year 2013 should be described in detail. All projects require written justification. List all projects on the attached Project Consolidation Form by anticipated funding source and appropriation year. The attached Project Description Form is to be provided for each project. **This year projects are to be listed in priority order as determined by the Department or Agency.**

FUNDING SOURCE:

The two primary funding sources that are reviewed during the upcoming fiscal year are the Current Year Capital Improvements Account and the Capital and Non-Recurring Expenditure Fund. All capital improvements must be included in your CIP request even if they are to be funded through other sources. A list of Funding Source(s) is on the bottom of the Project Consolidation Form. **While you should indicate on the appropriate form the recommended funding source, the First Selectman is responsible for recommending the funding source.**

Projects to be included must meet the definition of a Capital Improvement. Projects funded through the Capital and Nonrecurring Expenditure Fund are required to be resubmitted for appropriation by the Board of Finance and RTM prior to project initiation. Even though projects must be resubmitted for appropriation, your justification for the project now should be no less detailed or complete. Prior to making a request for an appropriation from this fund you must submit your request to the First Selectman for a letter as to the consistency of the project with the CIP as ratified.

DEFINITION OF A CAPITAL IMPROVEMENT

NOTE: Capital Improvements will not be accepted for amounts below \$10,000. In the past this limit applied only to vehicles and equipment. For items/projects estimated below \$10,000, include these in your operating budgets. Please consult with Rudie Beers, Finance Director on the appropriate line item to use.

For purposes of this plan, a Capital Improvement shall mean one or all of the following:

1. Acquisition or lease of land.
2. All new building construction, including additions to existing buildings.
3. Reconstruction of a building for an alternative use or substantial rehabilitation of an existing building, as opposed to normal routine maintenance of an existing facility.
4. Installation, reconstruction, extension, or improvement of new or existing roads, bridges, drainage structures, flood control projects, sewer lines, water lines, or other public utilities.
5. Creation of new, expansion of or improvement to, existing outdoor uses of land and coastal waters including, recreational facilities, parking facilities, accessory facilities, water quality improvements and cove dredging.
6. All necessary architectural, engineering and feasibility analysis related to a planned Capital Improvement as defined in 1-5 above.
7. Equipment in excess of \$10,000 that is not governed by the Fleet Management Plan.
8. Information technology (IT) software and equipment.

OTHER REQUIREMENTS:

1. Each department has received from the Director of Finance a request to update their vehicle information so that a final FY 2013-2017 **Fleet Management Plan** can be prepared. Each department will get a copy of the final updated plan. The Department's vehicles/equipment replacement plan will be included as part of your CIP submission. If the Department is requesting any variation from what is in the plan it **must** be justified in a written narrative. If there are no variations, each Department must acknowledge their intent to replace vehicles/equipment in FY12 in accordance with the Plan and submit documentation to justify the scheduled replacement in accordance with the Board of Finance guidelines.
2. All computer hardware and software must be submitted through the Information Technology Committee. Contact Rudie Beers, Chairman of the IT Committee.
3. Submission of a project for inclusion into the CIP does not constitute a budget submission for FY 2013. Contact Rudie Beers, Finance Director regarding requirements for submission of a capital project to be funded from an operating budget, the Current Year Capital Improvement Budget, or the Capital and Nonrecurring Expenditure Fund.
4. Your submission does not fulfill your agency's requirement to submit to the Planning and Zoning Commission a separate request for a municipal improvement report pursuant to CGS 8-24. Contact Tom Wagner, Planning Director at 444-5813 on this process.

SUBMISSION DEADLINE:

In order to allow adequate time for review, **submit by email in Word format to my office by November 10, 2011.** If you cannot submit in digital form (6) paper copies will be required (no staples please). **These forms are located on the G Drive under budget forms and are in Word format.** If you do not have any items which require submission please confirm this in writing by November 10, 2011.

Sincerely,

Daniel M. Steward
First Selectman

DISTRIBUTION LIST CAPITAL IMPROVEMENT PROGRAM

Board of Selectmen

Moderator, Representative Town Meeting

Majority Leader, Representative Town Meeting

Minority Leader, Representative Town Meeting

Chairman, Board of Finance

Chairman, Board of Education

Treasurer

Finance Director

Human Resources Director

Superintendent of Schools

Youth Service Director

Fire Administrator, Fire Commission

Flood & Erosion Control Board

Library Director

Police Chief / Emergency Management Director

Public Works Director

Recreation and Parks Director

Senior Services Director

Assessor

Tax Collector

Town Clerk

Chief Engineer, Utility Commission

Director of Building and Grounds, Board of Education

Business Manager, Board of Education

Planning Director

Eugene O'Neil Theater Center

CAPITAL IMPROVEMENT PLAN
PROJECT DESCRIPTION FORM

A) AGENCY:

B) PROJECT NAME:

C) CONTACT PERSON:

D) DEPARTMENT'S PRIORITY: #

E) DESCRIPTION:

- a) Describe the type, purpose, and anticipated accomplishments of the project.
- b) Indicate the progress to date on the project.
- c) List other projects it is coordinated with whether in your Department or with other Departments.
- d) Describe the affect the project will have on your annual Department expenditures:
- e) Attach plan, estimate, service area map and/or other support documentation.

CAPITAL IMPROVEMENT PLAN: PROJECT CONSOLIDATION FORM-FY2013-2017							
DEPARTMENT/AGENCY:							
PROJECT NAME: IN ORDER OF DEPT. PRIORITY	S O U R C E	FY-2013	FY-2014	FY-2015	FY-2016	FY-2017	TOTAL FY 2013-2017
1							
2							
3							
4							
5							
6							
7							
8							
9							
TOTAL							

INDEX TO FUNDING SOURCES,

1 = CURRENT YEAR CAPITAL IMPROVEMENTS

2 = OPERATING BUDGETS,

3 = WASTE WATER BUDGET/SEWER CAPITAL MAINTENANCE FUND

4 = TRANSFER TO CAPITAL & NONRECURRING.

5 = SHORT AND LONG TERM DEBT FINANCING

6 = LOCAL CIP & OTHER GRANTS

7 = DESIGNATE FUNDS FROM UNDESIGNATED PROJECTS CNR;

8 = FUND FROM EXISTING DESIGNATIONS CNR

CAPITAL IMPROVEMENT PLAN: PROJECT CONSOLIDATION FORM-FY2013-2017
DEPARTMENT/AGENCY: Selectmen

PROJECT NAME: IN ORDER OF DEPT. PRIORITY	SOURCE	FY-2013	FY-2014	FY-2015	FY-2016	FY-2017	TOTAL FY 2013-2017
1 Cohanzie School Remediation	7	263,100					263,100
2 Cohanzie School Demolition	7		200,000				200,000
3							
4							
5							
6							
7							
8							
9							
TOTAL							463,100

INDEX TO FUNDING SOURCES,

1= CURRENT YEAR CAPITAL IMPROVEMENTS

2 = OPERATING BUDGETS,

3 = WASTE WATER BUDGET/SEWER CAPITAL MAINTENANCE FUND

4 = TRANSFER TO CAPITAL & NONRECURRING.

5 = SHORT AND LONG TERM DEBT FINANCING

6 = LOCAL CIP & OTHER GRANTS

7 = DESIGNATE FUNDS FROM UNDESIGNATED PROJECTS CNR;

8 = FUND FROM EXISTING DESIGNATIONS CNR

CAPITAL IMPROVEMENT PLAN
PROJECT DESCRIPTION FORM

A) AGENCY: First Selectman

B) PROJECT NAME: Demolition of Cohanzie Elementary School

C) CONTACT PERSON: Thomas V. Wagner, AICP

D) DEPARTMENT'S PRIORITY: #

E) DESCRIPTION:

- a) Describe the type, purpose, and anticipated accomplishments of the project.
- b) Indicate the progress to date on the project.
- c) List other projects it is coordinated with whether in your Department or with other Departments.
- d) Describe the affect the project will have on your annual Department expenditures:
- e) Attach plan, estimate, service area map and/or other support documentation.

The Cohanzie Elementary School was turned over to General Government by the Board of Education. The demolition of this building is proposed since it currently has no heat and has been used for emergency training and storage. Efforts to find an adaptive reuse of the building included a public solicitation and more recently a proposal to work with AHEPA to build a senior housing project similar to the one on Clark Lane.

Funding from the Small Cities Community Development Block Grant program was requested to cover the cost of demolition, but the project was not funded. The purpose of the demolition is to remove the liability associated with retaining this building including the operating costs, specifically electricity.

Options for reuse of the property will improve with the demolition. A recent appraisal of the property indicated it was more valuable without the building. We are still interested in a senior housing project or other institutional use as a buffer between the commercial activity on route 85 and the residential areas along Kenyon and Dayton Roads. Something compatible with the softball field and fire house across the street.

Based on an environmental report done for the grant application dated 5/31/2011 the costs associated with removal of the underground fuel tanks, lead, asbestos and demolition of the building, are as follows:

Remediation: \$198,000: includes

Removal and disposal of 6,000 & 8,000 gallon fuel tanks.

(extra costs not included for soil testing removal \$110/hr & \$120/ton)

Testing and removal of lead paint

Removal of Asbestos

Demolition: \$200,000

Environmental consultant oversight: \$23,000

Contingency @10%: \$42,100

TOTAL: \$463,100

Based on recent school demolition the cost per square foot is about \$10 X 41,000 square feet or \$410,000.

SOIL & ENVIRONMENTAL SERVICES, INC.

61 Plants Dam Road, East Lyme, CT 06333 860-739-6691 Fax 739-4150 soilinc@gmail.com

May 31, 2011.

Re: AHEPA Chapter 250, Inc., Cohanzie School, Waterford, CT

APPROXIMATE COSTS

Asbestos removal, lead paint testing/removal, and potential PCB oil unit disposal.

- Quotes from ~\$193,000.00 to \$198,000.00

UST removal, UST containment removal and piping, disposal of boiler room containers, and complete demolition and disposal of building.

- Quotes ~\$200,000.00

Total Demolition Cost ~ \$393,000.00 to \$398,000.00

NOTE: This is within normal costs of approx. \$10.00 per square foot. The building is 41,000 sq.ft X \$10.00/sq. ft. = \$410,000.00

SES, Inc. to collect and analyze soil samples in the tank graves according to CTDEP UST protocol. SES, Inc. personnel will be present for the removal to insure that the operation has been completed properly. A Final UST Closure Report will be issued to AHEPA Chapter 250, Inc., the Town, and HUD to document that the UST operation is completed. Our firm will also update the CTDEP UST Notification Form which needs to be re-filed.

SES Cost ~\$23,000.00

Grand Total Estimated Cost ~ \$421,000.00

SOIL & ENVIRONMENTAL SERVICES, INC.

Re: AHEPA Chapter 250, Inc., Cohanzie School, Waterford, CT

Potential Other Costs:

If soil contamination is found below the USTs, charges by time and material (T & M) will most likely apply. We would expect a minor amount of soil removal will be required.

SES, Inc. Tasks: waste profile sheets prepared for Soil Recycling facility for soil disposal, additional composite soil samples to lab for disposal testing (Metals, PCBs, flashpoint, VOCs, etc.), on-site supervision, samples, and Reports. **\$110.00 per hour.**

Soil removal: Disposal fees are \$45/ton and labor-trucking is approx. \$50/ton. The excavated hole will require compacted fill. Machinery, time, materials, etc.

\$120.00 per ton

CAPITAL IMPROVEMENT PLAN: PROJECT CONSOLIDATION FORM-FY 2013-2017

DEPARTMENT/AGENCY: Waterford Senior Services

PROJECT NAME: IN ORDER OF DEPT. PRIORITY	S O U R C E	FY- 2013	FY- 2014	FY-2015	FY-2016	FY-2017	TOTAL FY 2013-2017
1 Chair Replacement for Waterford Community Center	4	\$12,155					\$12,155
2							
3							
4							
5							
6							
7							
8							
9							
TOTAL		\$12,155					\$12,155

INDEX TO FUNDING SOURCES,

1= CURRENT YEAR CAPITAL IMPROVEMENTS

2 = OPERATING BUDGETS,

3 = WASTE WATER BUDGET/SEWER CAPITAL MAINTENANCE FUND

4 = TRANSFER TO CAPITAL & NONRECURRING.

5 = SHORT AND LONG TERM DEBT FINANCING

6 = LOCAL CIP & OTHER GRANTS

7 = DESIGNATE FUNDS FROM UNDESIGNATED PROJECTS CNR;

8 = FUND FROM EXISTING DESIGNATIONS CNR

FIFTEEN ROPE FERRY ROAD



RECEIVED
WATERFORD, CT 06385-2886

NOV 21 2011

FINANCE DEPT.

TO: Daniel M. Steward
First Selectman

FROM: Sally B. Ritchie
Director of Senior Services

RE: Capital Improvement Program Fiscal Year 2013-2017

DATE: November 3, 2011

In response to your request, I am providing the requisite forms and supporting documentation for the Senior Services Capital Improvement Plan for Fiscal Years 2013-2017. The following documents are attached:

- Capital Improvement Plan Project Description Form
- CIP Project Consolidation Form
- Comparison on the Cinch and Strive Chairs
- Quote from Insalco (State Contract) for chairs to be provided through drop ship delivery
- Back up information on existing chairs

If you have any questions or concerns, you may contact me by calling x753. Thank you.

CAPITAL IMPROVEMENT PLAN
PROJECT DESCRIPTION FORM

A) AGENCY: Senior Services

B) PROJECT NAME: Replacement Chairs for Waterford Community Center

C) CONTACT PERSON: Sally B. Ritchie

D) DEPARTMENT'S PRIORITY: # 1

E) DESCRIPTION: To purchase replacement chairs for use in the multi-purpose/dining room in the Community Center. Originally purchased or donated in 2002, these chairs were moved from the Hartford Road facility when the center opened in 2005 as a cost saving measure for the Community Center Building Project. The chair welds are breaking, the chromed surfaces are rusting, and the upholstered seats are tearing and bending.

- a) Describe the type, purpose, and anticipated accomplishments of the project.
To purchase new stackable chairs that can withstand a minimum of 250 lbs, can be connected together safely for lectures, seminars, and programs, and can also provide seating for special luncheons and dinners, facility rental events, community band practices and performances, and fundraising events. Chairs will be used daily for lunches, and regularly for Duplicate Bridge, Pinochle, and seated fitness classes.
- b) Indicate the progress to date on the project.
Two quotes obtained through the Town's Purchasing Agent from vendors on the State contract.
- c) List other projects it is coordinated with whether in your Department or with other Departments. As this is a request for the Community Center it is being made in conjunction with the Recreation and Parks Department.
- d) Describe the affect the project will have on your annual Department expenditures:
While this project will have no affect on expenditures, it will eliminate the potential liability attributed to chair breakage as well as custodial time spent on repairing existing chairs.
- e) Attach plan, estimate, service area map and/or other support documentation.
See attached explanation and quote.

NOTE: The existing chairs are not holding up to the daily wear and tear that is required of furnishings in a multipurpose room. They are stacked and moved daily causing the short screws, which secure the vinyl covered fiberboard seat cushions to the frames, to work loose. The fiber board is also disintegrating. Individuals who appear to weigh between 200 and 300 pounds are breaking the chair welds.

	Chair Price w/ out Arms 2010	Chair Price w/out Arms 2011	Qty	Chair Price with Arms 2010	Chair Price with Arms 2011	Qty	Dolly Price 2010	Dolly Price 2011	Qty	Stack Height on Floor	Stack Height on Dolly	Can Connect for Use in Rows	Total Cost
Warranty													
Lifetime	\$77.35	\$75.60	124	\$105.85	\$111.75	12	\$184.14	\$179.96	8	15	12	Yes	\$12,155
10 year	\$78.00	\$81.00	124	\$106.50	\$105.50	12	\$171.50		0	4	0	No	\$11,310

Selected the chair that can withstand more weight, can be placed on a dolly for efficient room set up, and has a better warranty. Chairs can also be attached to each other to create row seating.

**INSALCO***"Your Contract Furniture Source"*

Insalco Corporation
7 Capital Drive
Wallingford, CT 06492
Phone: (203) 269-1238
Fax: (203) 265-9378
www.insalco.com

Customer: TOWN OF WATERFORD

Sales Rep: Evan Golden evan-insalco@snet.net
203-269-1238 (phone) 203-265-9378 (fax)**Proposal**

2010-3218

11/02/2011

Propose To: Installation Location:

TOWN OF WATERFORD
15 ROPE FERRY RD
WATERFORD, CT 06385-2886

Attention: Kate Rotella
 860-440-0540 (phone)
 860-440-0579 (fax)
 krotella@waterfordct.org

Senior Center Chairs

Item No.	Item Description	Qty	Item Sell	Ext Sell
Line: 1	INSALCO CORPORATION - Contract Information Office Case Goods Furniture Metal, Wood, Chairs	1	\$0.00	\$0.00
	Priced @ State Of Connecticut Contract Discount Contract #07PSX0275EA Sch "C" Discount = Delivered & Installed. Contract Duration: 6/01/2008 - 5/31/2013			
Line: 2	INSALCO CORPORATION - Contract Information SBE	1	\$0.00	\$0.00
	Insalco Corporation is a Certified Small Business Set-Aside Vendor to the State of Connecticut.			

Group Schedule "C" Delivered & Installed:

Item No.	Item Description	Qty	Item Sell	Ext Sell
Line: 3	National Office Furniture - Furniture Sales N45AP CINCH,PLASTIC STACKER,ARMLESS,BLACK,,PKG OF 4 Shell Color: Sold 4/Box \$ 80.44 per chair	34	\$321.75	\$10,939.50

Printed On: 11/02/2011 10:59 am

1 of 3

Customer: TOWN OF WATERFORD

Sales Rep: Evan Golden: evan.hgolden@wmu.edu
201-269-0758 (phone) 201-269-0758 (fax)

Proposal

2010-3218

11/02/2011

Item No.	Item Description	Qty	Item Sell	Ext Sell
Line: 4 N45MM	National Office Furniture - Furniture Sales Cinch Chair Arms, Sold Package of 4 ARMS NEED TO BE INSTALLED ON-SITE. PLATINUM/METALLIC ARMS AVAILABLE @ AN UPCHARGE	3	\$153.05	\$461.05
Line: 5 N45TD	National Office Furniture - Furniture Sales CINCH,PLASTIC STACKER, CHAIR DOLLY - BLACK CHAIRS STACK 12 HIGH ON DOLLY/15 HIGH ON FLOOR	8	\$191.57	\$1,532.56
	Group Schedule "C" Delivered & Installed Total:			\$12,933.91

Group Schedule "A" Drop Ship:

Item No.	Item Description	Qty	Item Sell	Ext Sell
Line: 6 N45AP	National Office Furniture - Furniture Sales CINCH,PLASTIC STACKER,ARMLESS,5, BLACK,,PKG OF 4 Shell Color: Sold 4/Box: \$ 75.60 per chair	34	\$302.35	\$10,278.50
Line: 7 N45MM	National Office Furniture - Furniture Sales Cinch Chair Arms, Sold Package of 4 ARMS NEED TO BE INSTALLED ON-SITE. PLATINUM/METALLIC ARMS AVAILABLE @ AN UPCHARGE	3	\$144.02	\$432.06
Line: 8 N45TD	National Office Furniture - Furniture Sales CINCH,PLASTIC STACKER, CHAIR DOLLY - BLACK CHAIRS STACK 12 HIGH ON DOLLY/15 HIGH ON FLOOR	8	\$179.95	\$1,439.60
	Group Schedule "A" Drop Ship Total:			\$12,150.04

Item Grouping Summary:

- Schedule "C" Delivered & Installed Total: \$12,933.91
- Schedule "A" Drop Ship Total: \$12,150.04

Printed On: 11/02/2011 10:43am

Page 3



Insalco Corporation
7 Capital Drive
Wallingford, CT 06492
Phone: (203) 269-1238
Fax: (203) 265-9379
www.insalco.com

"Your Contract Furniture Source"

Customer: TOWN OF WATERFORD
Sales Rep: Evan Golden evan@insalco.com
203-269-1238 (phone) 203-265-9379 (fax)

Proposal

2011-4238
10/31/2011

Propose To: Installation Location:

TOWN OF WATERFORD
15 ROPE PERRY RD
WATERFORD, CT 06385-2888

Administrator: Kate Rotella
860-440-0540 (phone)
860-440-0579 (fax)
krotella@waterfordct.org

Senior Center - KI - Strive

Group Schedule "C" Delivered & Installed:

Item No.	Item Description	Qty	Item Sell	Ext Sell
Line: 1	KRUEGER INTERNATIONAL - Furniture Sales	138	\$90.72	\$12,337.92
SLNAP	Strive, Four-Leg Armless Chair, Poly Seat & Back			
Line: 2	KRUEGER INTERNATIONAL - Furniture Sales	12	\$118.16	\$1,417.92
SLCAP	Strive, Four-Leg Cantilever Armchair, Poly Seat & Back			
Line: 3	KRUEGER INTERNATIONAL - Furniture Sales	1	\$90.72	\$90.72
SLNAP	Strive, Four-Leg Armless Chair, Poly Seat & Back			

Group Schedule "C" Delivered & Installed \$13,846.56
Total:

Group Schedule "A" Drop Ship:

Item No.	Item Description	Qty	Item Sell	Ext Sell
Line: 4	KRUEGER INTERNATIONAL - Furniture Sales	138	\$81.00	\$11,018.00
SLNAP	Strive, Four-Leg Armless Chair, Poly Seat & Back			
Line: 5	KRUEGER INTERNATIONAL - Furniture Sales	12	\$105.50	\$1,266.00
SLCAP	Strive, Four-Leg Cantilever Armchair, Poly Seat & Back			
Line: 6	KRUEGER INTERNATIONAL - Furniture Sales	1	\$81.00	\$81.00
SLNAP	Strive, Four-Leg Armless Chair, Poly Seat & Back			

Group Schedule "A" Drop Ship Total: \$12,363.00

Printed On: 10/31/2011 10:07 pm

1 of 2

Customer: TOWN OF WATERFORD
Sales Rep: Evan Golden evan.haden@gmail.com
301-384-0334 (phone) 208-348-0378 (fax)

Proposal

2011-4239

10/31/2011

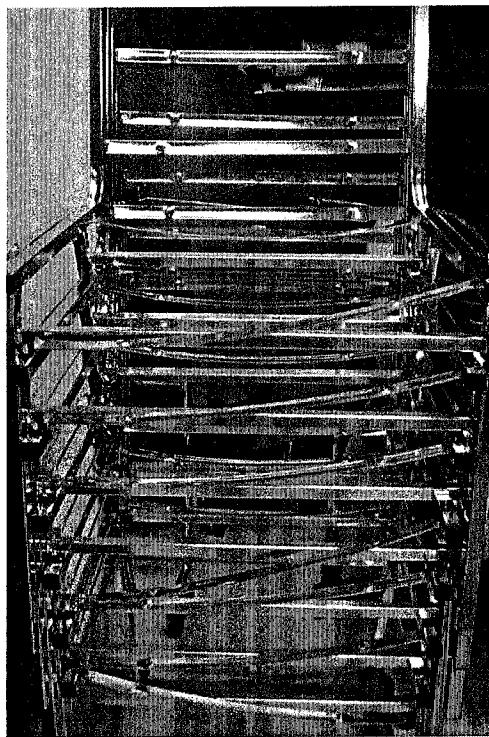
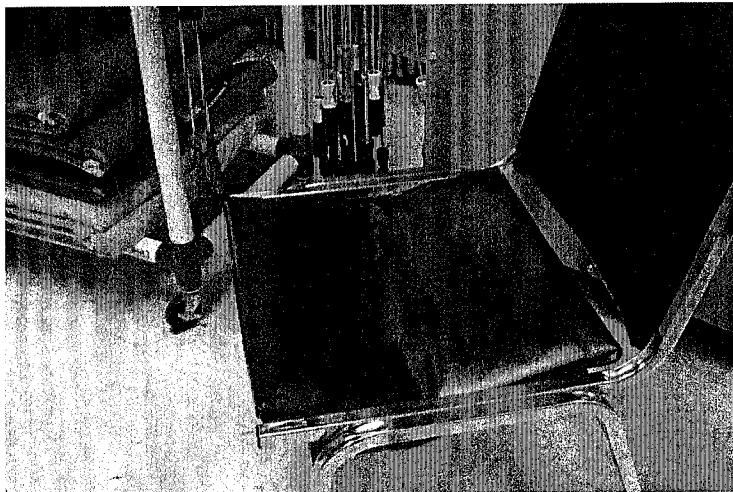
Item No.	Item Description	Qty	Item Sell	Ext Sell
Line 7	INSALCO CORPORATION - Contract Information Office Case Goods Furniture Metal, Wood, Chairs	1	\$0.00	\$0.00
Line 8	INSALCO CORPORATION - Contract Information SBE Insalco Corporation is a Certified Small Business Set-Aside Vendor to the State of Connecticut	1	\$0.00	\$0.00

Item Grouping Summary:

- Schedule "C" Delivered & Installed Total: \$13,848.58
- Schedule "A" Drop Ship Total: \$12,363.00

Thank you for your business! Please accept this proposal by signing below.

Accepted by: _____ Date: _____



Senior Services Department Capital Improvement Plan Back Up

2013-2017

Community Center Multi-purpose/Dining Room Chairs

Seating Capacity for Tables & Chairs in Dining Room 201

Total Seating Available Chairs Only in Dining Room 440

Average number of attendees at special functions 130

Date Purchased	Description	Initial Quantity	Chair Count 2010	Current Count 2011	Funding Source
Apr-02	Blue Vinyl armless	54			Waterford Senior Club Donation
Apr-02	Blue Vinyl armless	31			Senior Services Budget
May-05	Blue Vinyl armless	36			CC Building Project
Total		121	108	83	
2002	Blue Vinyl with arms	15	12	12	Waterford Senior Club Donation
	Total Frames	16		25	
	Disposed of due to welds failing and breaking while chair is occupied.				
	Total Seat cushions Repaired	15		12	

Town of Waterford

Utility Commission



Capital Improvements Program

FY 2013 to FY 2017

CAPITAL IMPROVEMENT PLAN
PROJECT DESCRIPTION FORM

A) AGENCY: Utility Commission
B) PROJECT NAME: Rehabilitation (or Replacement) of a section of sewers at Logger Hill Road
C) CONTACT PERSON: Chief Engineer – Neftali Soto
D) DEPARTMENT PRIORITY NUMBER: 1
E) DESCRIPTION:

- a) Describe the type, purpose, and anticipated accomplishments of the project.
- b) Indicate the Progress to date on the project.
- c) List other projects it is coordinated with whether in your Department or with other Departments.
- d) Describe the impact the project will have on your annual department expenditures:
- e) (Attach Conceptual Plan)

Early this past spring, during a routine inspection of our wastewater collection system at the Loggers Hill Road area, our staff discovered a major structural failure in a 2,400 foot section of the 24-inch concrete pipe that collects wastewater generated at the southwestern part of town, and all the wastewater generated from East Lyme. The attached (Appendix A) preliminary report/proposal prepared by Wright-Pierce Engineers, Inc. classifies the degree of deterioration of the pipes from a level on which the pipes may completely fail within 10 years, to a most critical level of failure within the next 5 years. As shown on the report, the scope of the work needed is of Capital nature, regardless of the alternative used. Preliminary estimates from Wright-Pierce indicate that corrective measures may cost in excess of \$1.6M. Since the final cost is unknown, and considering that the Town of East Lyme is also financially responsible for any work related to this section of our sewers infrastructure, a request of \$1.2M is hereby made. Please note that the condition of this pipe represents a serious danger to the stability of this important road artery. Furthermore, this reach of pipe, besides collecting wastewater generated at the southwestern section of Waterford, it also collects and transport to the Evergreen PS all the wastewater generated in the Town of East Lyme.

The writer uploaded videos of the condition of this pipe and manholes. The videos are posted on “YouTube.com”. Search under WUCChiefEng to see videos.

It is imperative that the condition of these pipes (and manholes) be corrected immediately. Not only does this situation pose a problem with the collection/transport of wastewater within the Town of Waterford and our ability to provide for the means for East Lyme to discharge into the New

London wastewater plant, but also the possibility of a major road collapse along this important route. Based on the East Lyme/Waterford agreement, both towns have financial responsibilities regarding the repairs to be done. This would include based on the approach that was taken five years ago on the rehab done at the Evergreen pump station, it appears that East Lyme cost-sharing for these repairs will be about 68.2% of the total cost, including engineering, construction cost, and contract administration.

In accordance to the Waterford/East Lyme agreement, the Town of East Lyme was put on notice regarding this event and the upcoming financial obligations.

Request: The designation of \$1,200,000 to Capital and Capital Non-Recurring to reline, or replace the above referred section of sewer pipe.

Although this request is for FY13, there is a very high possibility that funds will be requested prior to the beginning of the fiscal year., depending of the recommendations of our consultants.

CAPITAL IMPROVEMENT PLAN
PROJECT DESCRIPTION FORM

B) AGENCY: Utility Commission
B) PROJECT NAME: Wastewater Infrastructure Retrofit and Rehabilitation Program
C) CONTACT PERSON: Chief Engineer – Neftali Soto
D) DEPARTMENT PRIORITY NUMBER: 2
E) DESCRIPTION:

- f) Describe the type, purpose, and anticipated accomplishments of the project.
- g) Indicate the Progress to date on the project.
- h) List other projects it is coordinated with whether in your Department or with other Departments.
- i) Describe the impact the project will have on your annual department expenditures:
- j) (Attach Conceptual Plan)

For the past thirty-five years, the Town of Waterford embarked on a massive sewers extension program directed to protect our natural resources, improve our living standards, and to provide the proper environment and infrastructure to attract industrial and commercial institutions. Except for the need of sewers on various scattered areas around Town, the massive program of the 1970s, 80s, 90s and early 2000 is coming to an end. The one remaining major project is the Harrison's Landing sewers [which also includes about 300 feet of water and sewer lines at Maple La.]. A bid price of \$2.1M was received on November 18, 2011, and we are scheduling implementation during the spring of 2012. Funding for his project will be requested from Undesignated Capital Projects Fund Balance during the January 2012 BOS and BOF meetings, expecting final RTM funding approval in February 2012.

The Town of Waterford has a significant investment in its wastewater infrastructure that needs to be safeguarded for our future. Through the last three decades, the investment in wastewater infrastructure has been approximately \$120M. The present depreciated value is about \$65M. However, its replacement value could be estimated 2 ½ to 3 times its initial cost. An example of how significant construction cost has increased – the cost for the repair of about 20 feet of the force main at Smith Cove about three years ago was just shy of the initial total cost to install the whole force main [under the cove and dry land]. The time has come to address those needs. Another example is the cost for the on-going rehab of the Mago Point pump station.

The Utility Commission Maintenance Staff is currently responsible for the maintenance of about 145 miles of sewer mains, 27 pumping stations (with the Harrison's Landing PS it would be 28), about 245 grinder pumps and over 3,000 infrastructure appurtenances including:

- Manholes
- Air Release Structures
- Atmospheric Vent Structures
- Low Pressure Blow Off Structures
- Subsurface Odor Control Structures
- Metering Vaults
- Sand and Grit Basins
- Valve Chambers

The complexity of our system and its reliance on a significant number of electro-mechanical components working on a continuous highly corrosive environment requires for this program to be addressed promptly and aggressively.

About 7 ½ years ago, the Utility Commission retained the services of Wright-Pierce Engineering (WPE) to conduct evaluation studies and, based on such, develop a comprehensive major rehabilitation program based on the needs and the fact that our infrastructure system is showing significant signs of aging. WPE, together with UC staff, and the Office of the Planning Director developed the revised Wastewater Facilities Plan. The level of rehabilitation and other needs associated with our wastewater infrastructure system are detailed in the following reports:

- *Preliminary Evaluation of Capital Funding Needs for Waterford Collection System* – by Wright-Pierce Engineers, October 2004
- *Evaluation of Evergreen Wastewater Pump Station* – by Wright-Pierce Engineers, May 2004
- *Evaluation of Mago Point Wastewater Pump Station* - by Wright-Pierce Engineers, August 2004
- *Supervisory Control and Data Acquisition and Telemetry Evaluation* – by Wright-Pierce Engineers, June 2004
- *Infiltration and Inflow Analysis for the Waterford Utility Commission* – by Wright-Pierce Engineers May 2004
- *Staffing Evaluation* – by Wright-Pierce Engineers, August 2004
- *Odor Control Evaluation* – by Wright-Pierce Engineers, draft August 2004

The Waterford Utility Commission initiated the efforts to address the need to rehabilitate our infrastructure by conducting the above-referred studies. As a result of these studies, a short and long term wastewater capital improvements plan was developed. The required improvements or corrections were prioritized. The Evergreen PS was the first step towards these rehab needs. This station went through a comprehensive rehabilitation at a cost of \$1.4M. This placed the total investment on this station to approximately \$2.0M in the past three years, with \$1.059M funded through the General Fund and about \$950,000 from the UC Sewer maintenance Fund. The rehab was completed over a two ago. It is intended with this rehab to extend the useful life of this station for another 30 years.

The Mago Point pump station is presently going through an upgrade a total cost of \$862,000, and such rehab is almost complete. The generator at the Old Norwich Road PS was replaced using Sewer Maintenance and Development Funds.

The funding requirements to implement other findings and measures of this extensive wastewater infrastructure evaluation are detailed in the report *Preliminary Evaluation of Capital Funding Needs for Waterford Collection System* – by Wright Pierce Engineers, October 2004. This report includes a summary of the findings as well as a detailed implementation and funding plan developed through the combined efforts of the UC staff, the Director of Finance, and staff from the Connecticut Department of Environmental Protection Clean Water Fund Program (CWF) program. This funding plan provides a fiscally responsible approach to address the needs of our wastewater infrastructure.

It is important to recognize, however, that the Department of Environmental Protection Clean Water Fund (CWF) Program depends on legislative approval and State appropriation of funds to carry this program. The competition for funds through the State of Connecticut is fierce and considering the ranking that several of our projects may receive, funding through the CWF may not be feasible, or reliable. Although our CIP approached the funding of our rehab program through the CWF, reliance on such program may jeopardize our ability to implement the necessary measures presented in our consultant's capital funding needs report on a timely manner. The CWF funding approved by the State of Connecticut is not sufficient to cover all the funding applications submitted by the state wastewater pollution control authorities. **CT DEP officials have informed this writer that funding of the CWF will be reduced in upcoming years. Our priority ranking is very low as compared to other areas of the State. Therefore, it is very unlikely that CWF will be available for these upcoming projects in Waterford. The Town may want to consider other financing sources, such as bonding, for these most necessary infrastructure rehab work.**

OTHER MAJOR PUMP STATIONS)

Future years partial funding is requested for the other major "cast in place" pump stations in Town, which will be in need of a certain degree of upgrades within the coming years. The priorities for our other stations are being evaluated on a continuous basis, and these may change according to the needs.

Two years ago, funded through the SDMF, the emergency generator at the Harvey Avenue PS was replaced at a cost of \$320,000. Additional future minor rehab work at this pump station was expected to take place in the year 2013. However, according to staff this rehab can be moved a couple of years out to accommodate other priorities. As of today the expected rehab at the Harvey Ave. PS is planned for 2017.

The partial rehab of the Bolles Court PS was planned to take place in the year 2014. However, our maintenance staff has indicated that the Bolles Court generator (Solar turbine type) should be

replaced this [last] year, since parts are very hard to find and the generator itself is very unreliable. This is the same situation we had with the generator at the Harvey Avenue pump station. Together with the generator, the transfer switch and other minor components must be replaced. The design of the new generator at Bolles Ct. PS is being done, now. Although the estimated cost for future years was based on escalating estimates, it is recommended that this escalator still be applied for this project that is being moved forward. A partial rehab of the Bolles Ct. PS is expected on FY 13 [already approved under the FY 12 CIP]. The full rehab of Bolles Ct. PS was moved to 2016.

The replacement of the Marilyn Rd. and Wiemes Ct. ejectors are expected to take place in the year 2015. The partial upgrade of the Stony Brook PS is being scheduled for 2017. The partial upgrade to the Richards Grove PS is being moved to 2013 due to immediate need to upgrade the pumps at this station. This is an immediate need recommended within the Wastewater Facilities Plan. The Cross Road PS partial upgrade is planned to take place in the year 2014. The Utility Commission understands that this is a very aggressive rehabilitation and upgrade plan. However, the upgrade of the cast in place pump stations should not be extended beyond the year 2017.

The Board of Selectmen and the Board of Finance have both indicated their preference for the Utility Commission to carry-on some of these improvements with funding from the Sewer Development and Maintenance Fund (SDMF). The Utility Commission followed this directive. The WUC absorbed the cost of the replacement of the Harvey Avenue generator (and other minor improvements), as well as the replacement of valves at the Old Norwich Road pump station. The UC, through the SMDF, is also co-sharing the cost of the Bolles Ct. PS generator replacement.

However, it is important to understand and recognize that the SDMF is very limited, and already committed as requested by the BOS and BOF. This is also the fund that the UC utilizes in case of an emergency, or any unpredictable events such as the major failure that occurred at the Smith Cove a couple of years ago. The UC spent \$1.057M during this single event. The major source for this fund is sewer connection fees, and these are no longer generated at the pace they were in the past.

The planning of the implementation dates and funding sources for the rehabilitation of the wastewater pumping stations was carefully developed by the Utility Commission staff, Wright Pierce engineers, and the town of Waterford Director of Finance.

Request: this CIP requests for the designation of \$422,000 to Capital and Non-Recurring projects for the partial upgrade of the Richards Grove pump station during FY 12-13. This request represents moving this project from FY 15 (as previously planned) to FY 13. The partial upgrade of the Cross Rd. PS is scheduled for FY 2014. The Replacement of the Marilyn Rd and Weimes Ct. ejectors is scheduled for FY 15. The Bolles Ct. PS partial upgrade is scheduled for FY 2016. It is recommended that the rehab of the Harvey Avenue PS (previously planned for FY 2014) to be moved to FY 2017.

CAPITAL IMPROVEMENT PLAN
PROJECT DESCRIPTION FORM

A) AGENCY:

Utility Commission

B) PROJECT NAME:

NLWWTP –CTDEP Draft Order

C) CONTACT PERSON:

Chief Engineer – Neftali Soto

D) DEPARTMENT PRIORITY:

E) DESCRIPTION:

- a) Describe the type, purpose, and anticipated accomplishments of the project.
- b) Indicate the Progress to date on the project.
- c) List other projects it is coordinated with whether in your Department or with other Departments.
- d) Describe the impact the project will have on your annual department expenditures:
- e) (Attach Conceptual Plan)

Background

Through an inter-local agreement, the City of New London, the Town of Waterford, and the Town of East Lyme treat their wastewater at the Piacenti Regional Wastewater Treatment Plant in the City of New London. In 2006, the Connecticut Department of Environmental Protection issued a draft order to the three towns to insure that the treatment facility complied with the pollutant discharge permit. Furthermore, the CTDEP directed the communities to re-evaluate the plant and implement any necessary improvements to insure that the plant would have capacity for future projected flows based on the water supply plans of the three communities. DEP required for the City of London be the leader of this project.

Any necessary improvements to the wastewater treatment plant provide for the benefit of both, residents serviced by public sewers and, because the waste from the pump-out of septic systems is also discharged to the plant, residents on septic systems are also benefited.

A qualification-based selection (QBS) process was conducted. The firm AECOM engineering, a worldwide engineering company with offices in Glastonbury and Rocky Hill Connecticut, was preliminarily selected to conduct the necessary work. A scope of work was submitted to DEP for their approval. Once the scope of work is approved by DEP, the communities will enter into a fee negotiation with the consultant. The consulting fee will be equally shared by the three communities.

At this time there is not a clear understanding of what the necessary capital improvements at the treatment plant would be; therefore, the request shown for future years may change according to the findings and recommendations by the consultant. According to the wastewater inter-local

agreement capital improvements at the treatment plant will be cost shared by the City of London, the Town of Waterford, and the Town of East Lyme at 55, 30, and 15% respectively.

Through the Finance Director, the Town of Waterford requested from the New London Finance Department a full disclosure and itemization of the dollars that Waterford has paid new London on depreciation cost for the Wastewater Treatment Plant. These payments are intended to cover capital depreciation. Therefore, it is our opinion that such depreciation charges should cover capital improvements. However, depending on the consultant's report, and depending on the cost for those improvements necessary at the WWTP, then this request may be moot. At this point of time we need to wait for New London final financial report. Please note that at the time this CIP is being prepared, the consultant has not provided estimated cost for improvements. Therefore, this figure is submitted to provide a funding straight line approach.

DEP has already approved the refunding of 55% of the consulting cost. However, this refund will come after the work is conducted.

Request – at this time no funding is requested for this project until the various unknowns are resolved. A future request of \$300,000 for FY 17 is hereby included. However, as above indicated, such funding might not be necessary.

CAPITAL IMPROVEMENT PLAN
PROJECT DESCRIPTION FORM

A) AGENCY: Utility Commission
B) PROJECT NAME: Inflow and Infiltration – Mitigation and Control
C) CONTACT PERSON: Chief Engineer – Neftali Soto
D) DEPARTMENT PRIORITY:
E) DESCRIPTION:

- a. Describe the type, purpose, and anticipated accomplishments of the project.
- b. Indicate the Progress to date on the project.
- c. List other projects it is coordinated with whether in your Department or with other Departments.
- d. Describe the impact the project will have on your annual department expenditures:
- e. (Attach Conceptual Plan)

INFLOW AND INFILTRATION (I/I)

The Town of Waterford, East Lyme, and New London, through an Inter-local agreement, cost-share the operational and (non-capital) maintenance cost of the Piacenti Regional Wastewater Treatment Plant in New London. This cost is shared based on their proportional flow contribution into the plant. For the past years, New London has developed an aggressive I/I mitigation program within the City, which reflects in a reduction of their contribution percentage. The closing of a major corporation within the city will reflect in an additional decrease within the City and an increase of the contributing percentage from both Waterford and East Lyme. This change in flow contribution reflects into East Lyme and Waterford by increasing their proportional cost-share. Furthermore, with the advent of the Veolia Water Co. as the City's new contract operator, it required a significant increase to the plant's O&M cost. This is the reason that the UC was obligated to request an adjustment to the sewer rates for 2008-09 and 2009-10. Our treatment cost, which is just one of our UC operational expenditures, increased 76%, as compared to last year's. The increase is expected to be about \$620K for a single year.

Inflow and infiltration (I/I) is clean water entering the sewer system which increases the cost to pump and treat sewage. Infiltration is ground water leaking into the sewers from cracks or leaking joints in the sewer lines, manholes, or individual service laterals. Inflow is a storm water entering the collection system from roof leaders connections, catch basins tied into the sewers, foundation drains, sump pumps, area drains, and pavement runoff through manhole covers.

For the past six years, we have been [and continue] pursuing for funding from the State of Connecticut for the necessary evaluations and studies regarding the mitigation and control of clean water inflow and infiltration into our collection system. For this reason, the UC together with Wright-Pierce Engineers, developed an *Infiltration and Inflow Analysis*, dated May 2004. This report provides a preliminary and general concept regarding those areas where infiltration and inflow appears to be excessive. The inflow and infiltration of clean water into our collection system has increased our treatment cost (paid to the City of New London) to unacceptable levels. The State of Connecticut DEP program allows for a 55% cost of sewer system evaluation surveys (SSES) with a 45% contribution from the municipality. The studies can be implemented by stages based on areas that have been preliminarily prioritized for consideration.

Although the Town of Waterford qualifies and meets the requirements to obtain funding, State funds are very limited and most of the funding is directed to other communities with higher priorities as determined by their [State] priority ranking system. However, as funds may become available, any work approved by the State (not necessarily funded) may qualify for reimbursement, if the Town does the work on its own.

We are at a stage that waiting for State funds should not deter us from continuing the implementation of the on-going I/I mitigation and control program. The escalating treatment already being paid to the City would only reflect in the need to reevaluate and accelerate the adjustment of our sewer rates to meet this cost. Therefore, the mitigation of I/I is top priority to the Utility Commission.

Two years ago the UC hired Desell Construction to adjust some manhole covers and frames in town roads. This work was intended to remedy the inflow of storm water into the sewer system. This program will continue as necessary. This work, at a cost of \$37,200, was funded through the SDMF. Our very own staff has been on alert and inspecting manholes at those areas where high I/I are suspected.

The Board of Selectmen and the Board of Finance have expressed their full support to the mitigation of I/I by, for the past three years, approving the designation of \$775,000 for I/I mitigation.

Two years ago, the UC staff, together with our consultant, conducted an extensive evaluation of I/I at the area behind Stop and Shop on Boston Post Road. The cross country pipe located at this area, surrounded by wetlands, collects a significant amount of I/I. As the result of such investigations, last year the National Water Main Co. was hired to conduct I/I mitigation at a total cost of \$101,244.

The next area intended to be mitigated is the Harvey Avenue pump station watershed, followed by the Richards Grove watershed area. There is additional investigative work that needs to be done for this sewershed. It is our intention to do mitigation during FY 13.

It is our target to reduce our wastewater average daily discharge to within 20% of our average drinking water consumption in town. This will be determined once we obtain more drinking water data from our consumption-based sewers billing. Preliminary investigative work was conducted early last spring.

One of the most expensive aspects of conducting I/I studies is the need to TV the sewer lines. For previous investigative work conducted at the Seaside area, the cost to TV the lines was \$1.27 per foot or, \$6,705 per mile. Considering that the Town of Waterford has 145 miles of sewers this will reflect in a cost of \$972,000, just to TV our whole sewer system once. Standard sewer maintenance practices recommend that all the pipes be TV every 10 years. For Waterford, this represents an average of 14.5 miles per year at a cost of \$97,225 per year. We have been in conversations with the Director of Finance regarding the advantages of the Town of Waterford to own this type of technology, so that we can conduct these investigations in-house. The Assistant Director investigated the price of this technology. According to his research, we can have this technology and equipment in-house for \$100,000. Therefore, the CCTV system will be paid within the first year. The system researched by the Assistant Director is a trailer mounted multi-conductor unit. This unit will be towed by one of our utility vehicles. It is intended to have our own existing staff operate this equipment. Training and certification is provided by the vendor. There is no need for additional staff. Also note that this equipment would also be available to the Department of Public Works to troubleshoot any problems that they may have in their drainage system.

The Director of Finance indicated that this cost is justified under the I/I mitigation program. Our return on investment is almost immediate. Furthermore, this equipment will assist us to identify the location of illegal discharges, as well as pinpoint properties with a high discharge of grease, oils, and fats. The final benefit of identifying and eliminating these discharges is a reduction in the treatment cost.

Request - No additional designations are herein requested for this FY13. Last year the First Selectmen recommended to exhaust the \$775,000 already designated for I/I mitigation prior to additional designations. The Utility Commission is proceeding in that direction. Future years designations are listed on the Project Consolidation Form.

It is recommended that \$100,000 of the already designated \$775,000 (\$637,000 remaining) be used to acquire a CCTV system to reduce the expenditures associated with I/I investigative work and other benefits obtained by having this equipment readily available for our use 24/7.

CAPITAL IMPROVEMENT PLAN
PROJECT DESCRIPTION FORM

A) AGENCY:

Utility Commission

B) PROJECT NAME:

Wastewater Infrastructure -SSES/CMOM

C) CONTACT PERSON:

Chief Engineer – Neftali Soto

D) DEPARTMENT PRIORITY:

B) DESCRIPTION:

- a) Describe the type, purpose, and anticipated accomplishments of the project.
- b) Indicate the Progress to date on the project.
- c) List other projects it is coordinated with whether in your Department or with other Departments.
- d) Describe the impact the project will have on your annual department expenditures:
- e) (Attach Conceptual Plan)

For the past thirty years, the Town of Waterford embarked on a massive sewers extension program directed to protect our natural resources, improve our living standards, and to some degree provide the proper environment and infrastructure to attract industrial and commercial institutions. Except for the need of sewers in various scattered areas around Town, the massive program of the 1970s, 80s, 90s and early 2000 is coming to an end. The one remaining major project is the Harrison's Landing sewers, for which funds are already approved and, which will be aggressively pursued for implementation this year.

The Town of Waterford has a significant investment in its wastewater infrastructure that needs to be safeguarded for our future. Through the last three decades, the investment in wastewater infrastructure has been approximately \$120M. The present depreciated value is about \$65M. However, its replacement value could be estimated 2 ½ to 3 times its initial cost. An example of how significant construction cost has increased – the cost for last year's repair of the force main at Smith Cove was just shy of the total cost to install the force main [under the cove and dry land]. The time has come to address those needs.

The Utility Commission maintenance staff is currently responsible for the maintenance of over 144 miles of sewer mains, 27 pumping stations, about 255 grinder pumps and over 3000 infrastructure appurtenances including:

- Manholes
- Air Release Structures
- Atmospheric Vent Structures
- Low Pressure Blow Off Structures

- Subsurface Odor Control Structures
- Metering Vaults
- Sand and Grit Basins
- Valve Chambers

The complexity of our system and its reliance on a significant number of electro-mechanical components, working on a continuous corrosive environment, requires for this program to be addressed promptly and aggressively.

About 5 ½ years ago the Utility Commission retained the services of Wright-Pierce Engineering (WPE) to conduct studies and, based on such, develop a comprehensive major rehabilitation program based on the needs and the fact that our infrastructure system is showing significant signs of aging. The level of rehabilitation and other needs associated with our wastewater infrastructure system are detailed on the following reports:

- *Preliminary Evaluation of Capital Funding Needs for Waterford Collection System* – by Wright-Pierce Engineers, October 2004
- *Evaluation of Evergreen Wastewater Pump Station* – by Wright-Pierce Engineers, May 2004
- *Evaluation of Mago Point Wastewater Pump Station* – by Wright-Pierce Engineers, August 2004
- *Supervisory Control and Data Acquisition and Telemetry Evaluation* – by Wright-Pierce Engineers, June 2004
- *Infiltration and Inflow Analysis for the Waterford Utility Commission* – by Wright-Pierce Engineers May 2004
- *Staffing Evaluation* – by Wright-Pierce Engineers, August 2004
- *Odor Control Evaluation* – by Wright-Pierce Engineers, draft August 2004

In recognition to the aging wastewater infrastructure, the EPA is in the process of expanding the *Clean Water Act* with new rules which will require municipal systems to develop and implement a formal *Capacity, Management, Operation, and Maintenance Program (CMOM)*. The proposed CMOM program rules include requirements to continuously monitor and maintain wastewater collection systems in order to prevent sanitary sewers overflows (SSOs). In order to comply with this program, the municipality must continue to conduct detailed sanitary system evaluations and surveys (SSES). The UC initiated its SSES program four years ago and it is committed to continue it for the efficient, safe, and reliable operation of our system. Therefore, funds from the Sewer Development and Maintenance Fund (SDMF) are intended to be used for these endeavors.

Request: The UC will use \$33,000 from the SDMF in 2012-13. Future years will be funded as detailed on the project consolidation form, and depending on the balance of the SDMF.

C

CAPITAL IMPROVEMENT PLAN
PROJECT DESCRIPTION FORM

A) AGENCY: Utility Commission
B) PROJECT NAME: Long Island Sound Nitrogen Reduction
C) CONTACT PERSON: Chief Engineer – Neftali Soto
D) DEPARTMENT PRIORITY:

E) DESCRIPTION:

- a) Describe the type, purpose, and anticipated accomplishments of the project.
- b) Indicate the Progress to date on the project.
- c) List other projects it is coordinated with whether in your Department or with other Departments.
- d) Describe the impact the project will have on your annual department expenditures:
- e) (Attach Conceptual Plan)

A State mandate effective January 2001 required the New London sewage treatment facility to reduce the amount of discharged nitrogen by 56%.

This funding represents the Town of Waterford's share (\$780,000 at 2% interest over twenty years) of the improvements made to the regional treatment facility necessary to comply with the mandate.

Request: The use of \$48,100 on FY13 from the Sewer Development and Maintenance Fund.

CAPITAL IMPROVEMENT PLAN
PROJECT DESCRIPTION FORM

A) AGENCY: Utility Commission

B) PROJECT NAME: Sewers Extension Program

C) CONTACT PERSON: Chief Engineer - Neftali Soto

D) DEPARTMENT'S PRIORITY: – As determined by the recommendations of the Wastewater Plan and as needed.

E) DESCRIPTION:

- a) Describe the type, purpose, and anticipated accomplishments of the project.
- b) Indicate the Progress to date on the project.
- c) List other projects it is coordinated with whether in your Department or with other Departments.
- d) Describe the impact the project will have on your annual department expenditures:
- e) (Attach Conceptual Plan)

As recommended in the 1981 and 1998 partial Facilities Plan, a pre-determined amount of funds is required to extend lateral sewers into areas with pollution, health problems, and other concerns. All current requests are listed on the attached project- ranking sheet (Sewer Extension Program spreadsheet) and submitted by utilizing actual cost estimates rather than the \$500,000 historically requested in the past.

On the 2008-12 CIP, BOS ratified CIP, only the Harrison's Landing project was approved for funding. Other sewer extension projects are to be individually justified prior to funding. Under the 2009-13 CIP, funding was approved for the revision and development of the *Town of Waterford Comprehensive Wastewater Facilities Plan*. The development of the plan is on its final stages. The CT DEP requires for the sewer facilities Plan to be updated every ten years. The last partial revision was in 1998. The Waterford Utility Commission, together with the Planning Department has been [and continue] working during this past year developing this plan. The sewer facilities plan will assist the Town in identifying and justifying the future construction of sewers in Town, delineated which areas within the Town sewers would be permitted, as well as establishing future flow generation, and improvements needed at the treatment facilities. Capital improvements at the New London wastewater treatment facilities are to be 30% cost-shared by the Town of Waterford.

No new [sewer] projects are presented in this CIP. However, in order to maintain continuity with previous UC and DPW CIPs, older projects are herein presented.

The following are sewer extension projects expected to be incorporated into the Wastewater Facilities Plan under development:

Project

- Oil Mill/Gurley Rd. Sewers
- Hickory La./Dayton South
- Paula La./Ina La./Doyle Road
- Dimmock Rd/Pepperbox Rd.
- Bloomingdale Rd. South (from Gallows La. To Applewood Dr.)
- Cinderella La./ Woodworth Dr.
- Braman Rd./Marry Butler Rd.
- Rt. 32 Interceptor
- Stony Brook Interceptor

It is important to understand that the order of the projects herein presented could be altered, or projects added or deleted according to public health protection and pollution control needs priorities as they may develop and/or as recommended by the updated facilities plan.

The implementation of the long range Bloomingdale Rd. sewers project could be affected by the water system improvements or road improvements planned for the area. However, all the construction of sewers must be justified as requested by the BOS. This is one of the main purposes of the Wastewater Facilities Plan.

Request – No funding for the construction of sewers is requested for FY 12-13. However, future years potential sewer projects [expected to be recommended under the Wastewater Facilities Plan being developed] are herein included for information purposes only.

CAPITAL IMPROVEMENT PLAN
PROJECT DESCRIPTION FORM

C) AGENCY:

Utility Commission

B) PROJECT NAME:

Wastewater SCADA System

C) CONTACT PERSON:

Chief Engineer – Neftali Soto

D) DEPARTMENT PRIORITY:

D) DESCRIPTION:

- f) Describe the type, purpose, and anticipated accomplishments of the project.
- g) Indicate the Progress to date on the project.
- h) List other projects it is coordinated with whether in your Department or with other Departments.
- i) Describe the impact the project will have on your annual department expenditures:
- j) (Attach Conceptual Plan)

Supervisory Control and Data Acquisition (SCADA) system

The existing computerized system monitors the status of 27 wastewater pump stations. It is somewhat antiquated and does not provide the more extensive monitoring and control functions of modern systems. The SCADA evaluation recommended an eventual complete upgrade of this system in order to take full advantage of the capabilities of modern technology. The cost for a complete upgrade was estimated at approximately \$540,000 in 2006. This estimate is four years old, and the rehab of the SCADA system is not expected for at least two years.

This upgrade is not an immediate critical element, but does impact the efficiency of staff utilization and costs. Many utilities have had a positive return-on-investment for SCADA upgrade in terms of less labor cost, for emergency call outs, and for routine pump station monitoring. It is recommended that the system be totally upgraded within the next three years.

A more detailed evaluation will be conducted to determine the actual needs, which technology will provide the Town of Waterford with the fastest return-on- investment, and a more accurate cost.

Request: The future designation to CNR of \$241,000 each year for the years 2014, 20154, and 2016 for the upgrade of the SCADA system.

CAPITAL IMPROVEMENT PLAN
PROJECT DESCRIPTION FORM

A) AGENCY: Utility Commission

B) PROJECT NAME: Fleet Management (Vehicles Replacement)

C) CONTACT PERSON: Chief Engineer – Neftali Soto

D) DEPARTMENT PRIORITY:

E) DESCRIPTION:

- a) Describe the type, purpose, and anticipated accomplishments of the project.
- b) Indicate the Progress to date on the project.
- c) List other projects it is coordinated with whether in your Department or with other Departments.
- d) Describe the impact the project will have on your annual department expenditures:
- e) (Attach Conceptual Plan)

REQUEST: None.

Appendix A



— - - - - Damaged Section

October 24, 2011
W-P Project No. T9453

Mr. Peter M. Green
Chairman
Town of Waterford Utility Commission
15 Rope Ferry Road
Waterford, CT 06385

Subject: Proposal for Sewer Rehabilitation - Preliminary Design
Logger Hill Road Sewer Main

Dear Mr. Green:

As requested, we have completed a preliminary review of the closed-circuit television (CCTV) pipe inspection of the gravity sewer mains on Rope Ferry Road, Logger Hill Road, B Lane and North Road. Approximately 2,400 linear feet of pipe was inspected between Manhole No. 24 at the intersection of Rope Ferry Road and Gallup Lane to Manhole No. 11 at the bottom of Logger Hill Road. The pipes inspected were reported to be made of pre-stressed concrete cylinder pipe (PCCP) and range in size from 24-inch to 36-inch diameter. The inspection videos were reviewed for deficiencies in order to identify pipeline sections in need of immediate repair to avoid the potential for a catastrophic pipe failure. After review of the television inspection videos, it was observed the 1,420 feet of pipe between Manhole No. 20 on Rope Ferry Road to Manhole No. 11 at the bottom of Logger Hill Road were observed to have grade 5 defects in need of immediate attention in accordance with current NASSCO (National Association of Sewer Service Companies) PACP (Pipeline Assessment Certification Program) standards for the identification of pipeline defects. Grade 5 pipe defect are those pipe segments that exhibited severe deterioration of the pipe interior walls due to corrosion including visible decreased pipe wall thicknesses and areas where the steel cylinder pipe has been exposed due to failed concrete coatings. Grade 5 defects have the potential to fail within the next 5-years. An example of a grade 5 defect is shown in Figures No. 1.



Figure 1 - Failed Concrete Coating

The remainder of the pipeline televised between Manhole No. 24 and Manhole 20 were observed to have grade 4 defects in accordance with current NASSCO PACP standards. Grade 4 pipe defect are those pipe segments that exhibited moderate deterioration of the pipe interior walls due to corrosion

including visible decreased pipe thicknesses. Grade 4 defects have the potential to fail within the next 10-years. An example of a grade 4 defect is shown in Figure No. 2.

In addition to the review of the television inspection DVDs, visual inspections of Manhole No. 11 through Manhole No. 24 were conducted by Wright-Pierce and WUC staff and again by National Water Main Cleaning Company of Canton, Massachusetts. Results of the inspections indicated that the manholes on Logger Hill Road are severely deteriorated with areas of exposed reinforcing steel and aggregate. It is recommended that the rehabilitation of these manholes be conducted as part of the project. The scope of work for manhole rehabilitation should include high pressure 5,000 psi water blasting, spray application of up to 1-inch of fiberglass reinforced cement, and the application of a 100 mil layer of epoxy coating.

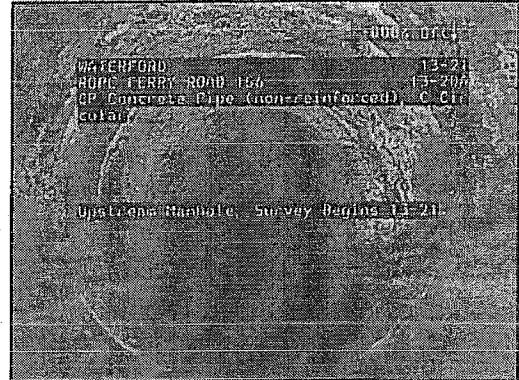


Figure 2 - Interior Corrosion

As part of the proposal effort, field sampling of the Mago Point Pump Station influent channel and twelve gravity sewer manholes along the sewer interceptor were inspected visually for corrosion and tested for field measurements of oxidation-reduction potential (ORP), hydrogen sulfide gas and dissolved oxygen (DO). The purpose of this testing was to try and determine the source(s) of the hydrogen sulfides so that a future evaluation could be conducted to try and reduce the downstream impacts at the source. As expected, the ORP reading of -70.0 mV and DO of 0.0 ppm at the East Lyme force main discharge indicate slightly anaerobic conditions. The results also indicate that as the wastewater is further aerated downstream at the drop manholes on Logger Hill Road, the DO is increased and dissolved sulfides are stripped from the wastewater and condensed to sulfuric acid on the exposed pipe interior walls, leading to the experienced corrosion and deterioration of the gravity sewer piping. Although the one day of field sampling and visual inspections confirmed our suspicions, additional field testing would need to be conducted to determine actual point sources and methods of control.

A summary of these measurements are presented in the Table No. 1. Manholes No. 19 through Manhole No. 24 on Rope Ferry Road were not inspected due to safety concerns with traffic.

REPAIR ALTERNATIVES

In addition to complete pipe replacement, a preliminary assessment of the available alternatives for rehabilitating the pipe was evaluated. The following options have been reviewed with contractors/vendors, specifically the Ted Berry Company of Livermore, Maine, for rehabilitating the deteriorated 24" PCCP gravity sewer in Waterford, CT:

- CIPP Lining - May be possible, however would need to be extremely thick to return structural strength. Cost may approach that of pipe bursting.
- Pipe Bursting - Possible depending on year pipe was installed and the detail of reinforcement. Although the concrete is visibly deteriorated, the steel reinforcing rings may not want to break and would pile up in front of bursting head. Manholes would be removed/replaced; manholes

Mr. Peter M. Green
 October 24, 2011
 Page 3 of 4



would act as pit locations for bursting. Ted Berry would want to see CCTV and general topo/profile of existing pipe.

- Carbon Fiber Reinforcement - The use of a carbon fiber reinforced polymer is rising as a rehabilitation alternative to replacement of large diameter PCCP. I have reviewed a couple of products but am trying to get some questions answered. Some products are not yet approved for wastewater and 24" may be too small for robotic application. Probably not a viable option, just wanted to explore it a bit more.
- Sliplining - Based on the difference in elevations and the length of pipe (2,400 feet), sliplining may be best option. Ted Berry discussed using a fusible 21" PVC pipe and adjusting slopes between manholes to make up the difference in capacity lost by the reduction of pipe size. This would be much less expensive than bursting or open cut. Manholes would be removed/replaced for this option also.

Complete pipe replacement would be the most expensive and would require extensive excavation and road repair work including portions on a state highway.

Table No. 1 - Field Sampling Results

Location		Time	Nuisance Odors	Visible Corrosion	ORP (mV)	H2S (ppm)	Temp. (F)	DO (%)	DO (ppm)	Notes
Rope Ferry Road	Mago Point P.S. Wet Well	9:58	No	No	+ 5.0	0	-	31.5	2.88	
	MH #22 @ Gallup Ln.	10:12	No	Yes	- 70.0	0	-	0.0	0.00	East Lyme P.S. discharge
	MH #22 @ Gallup Ln.	10:23	No	Yes	+ 22.0	0	59	0.0	0.00	Mago Point P.S. discharge
	M.H. No. 24 STA 43+55			No						Not inspected on 10/6/11 but reported on by the Town Staff
	M.H. No. 23 STA 41+70			No						
	M.H. No. 22 STA 39+65			No						
	M.H. No. 21 STA 37+50			1						
	M.H. No. 20A STA 36+25			No						
Logger Hill Road	M.H. No. 20 STA 34+35			Yes						
	M.H. No. 19 STA 32+75			Yes						
	M.H. No. 18 STA 31+20	10:34	Minor	Yes	+ 15.0	0	-	42.7	4.23	
	M.H. No. 17 STA 30+10	10:45	Minor	Yes	+ 17.2	0	-	57.0	5.32	
	M.H. No. 16 STA 29+00	10:48	Minor	Yes	+ 18.1	0	66	53.2	4.95	
	M.H. No. 15 STA 27+80	11:06	Minor	Yes	- 50.7	3.6	67	59.1	5.29	
	M.H. No. 14 STA 26+70	11:14	Minor	Yes	+ 13.5	0	67	67.1	6.15	
	M.H. No. 13 STA 25+20	11:22	Minor	Yes	+ 19.1	0	66	65.0	6.03	
	M.H. No. 12 STA 23+65	11:32	Minor	Yes	+ 5.9	0	67	65.6	6.02	
	M.H. No. 11 STA 22+30	11:37	No	Minor	+ 15.4	0	67	66.8	6.21	
	M.H. No. 10 STA 0+00	11:41	Minor	No	+ 21.4	0	-	69.4	6.46	
	M.H. No. 7 STA 12+70	12:01	No	No	+ 27.3	0	67	55.1	5.03	Located on B Lane
	M.H. No. 4 STA 0+40	11:50	Minor	No	+ 20.6	0	63	55.1	5.23	Located on North Road

At this time, it is recommended that each of the above options be evaluated in greater detail as part of a preliminary design phase effort in order to ensure the most feasible and cost effective option is selected. Our proposed scope for preliminary design phase services is as follows:

Mr. Peter M. Green
October 24, 2011
Page 4 of 4



Preliminary Design Phase

1. Provide information to and meet with sub-consultant Ted Berry in order to discuss findings of television report and potential construction constraints. At this meeting it is anticipated that the feasibility of pipe bursting and sliplining will be determined.
2. Conduct a one (1) day site visit with our sub-consultant in order to determine existing conditions regarding the following:
 - i. Verify existing pipe material.
 - ii. Identify surrounding utilities and other constraints regarding replacement of the sewer line.
3. Develop conceptual cost of complete replacement of the sewer line and manholes.
4. Provide preliminary design memorandum that will summarize the findings and recommend an alternative for implementation with a scope of work, including a preliminary construction cost estimate.
5. Attend one meeting with WUC staff to review the findings and recommendations of the preliminary design prior to preparing contract bidding documents.

Wright-Pierce proposes to complete the above scope of work on a time charge basis with a fee not to exceed \$7,500 without written authorization from the WUC. At this time, this fee does not include the development of the Final Design documents or Bidding and Construction Administration assistance for this project. It is anticipated that after the completion of the preliminary design, and once a repair alternative is selected, the scope and fee for Final Design, Bidding and Construction Administration services will be developed and submitted for approval.

If the proposed scope and fee are acceptable, please sign where indicated below and return one signed copy of this proposal to Wright-Pierce. We are prepared to begin this work immediately upon WUC authorization and would conduct this effort as an Additional Service under the terms and conditions of our existing Engineering Agreement.

Should you have any questions or desire additional information, please call me or Dennis Dievert at 860-343-8297.

Very truly yours,

WRIGHT-PIERCE

John W Braccio, P.E.
Vice President

Accepted by Waterford Utility Commission
Peter M. Green, Chairman

cc: Neftali Soto
Jim Bartelli

October 28, 2011
W-P Project No. T9453

Mr. Neftali Soto, P.E.
WUC Chief Engineer
Town of Waterford Utility Commission
15 Rope Ferry Road
Waterford, CT 06385

Subject: Planning Level Cost Evaluation
Logger Hill 24-inch PCCP Gravity Sewer Main

Dear Tali:

As requested, we have completed a preliminary project cost estimate for the replacement of approximately 2,200 linear feet of 24-inch PCCP gravity sewer main between Sewer Manhole No. 24 at the intersection of Rope Ferry Road and Gallup Lane to Sewer Manhole No. 11 at the bottom of Logger Hill Road. As presented in our October 24, 2011 letter proposal, there are many current unknowns to consider that require further investigation as part of our preliminary design effort for determining the feasibility of complete replacement versus trenchless technologies such as sliplining, pipe bursting or cured-in-place lining (CIPP) systems. Such investigations include a hydraulic evaluation of the wastewater flows to determine the required inside diameter of the new pipe, the preparation of an excavation plan for the required number of insertion and extraction pits, the required number of full sewer manhole replacements, the recommended pipe material, the appropriate type of pipe bursting method (static versus pneumatic), and the determination if sliplining, pipe bursting or CIPP lining is even possible for this application.

Therefore, it is recommended that the Waterford Utility Commission consider budgeting and appropriating funding for the complete replacement of the subject sewer main as presented in Table No. 1 to ensure adequate funding is in place to cover the costs for the recommended replacement/rehabilitation alternative that would be identified as part of the preliminary design effort.

Neftali Soto, P.E.
October 28, 2011
Page 2 of 2

Table No. 1
Planning Level Cost Estimate for Pipe Replacement Alternative

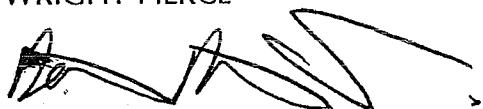
Item	Cost
Pipe Replacement of 2,200 L.F. with new PVC at \$300/L.F.	\$ 660,000 ¹
Replacement of 9 Sewer Drop Manholes (No. 20 to No. 11)	\$ 100,000 ²
Bypass Pumping Allowance for Pipe Replacement	\$ 150,000 ³
Bituminous Pavement Restoration	\$ 225,000 ⁴
Traffic Control Allowance	\$ 50,000 ⁵
20% Construction Contingency	\$ 237,000
15% Engineering (Design, Bidding, Construction & Inspection)	\$ 213,000
TOTAL ESTIMATED PROJECT COST	\$ 1,635,000

- Notes:
1. Includes excavation, demolition of existing pipe, installation of new PVC pipe and backfill.
 2. Includes excavation, demolition of existing manhole, installation of new manhole and backfill.
 3. Assumes 1-month of bypass pumping based on quote from Godwin Pump.
 4. Assume curb-to-centerline paving on Rope Ferry Road and trench paving on Logger Hill Road.
 5. Assumes 2-weeks of certified flaggers and 2-weeks of state police.

Once the preliminary design phase effort is completed, the most feasible and cost-effective option will be presented to the WUC for approval to incorporate it into the final design bidding package for construction in the Spring of 2012, which may be complete pipe replacement. Also, please find enclosed supplemental information on pipe bursting which may be a viable alternative to CIPP relining, sliplining or complete pipe replacement.

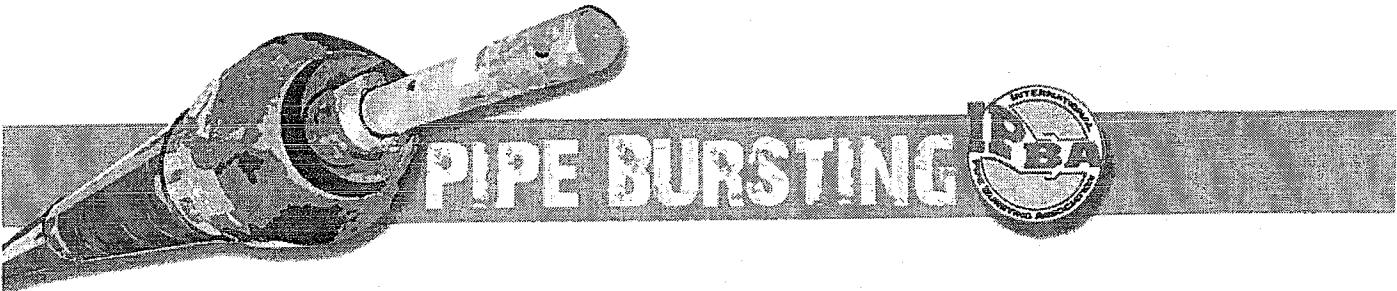
If you have any questions, or if you need additional information, please feel free to call me directly at 860-852-1920.

Sincerely,
WRIGHT-PIERCE



Dennis A. Dievert Jr., P.E.
Project Manager

Cc: J. Bartelli, WUC
C. DeScisciolo, WUC
All WUC Members



Pipe Bursting Various Types Of Pipes

by Matt Timberlake President, Ted Berry Trenchless Technologies LLC, International Pipe Bursting Association Marketing Committee

Pipe bursting is a mature technology used to replace existing underground utility lines. Some of the most common questions owners and engineers have when considering pipe bursting are "what types of pipes can be burst?" and "what types of pipes can be installed by pipe bursting?"

This article will provide insight into answering those questions as well as highlight some of the design and practical considerations that need to be accounted for on any pipe bursting project.

First and foremost when considering any pipe bursting project, it is important to identify the type of material of the existing pipeline. Over the last 100-plus years, gravity and pressure pipes have been installed made of wood, brick, clay, iron, cement, asbestos cement and plastics as well as other less common types and variations of these pipe materials. Pipe types like PVC truss pipe, Perma-Strand and others are trade names and these "hybrid" types of pipes should be identified clearly in the planning stage of a project as they should be looked at differently in some cases than more common pipe types.

Pipe bursting must fracture and split the existing pipe. Therefore, pipes are most often classified as "fracturable" or "non-fracturable" and that helps determine both the method of pipe bursting that can and should be used as well as the type of "burst head" or "splitter" that will be necessary to properly "burst" the pipe. Nearly all pipe types can be safely burst if considerations are made for how they will react to the bursting process.

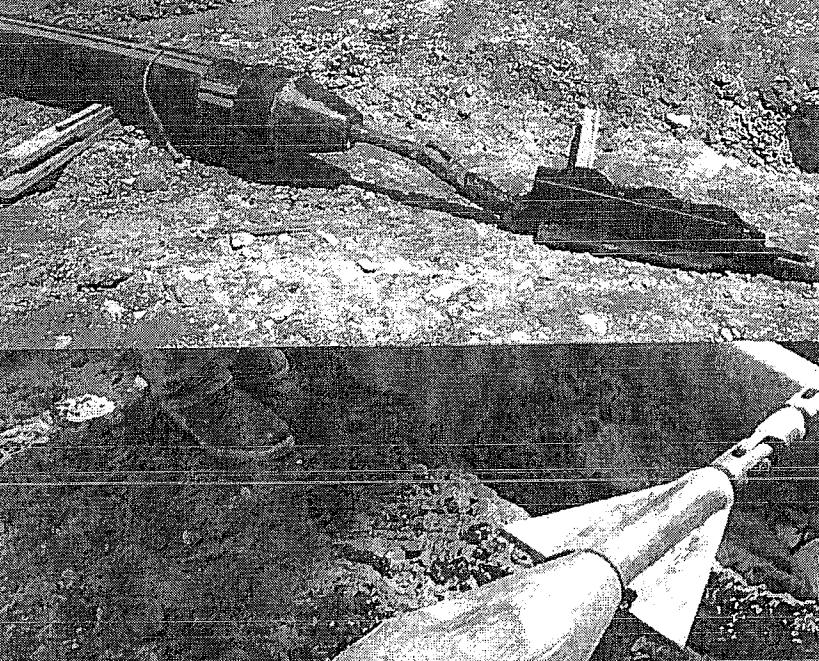
The most common types of gravity sewer pipes burst and replaced are: vitrified clay pipe (VCP), asbestos cement (AC), concrete pipe (CP) and polyvinyl chloride pipe (PVC). With a properly designed pipe bursting system, materials such as reinforced cement pipe (RCP) and even brick can be burst very successfully. In municipal water main replacement, which is widely popular overseas and gaining more acceptances in North America, CI, AC and ductile iron are the most common pipe types being replaced. These pipes must be "split" and not necessarily "burst." However, the process is very similar to that of a fractur-

Editor's Note:

The International Pipe Bursting Association (IPBA), a division of NASSCO, is presenting a series of articles on Underground Construction that will provide the reader with a better understanding of the technology, many myths and misconceptions exist regarding this proven rehabilitation method for replacing existing underground utilities.



Pipe bursting with DI Cutter often referred to as pipe splitting.



Typical tooling being used on a VCP sewer main replacement.

able pipe. This is often referred to as pipe splitting but is simply a different attachment on the front of the pipe bursting tooling.

Special attention

Pipes that require special attention and are, in most cases, still considered experimental, are concrete mortar pipe (CMP), large diameter brick among others. CMP, for example, does not typically burst or split very well. However, there is work being done daily to help overcome this and make bursting of CMP a viable option.

The most common type of pipe installed with pipe bursting, much like HDD, is overwhelmingly high density polyethylene (HDPE). However, advancements in other pipe types have made installations of those much more common. HDPE can be installed as a gravity or pressure pipeline and is very flexible and forgiving which gives it a tremendous amount of versatility in the field. Most trenchless installation crews are experienced in how to properly handle, fuse, and install it, making it a favorite of contractors. Learning the difference between IPS (iron pipe size) and DIPS (ductile iron pipe size) and the appropriate DR value required to install it and the relationship that has to actual inside diameter (ID), is always challenging to owners and engineers unfamiliar with the product. HDPE pipe can be installed by static or pneumatic pipe bursting methods.

Advancements in PVC products from restrained joint to fusible PVC have helped make pipe bursting an option in communities where they prefer PVC pipe. Restrained joint PVC pipe can be installed by a cartridge type installation which reduces the footprint of a project in a congested area, and fusible PVC is installed through a continuous installation similar to that of HDPE. PVC pipes can only be installed by static pipe bursting.

Ductile iron is a standard in many utility systems and is also a product that can be installed by pipe bursting. Special restrained joints allow for the pull loads required to install the pipe. However, installations require special considerations due to the need for a bell and socket style joint. DI pipe can only be installed by static pipe bursting and can be done through a cartridge style pit or a continuous installation.

Projects involving the installation of materials like VCP, fiber reinforced pipe (FRP), steel pipe and others have been accomplished and can be considered, although you should consult with the pipe manufacturer prior to installing.

The most important thing to understand when considering or planning a pipe bursting job is that special considerations must be made for the pipe types that are being replaced and installed. Consulting with a pipe manufacturer, pipe bursting contractor or

system manufacturer is a great way to assist in design considerations as they will often have the best real world experience and will be able to make recommendations on how best to design a constructible project. IPBA member companies have the experience and knowledge needed to help you consider and evaluate pipe bursting as a practical trenchless construction method. There are thousands of case studies available so you can be assured that the means and method you are considering is a proven approach.

Through continued education involving

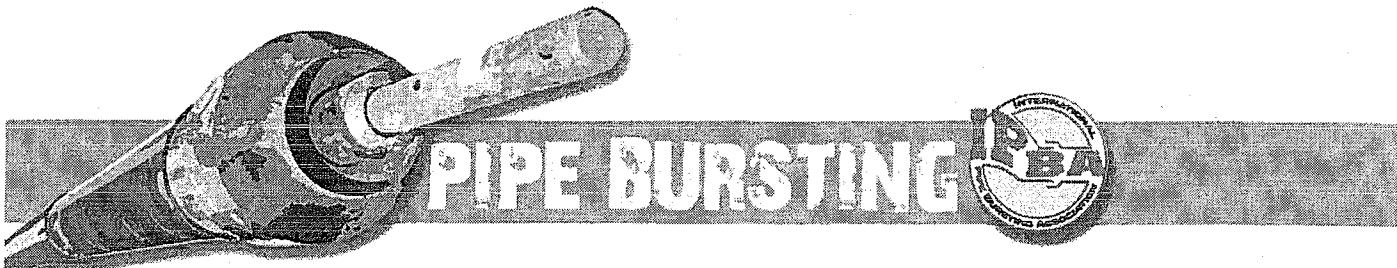
utility system owners, engineers, manufacturers, contractors and field crews, pipe bursting can continue to become a method of choice for rehabilitating failing underground infrastructure in your community.

Coming In July: How Geotechnical Conditions Effect Pipe Bursting

FOR MORE :

NASSCO

(410) 486-3500, www.nassco.org



Geotechnical Conditions And How They Relate To Pipe Bursting

by Matt Timberlake President, Ted Berry Trenchless Technologies LLC, International Pipe Bursting Association Marketing Committee

Pipe bursting is a method of pipe replacement that involves three main forces that must be overcome to accomplish installing a new pipe. A basic understanding of these three forces is required for anyone involved in a pipe bursting project from conception, to design, through final construction. A more detailed level of understanding of the effects of varying ground conditions (geotechnical data) is essential to the success of a project by the senior team members including engineer, owner, contractor and field crews.

The first force, from which pipe bursting derived its name, is the force required to fracture or "burst" an existing pipe. Most types of pipes from two-inches to more than 48-inches can be split or burst and that is accomplished by pulling a hardened steel head through an existing pipe that is configured to focus energy on the pipe wall until it fails from the inside out.

The second and third force will be the focus of this article. The second force is "expansion," which is the force required to expand the existing ground to allow insertion of the new pipe. Typically expansion of the existing hole (existing pipe inner diameter in inches) by 20-25 percent will be required to install the new pipe. Pipe bursting is the only pipeline rehabilitation method that allows the newly installed pipe to have the same or larger ID than the existing pipe.

This force will change dependent on existing ground conditions. Typical geotechnical reports for construction and, more specifically, trenchless construction, will not be of much value to a pipe bursting project unless they are given for the area inside the original trench. Because the pipe bursting process is replacing a pipe that was originally laid in a trench, the required information for properly estimating the force required to expand the soil to the required diameter needs to be supplied from the original trench design. Often, it is not practical to perform standard soil borings in such close proximity to an active pipe like a sewer, water or gas main. However, any borings must provide data that is comparative to those soils found inside the trench.

Editor's Note:

The International Pipe Bursting Association (IPBA), a division of NASSCO, is presenting a series of articles in *Underground Construction* that will provide the reader with a basic understanding of the technology, many myths and misconceptions regarding this proven rehabilitation method for replacing existing underground utilities.

Soil reaction

Virgin soils may, in some areas, have been used as backfill in the original trench, but in most cases "fill" was hauled in, which replaced the original soils during construction. Frequently, a "test pit" is specified in the bid but it is not given prior to the bid opening. It is simply a way to pass soil condition responsibility off to the general contractor. A test pit prior to the bid being released is a more practical alternative. This will give the project team and the bidders real world information in which to base their burst plan. If any special materials were used for the original launching of the pipe or shoring of the original trench, they are essential to the preplanning stages.

Narrow ledge trenches were often dug to install pipes and they may be only slightly larger than the OD of the existing pipe. A pipe bursting expander head may not physically fit through that narrow trench. These locations should be determined prior to considering the project as they are not conducive to pipe bursting.

The groundwater table is a very important consideration in any pipeline construction plan and pipe bursting is no exception. Although pipe bursting can in many cases be completed successfully with little or no dewatering over the entire length of the project, there may be very specific dewatering needs at the insertion and receiving pits. This should be carefully considered and part of the construction plan.

Certain soils are very favorable to pipe bursting and others are more challenging but can be overcome with properly preplanning the project using actual conditions. It is critical to understand soil dynamics and how varying types and densities can affect the expansion and insertion process.

Drag

The third force is referred to as "drag" and is the force of friction that is being exerted from the soil returning into contact with the new pipe as it is being installed. As the soil is expanded to allow installation of the new pipe, it is only in its expanded state for

Narrow ledge conditions.



a short amount of time. Shortly after the expander head passes through the soil, the "relaxation" period starts, putting the original soil back in contact with the new pipe. This contact will be the final state of the pipe in the ground as the 20-25 percent void is gone approximately four to 24-hours after the burst is complete, depending upon soil conditions.

Each of the three forces can be managed and influenced through proper planning and understanding of the process. For example, the burst force can be directly influenced and reduced through varying burst head and tooling configurations. A vitrified clay pipe sewer line burst head is different from a cast iron burst head. An understanding of the tooling that is used during the burst is important and should be a part of

the project plan and submittal. This force can be controlled through the bursting system which can be either static or pneumatic. Expansion forces can be influenced through expander head design and is most often a factor when choosing the actual dynamic tonnages needed to complete a burst. Drag is a very important consideration and must be managed carefully in the field. Typically, drag increases as the length of the newly installed pipe is continually inserted into the ground.

Lubrication mixtures can often be used to stabilize the hole and lubricate the new pipe as it is inserted. A true understanding of downhole fluids or lubrication is needed as soils react to fluids in a number of ways dependent on the mixture. A common mistake is to utilize the wrong fluids and

actually increase drag in certain ground conditions. A HDD mud school is often a great way to learn the 101 level of the benefits of utilizing lubrication on a burst.

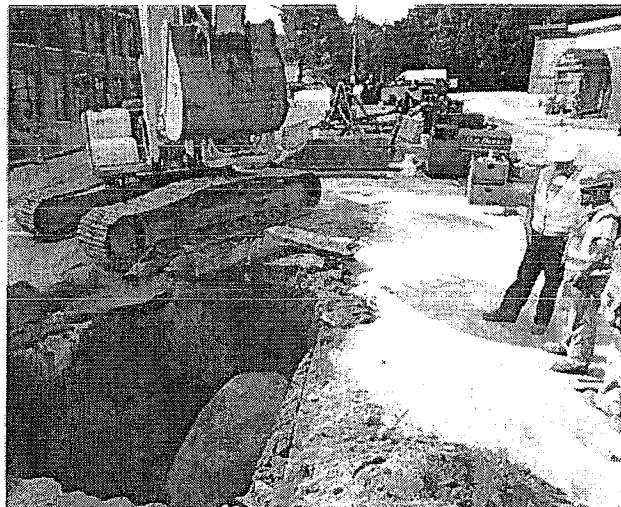
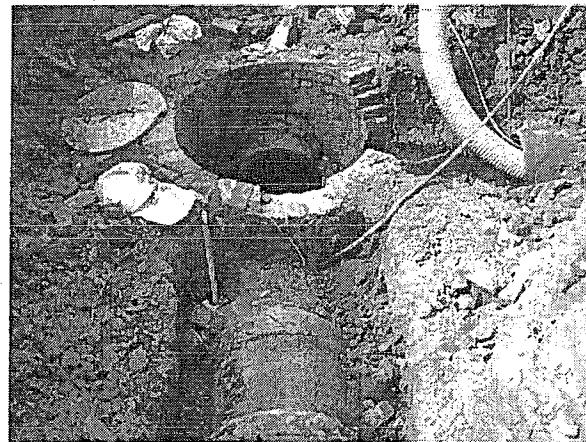
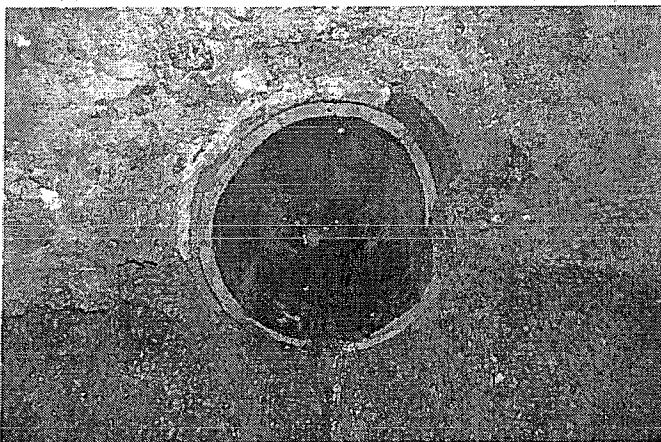
Through continued education involving utility system owners, engineers, manufacturers, contractors and field crews, pipe bursting can continue to become a method of choice for rehabilitating failing underground infrastructure in your community.

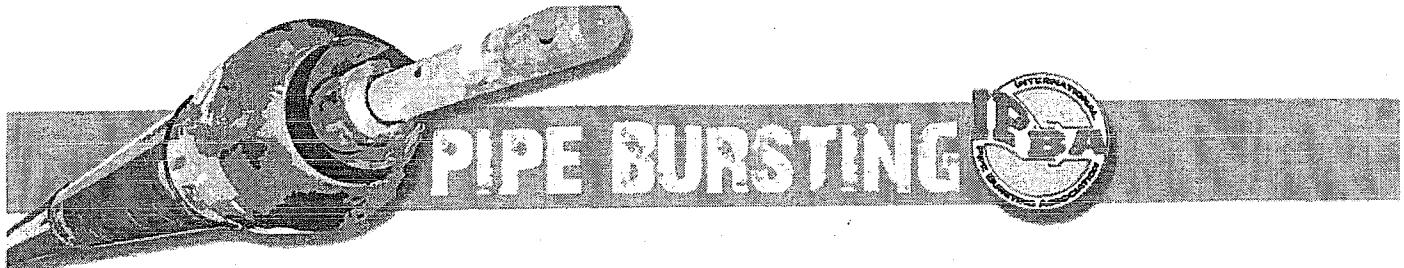
Coming In August: Breaking down barriers – How to help sell pipe bursting to utility owners in your community.

FOR MORE:

NASSCO
(410) 486-3500, www.nassco.org

Below, Top left (clockwise): Actual ground conditions surrounding a 15-inch VCP pipe approximately 28-feet to invert in Boston, MA (note the change in materials that the pipe was bedded on); This shows the original trench design of a 24-inch VCP pipe prior to the burst; A large rock approximately 6-feet in diameter that was used as backfill and needed to be removed to complete a burst; Installation of an expander head to a new HDPE pipe with a lubrication port located approximately 6-inches behind the head.





Breaking Down Barriers – Gaining Acceptance For Pipe Bursting In Your Community

by Matt Timberlake President, Ted Berry Trenchless Technologies LLC, International Pipe Bursting Association Marketing Committee

Pipe bursting is a proven method for replacing underground pipelines that provide critical services including municipal water, sewer, gas, storm water, electrical, telecommunications systems and more to people throughout North America and the world. In many parts of the country, pipe bursting is still considered a new technology; however, pipe bursting has been performed in Europe for more than 40 years and is a proven component of many long-term capital improvement programs in the United States.

In the U.S., many communities are unwilling or slow to consider an alternative to traditional construction methods. As engineers, contractors and manufacturers, we are often asked to help "sell" the new technology to utility system owners, consultants or public officials.

Utility system owners are constantly under pressure to do more with less. Finding alternative solutions is becoming more readily accepted; however, barriers remain to any technology new to a region.

The two primary challenges that pipe bursting faces in gaining community acceptance are:

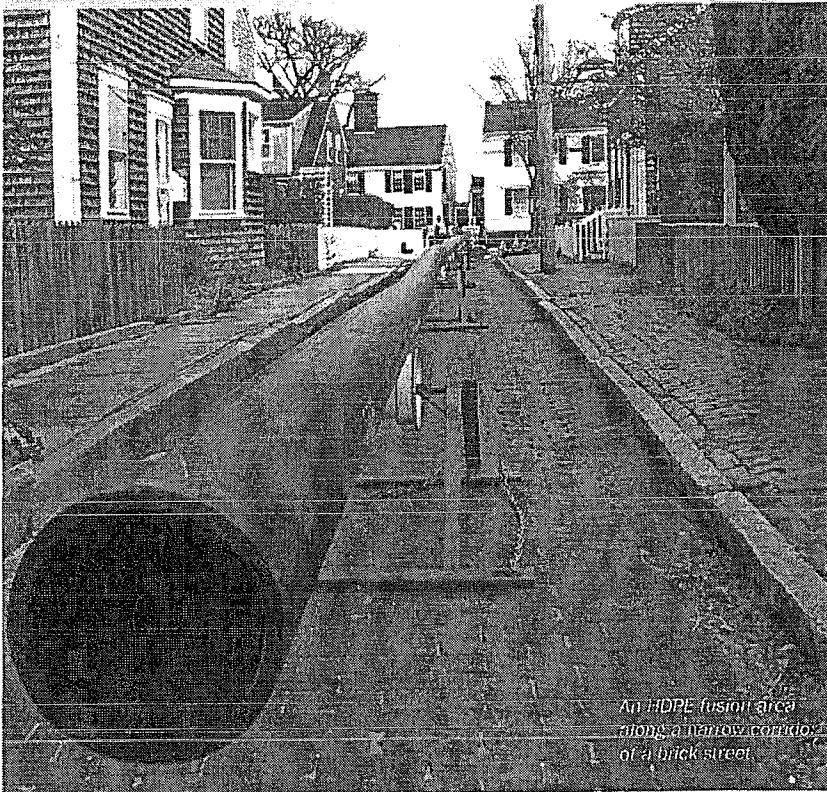
- Acceptance of a "new" or unfamiliar technology to those in the underground utility industry; and
- Acceptance of HDPE pipe for use in a system — it has been by far the primary type of pipe installed over the past 15 years.

Overcoming these barriers is critical to the responsible growth of this technology. However, it is important to build a comfort level with the decision makers responsible for spending public funds in your community. In many areas, most design and/or consulting engineers do not have a great deal of practical experience with pipe bursting and thus are reluctant to suggest the technology to clients. Industry events are a great way for engineers and consultants to gain knowledge of this technology without putting their clients at unwanted risk. Many IPBA members will invite engineers or utility system owners to a jobsite so they can see, in real life, how pipe bursting is accomplished and the benefits of this technology.

Having a network of resources is a crucial step in researching this technology and being able to present it to the decision makers

4th In A Series From The IPBA

Editor's Note: The International Pipe Bursting Association (IPBA), a division of AMSSCO, is just sending a series of articles in Underground Construction that will provide the reader with a better understanding of the technology, many myths and misconceptions regarding this proven rehabilitation method for replacing existing underground utilities.



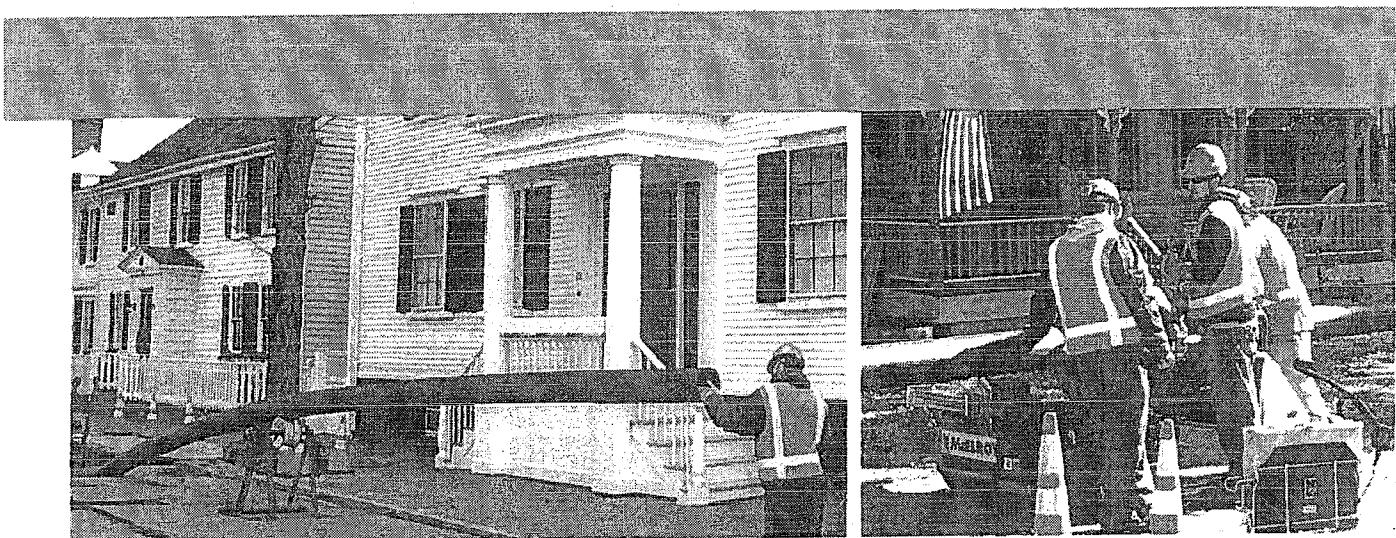
An HDPE fusion joint along a narrow corridor of a brick street.

in a community. Of course, the presentation of any technology will achieve better results if the information presented is consistent with audience needs or wants. The level of understanding an engineer must have to feel comfortable with the technology will be much different than that of a small town utility board. Gaining the trust of all stakeholders is important. Many IPBA members have found great success in presenting information as simple as basic jobsite layout and construction techniques to technical design calculations and project impact studies. It is also important to hear the perspectives of the utility service departments as they often have valid concerns in regards to bringing

a new type of pipe into their system or the constructability of the project.

Acceptance

One of the biggest factors holding pipe bursting back from gaining wider acceptance is simply "fear of the unknown" and the "if it ain't broke don't fix it" mentality. Most communities are very comfortable with the processes involved with traditional open-cut utility construction, CIPP for sewer main rehabilitation or even cement mortar lining for municipal water main rehabilitation. As the most common rehab methods in the U.S. for water and sewer, these methods are still very reliable; however, pipe



Left: An insertion pit in a high-end residential community. This was the first ever pipe bursting job in this area which took a significant amount of time to break down the barriers and sell the technology to the community. **Right:** HDPE fusion is used on a job and is a reminder that crews must be mindful of private property on a pipe bursting project.

bursting will offer advantages that many of these traditional methods can't match. Pipe bursting is the only method of pipeline rehabilitation that is able to replace an existing pipe with a new pipe that has the same or larger inner diameter without the need to dig a trench for the length of the project. With the growth of our communities and increase in demands from our underground utility infrastructure, as well as the public's demand for lessened disruptions and a greater environmental awareness, considering pipe bursting now may be good timing for your community.

In a world where there seems to be a new technology regularly announced on the evening news, we must understand the public's apprehension to consider anything other than business as usual. We need to look at the general public's lack of understanding of our technology more as an opportunity than a barrier. This can also be said for the overall underground construction market as it has recently expanded to include reliable methods for installing materials like polyvinyl chloride (PVC), ductile iron and others by trenchless methods including pipe bursting.

The IPBA Education Committee is currently active in promoting of pipe bursting through outreach efforts at national construction and utility seminars as well as to various associations, federal, state and local agencies throughout the United States. The IPBA has available a number of non-biased presentations and technical documents that can provide insight into the benefits and challenges associated with this type of construction.

Anyone in the construction industry knows all too well that good news travels slowly but bad news travels at the speed of sound. We would be remiss to say that there have never been failed pipe bursting jobs because there surely have; any veteran of the construction industry has a horror story they can find about a project gone bad with any type of method. Open cut, CIPP, sliplining or basically anything in the construction market has the potential for not going as planned. Unfortunately, negative selling is much more common in the construction industry than it should be and sometimes the knowledge of a process, product or technology is solely based on "hearsay" and not facts. The associations and groups that exist in this country and internationally are a great resource for research and, when all else fails, there is always the internet.

One of the benefits of collaboration through an organization or association is that you are able to learn and continually improve a process based on the challenges others have faced. The IPBA has united members from all aspects of the industry to help grow the market responsibly that involves a constant effort to "break down barriers" that exist.

Coming In September: The effects of pipe bursting on nearby utilities: predicting, preventing and managing the effects pipe bursting has on nearby utilities.

FOR MORE INFORMATION:

IPBA NASSCO)
410-486-3500, nassco.org

PIPE BURSTING

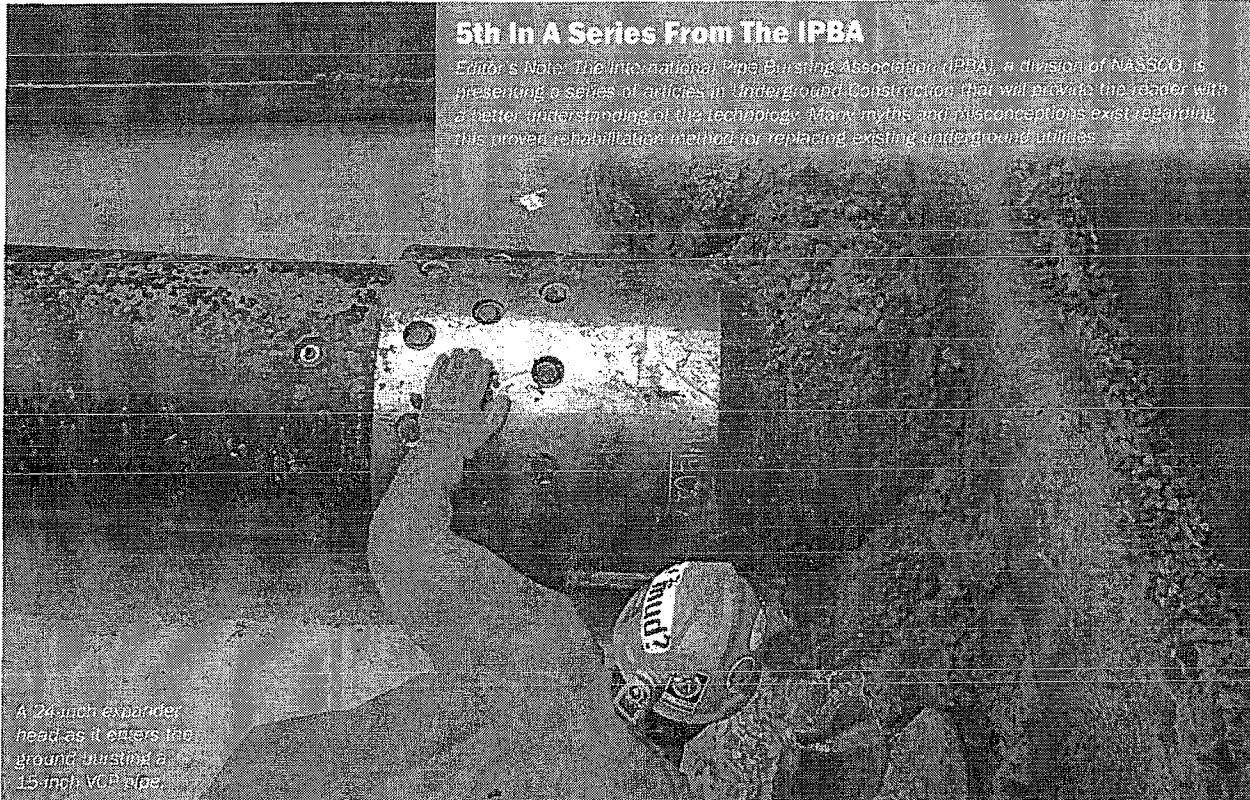


Effects Of Pipe Bursting On Nearby Utilities Predicting, Preventing And Managing Such Effects

by Matt Timberlake President, Ted Berry Trenchless Technologies LLC, International Pipe Bursting Association Marketing Committee

5th In A Series From The IPBA

Editor's Note: The International Pipe Bursting Association (IPBA), a division of NASSCO, is presenting a series of articles in "Underground Construction" that will provide the reader with a better understanding of the technology. Myths and misconceptions exist regarding this proven rehabilitation method for replacing existing underground utilities.

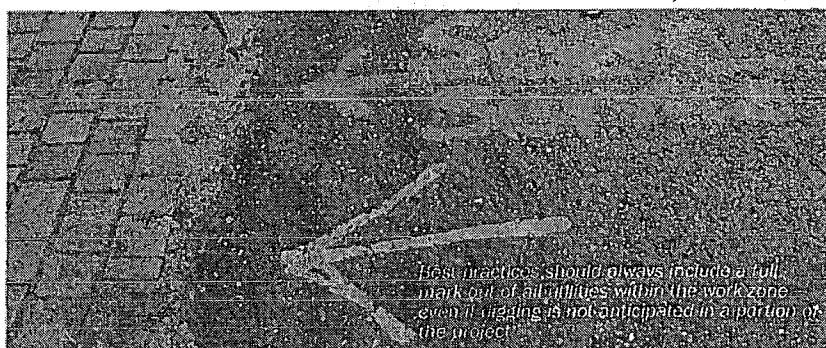


124-inch expander head as it enters the ground bursting a 15-inch HDPE pipe.

This article will discuss the potential impact on adjacent utilities as a pipe is replaced using pipe bursting technology. From concept through design and construction a true understanding of the effects pipe bursting has on your entire team is essential in preventing utility damage and ensuring a successful project.

First and foremost it is important to understand how pipe bursting works and this article will be written with the assumption that the reader has a basic level of understanding for the pipe bursting method and its common applications. (See previous articles beginning with the May edition of *Underground Construction*.)

As with any construction project, having a management model in regards to risk assessment and reduction is critical. Although many aspects of utility construction, including pipe bursting, cannot predict an exact scope of impact; tools to manage such an



Best practices should always include a full mark out of utilities within the work zone, even if they are not anticipated in a portion of the project.

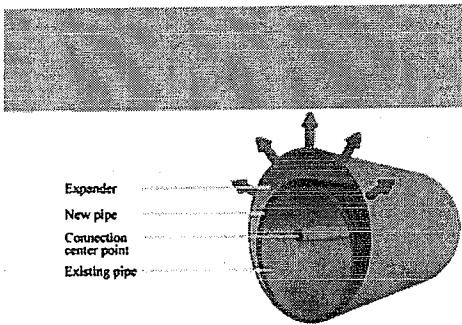
occurrence are available through proper design and risk management.

Pipe bursting, whether static or pneumatic, will pull a hardened steel burst head through an existing pipe and expand the soil, which allows a new pipe to follow in its path. The amount of influence a pipe burst will have on the surrounding soils and adjacent utilities is driven by the amount of

expansion required and type of soil surrounding the "potential impact area."

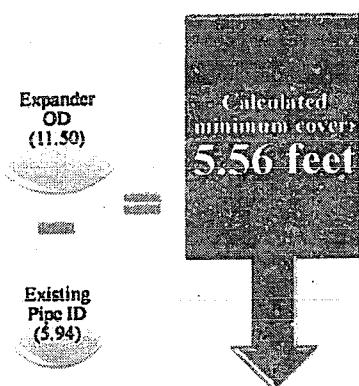
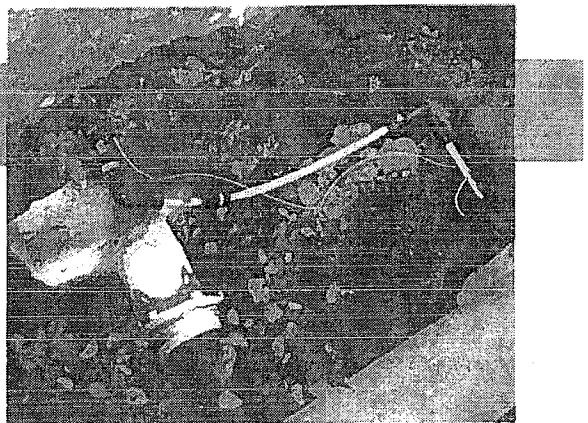
Rule of thumb

To calculate the potential effects, there are a few general rule of thumb calculations. However, these must be modified to account for varying geotechnical conditions. The potential impact zone is calculated



A common rule of thumb for determining the size of the expander is that it will be 20-25 percent larger than the OD of the newly installed pipe.

This photo shows a 2-inch plastic gas main that was rerouted prior to a static pipe burst around the receiving pit to allow machinery and equipment to be set. The separation shown between the existing CI pipe being burst and the gas line were sufficient enough that no risk of damage was present even through it followed the path of the burst.

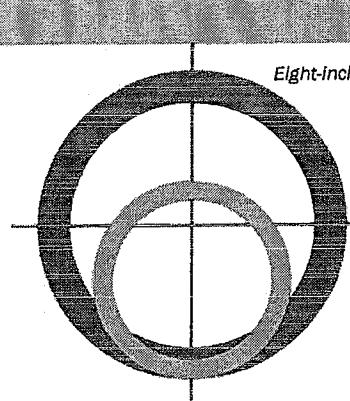


by subtracting the inner diameter (ID) of the existing pipe from the outside diameter (OD) of the expander head that is doing the pipe bursting and then multiplying by 10. This will give you in inches an approximate "potential impact zone" where the possibility of damaging another utility exists if previsions are not made.

An example would be an existing 8-inch vitrified clay pipe sewer main being replaced with a new 10-inch high density polyethylene (HDPE) pipe.

If a 12-inch OD expander head were selected, the potential impact zone would be 12 inches minus 8 inches equals four inches; then multiply by 10 inches which equals 40 inches. Forty inches will be a baseline for determining the amount of separation from the existing pipe center line to adjacent utilities. The example for an 8 inch, size-on-size burst would be 10 inches minus eight inches equals two inches times 10-inches equals 20 inches.

In addition to the simple math that is required to determine amount of oversize for the burst head, a more important but complicated formula is to determine the volumetric displacement of the amount of earth moved in the burst to allow passage of the head through the original pipe ID. For example, the volumetric displacement would be greater for replacing a 10-inch pipe with a 12-inch pipe than it would be for replacing a six-inch pipe with an eight-inch pipe. Either of these scenarios is often referred



Eight-inch ductile iron pipe burst head and an 11.5-Inche O.D. expander.

to as a "one upsize" in pipe bursting lingo. Whereas a 6 inch to 6 inch would be a "size-on-size" and a 6 inch to a 10 inch would be a "double upsize."

As stated earlier, geotechnical conditions play a critical role and may impact this potential by as much as double. Sands, loose gravel and small cobble move easily as the burst head travels through the ground and will have smaller potential impact. However, large cobble and some dense soils may have a greater potential impact area. This information should be provided on actual trench conditions

and not soil borings provided outside of the original pipeline trench as is common with HDD pre-design.

Direction of force

It is very important to remember that the force exerted during a burst is focused from the center point of the existing pipe upwards and outwards. This helps in designing the utility coordination and mark-out procedures which is where 90 percent of the pre-burst risk management will occur.

Often times on a project, a utility will be found to be in the potential impact area but will be marked as "actual location of water main to be field determined" which can, in most cases, be done through a small keyhole style hydro-excavation. Once actual invert, pipe size and type is found, it can be noted and either taken off line, removed prior to the burst, or safely passed by the bursting head depending on its proximity to the burst path.

The use of Quality Level A subsurface utility engineering data could potentially predict and prevent all utility strikes caused by pipe bursting. However, that may not always be practical or necessary on all projects. A Quality Level C would be the lowest acceptable level for most pipe bursting projects, but would not be sufficient for crossing "critical" utilities that would cause personal injury or property damage from a rupture. Most utilities being crossed by a traditional pipe burst would be in the order of sanitary sewer laterals, water services, storm drains, intersecting water and or gas mains, sewer force mains or electrical ducts. Most can be accurately estimated based on Level C field data such as opening sewer or storm manholes and shooting invert grades, opening and measuring gate valves or curb stops, or other "simple" field verification methods.

Utility damage prevention is a risk associated with all construction and pipe bursting is no exception. However, through a properly planned, designed and executed project, there have been millions of feet of pipe replaced utilizing pipe bursting technologies throughout the world both safely and without unreasonable risk to those involved or the general public. Experience establishes that more utilities have been damaged installing the insertion pits and receiving pits with excavation equipment than with any amount of pipe bursting.

Do your homework, select a quality project team and let's go pipe bursting!

Coming in October: Pre-chlorinated pipe bursting for potable water mains.

FOR MORE INFORMATION:

IPBA NASSCO)

410-486-3500, nassco.org

Appendix B

**EVALUATION OF CAPITAL FUNDING NEEDS
FOR WASTEWATER COLLECTION SYSTEM**
for the
**WATERFORD UTILITY COMMISSION
TOWN OF WATERFORD, CONNECTICUT**

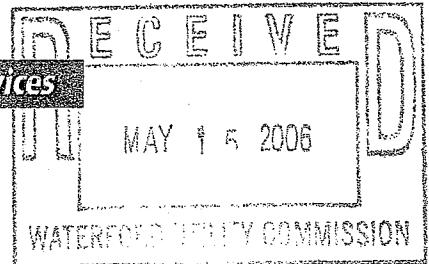


MAY 2006



Wright-Pierce

Civil and Environmental Engineering Services





Wright-Pierce

May 10, 2006
W-P Project No. 10232K

Mr. Neftali Soto, P.E.
Chief Engineer
Town of Waterford Utility Commission
1000 Hartford Road
Waterford, CT 06385

Subject: Evaluation of Capital Funding Needs for Wastewater Collection System - 2006

Dear Tali:

Please find attached nine (9) copies of the updated *Evaluation of Capital Funding Needs for Wastewater Collection System* for 2006. This report presents an overview of the type of capital improvements that the Utility Commission will likely need to address in the next 5 to 15 years in order to maintain a reliable wastewater infrastructure. The listing and priority of the specific items may change year-to-year, and should be fine-tuned accordingly. The sources and means to fund such work will also need to be modified depending on availability of funding sources. We believe this report justifies the need to provide an adequate capital reserve funding system in order to pay for necessary repairs to the wastewater infrastructure system.

As you are aware, the DEP does have 2% loan money available for the Evergreen Pump Station upgrade project and for the sewer extension projects. The Utility Commission should consider obtaining this loan money in the near future since such funding may or may not be available in the future, depending on the State Legislature actions. There are currently no DEP grant funds available for wastewater collection system projects.

Feel free to call with any questions.

Very truly yours;
WRIGHT-PIERCE

John W. Braccio, P.E.
Vice President

**EVALUATION OF CAPITAL FUNDING NEEDS
FOR WASTEWATER COLLECTION SYSTEM
FOR THE
WATERFORD UTILITY COMMISSION
TOWN OF WATERFORD, CONNECTICUT**

MAY 2006

Prepared By:

**Wright-Pierce
169 Main Street
Middletown, CT 06457**

EVALUATION OF CAPITAL FUNDING NEEDS FOR WASTEWATER COLLECTION SYSTEM

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EVALUATION OF CAPITAL FUNDING NEEDS FOR WASTEWATER COLLECTION SYSTEM

1 BACKGROUND

The Waterford Utility Commission is responsible for the operation and maintenance of a very extensive wastewater collection system including approximately 142 miles of gravity sewers and force mains, 26 centralized pump stations, and approximately 260 individual grinder style pump systems. The current replacement value of the sewers and pump stations is on the order of \$120 million. This system serves a relatively small population of approximately 17,000, when considering the size and complexity of the system. For example, the Town of Danielson has a similar sewerered population, but the system only includes 42 miles of sewers and 7 pump stations. Another example is the Town of Windham, which also has a similar sewerered population, but only has about 45 miles of sewers and 4 pump stations. As a result, even though Waterford currently has a favorable individual customer user rate compared to many other communities, the equivalent cost per customer to operate and maintain this extensive system will become more costly over time.

All wastewater collection systems require routine maintenance and repairs in order to provide safe, efficient and reliable service. Systems that are not properly maintained can result in sanitary sewer overflows (SSOs) due to sewer blockages or pump station failures, which in-turn will result in higher capital expenditures. Aging systems will also eventually require major capital repairs and upgrades that are beyond typical routine maintenance and repairs. If collection systems are not properly maintained and repaired, they can be subject to catastrophic failures, SSOs and/or damage to private property, which can all result in costly emergency repairs. In recognition of the aging wastewater infrastructure system throughout the country, the EPA is in the process of expanding the Clean Water Act with new rules which will require municipal systems to develop and implement a formal "Capacity, Management, Operation and Maintenance Program (CMOM)." The proposed CMOM program rules include requirements to continuously monitor and maintain wastewater collection systems in order to prevent SSOs. If SSOs do occur, a municipality will be subject to enforcement actions and fines.

In most communities, the wastewater collection systems have been constructed over a long time period; up to 200 years for some communities. Depending on the age and use, the condition of each section and component of the system will vary, with capital repair and upgrade needs occurring at different times. In contrast, the majority of the Waterford system was constructed in a relatively short time frame through the 1970s and 1980s, and the majority of the Waterford collection system is now between 20 to 30 years old. Because it is essentially all of similar age, it is reasonable to expect that capital repair and upgrades will be needed in the same relative time frame.

The Waterford Utility Commission is aware of the growing need to identify the upgrade needs of the collection system and as a result, requested that Wright-Pierce perform evaluations of known high priority concerns including the Evergreen and Mago Pump Stations, portable emergency generator for major pump stations, odor problems at several locations, and the SCADA (computer monitoring) system. The collection system has also been subject to an increasing volume of flow during wet weather, indicating the presence of a growing problem with infiltration and inflow (I/I).

I/I is clean water entering the sewer system which increases the costs to pump and treat sewage. Infiltration is groundwater leaking into the sewers through cracks or leaking joints in the sewer lines, manholes, or individual service laterals. Inflow is stormwater entering the collection system from roof leader connections, catch basins tied in to the sewers, foundation drains, sump pumps and area drains. For illustration, see Figure 1-1 on the next page.

The Utility Commission also requested Wright-Pierce to perform an initial I/I evaluation in order to determine the magnitude of the problem, and to develop the recommended scope to perform a more detailed sewer system evaluation survey (SSES) in order to identify specific I/I sources for repair.

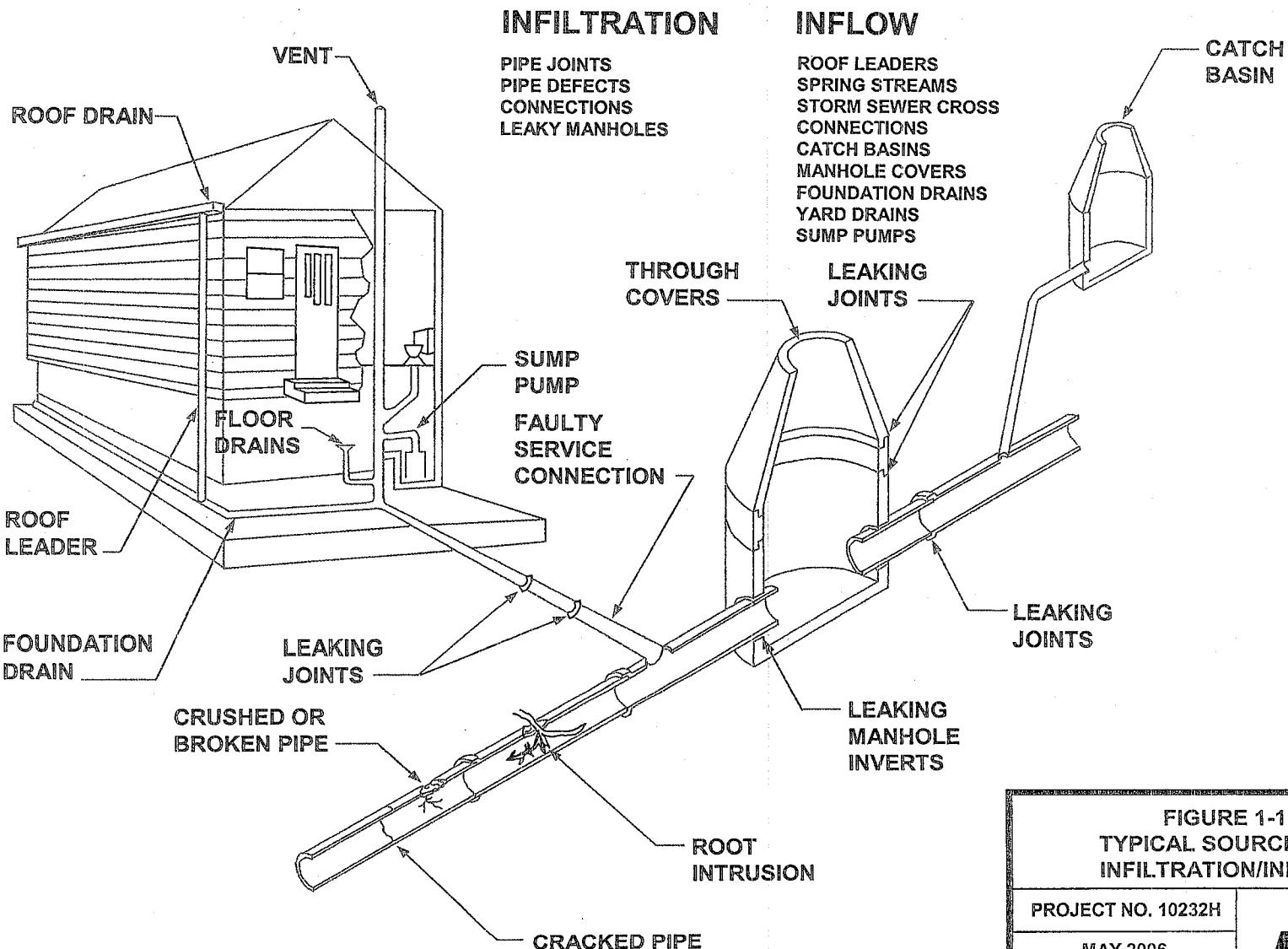


FIGURE 1-1
TYPICAL SOURCES OF
INFILTRATION/INFLOW

PROJECT NO. 10232H

MAY 2006

NOT TO SCALE



These initial evaluation efforts have identified some specific issues that Waterford will need to address, and which will require significant capital expenditures. However, these initial evaluations do not address the entire capital repair and upgrade needs that will eventually be required. The Waterford Utility Commission is developing a long-term plan for continuous capital repairs and upgrade of the existing collection system, separate of routine maintenance and repairs and separate of any planned sewer system expansions.

The following section presents a preliminary analysis of the possible magnitude of capital maintenance and repair needs that Waterford will likely face over the foreseeable future.

2 ANALYSIS OF CAPITAL IMPROVEMENT NEEDS

It is somewhat difficult to specifically identify all of the short-term and long-term capital maintenance and upgrade requirements and costs for the existing collection system. This should be a continuous process, based on a planned monitoring and inventory program to identify and prioritize the system needs. Based on known high priority issues, the Utility Commission did initiate the evaluations discussed above, and these studies have been finalized. Future studies will be needed to identify the next round of priority needs and costs. However, based on the experience of other communities, it is possible to develop the anticipated level of needs and costs to use as the basis for future planning and budgeting. The known and projected needs and anticipated costs¹ are discussed as follows:

2.1 Current Defined Needs

As discussed above, the Utility Commission initiated evaluations of the following high priority needs:

- Evergreen Pump Station - This is Waterford's main pump station which was originally constructed in 1976. This station receives the most wastewater flow and use, and is subject to increasing mechanical and electrical problems. The evaluation recommended an immediate partial upgrade, and a comprehensive upgrade of this station. The

¹ Costs presented in this section are based on costs presented in *Preliminary Evaluation of Capital Funding Needs for Waterford Collection System - October 2004*, which have been increased by 20% to account for the extraordinary construction inflation that occurred during 2004 and 2005.

immediate upgrade has recently been completed. The estimated cost for the complete upgrade of this station is approximately \$2.4 million and the final design has been completed. It is recommended that the construction of the complete upgrade be initiated in the immediate future since it will take on the order of 2 years to complete the construction.

- **Mago Pump Station** - This is the other major pump station in Waterford, which was originally constructed in 1982. This station also receives significant wastewater volumes and use, and is also subject to increasing mechanical and electrical problems. The evaluation recommended a partial upgrade of this station at an estimated cost of approximately \$480,000. It is recommended that this upgrade occur sometime within the next 5 years.
- **Supervisory Control and Data Acquisition (SCADA) System** - The existing computerized system monitors the status of the 26 main pump stations. It is somewhat antiquated and does not provide the more extensive monitoring and control functions of modern systems. The SCADA evaluation recommended an eventual complete upgrade of this system in order to take full advantage of the capabilities of modern technology. The cost for a complete upgrade is estimated at approximately \$540,000. This upgrade is not an immediate critical element, but does impact the efficiency of staff utilization and costs. Many utilities have had a positive return-on-investment for a modern SCADA upgrade in terms of less labor costs for emergency call-outs and for routine pump station monitoring. It is recommended that this system be upgraded within the next 5 years.
- **Portable Emergency Generator as Back-Up For Major Pump Stations** - Several of the existing emergency turbine generators at 6 of the major pump stations are no longer manufactured and there is increasing concern about their reliability and availability of spare parts. Wright-Pierce prepared a bid package for the purchase of a portable emergency generator, and also prepared plans for the construction of emergency power receptacles and transfer switches at 5 of the major pump stations. The cost for these items is estimated at approximately \$120,000. The generator should be purchased and

the receptacles constructed within the next 2 years in order to ensure reliable emergency power for these stations.

- I/I Evaluation - Based on the initial I/I evaluation, the recommended scope for a more detailed sewer system evaluation survey (SSES) was developed in order to identify specific I/I sources for repair. The estimated cost to perform the recommended SSES is approximately \$510,000. The Utility Commission intends to apply for the Connecticut Department of Environmental Protection (DEP) Clean Water Fund (CWF) Grant, which could cover 55% of the costs of the SSES program. However, the DEP CWF Grant is currently under funded; it is unlikely that grant funds will be available for several years. The Utility Commission does intend to conduct an initial SSES effort in some high priority areas in 2006. The budget for this initial SSES effort is \$45,000.
- Intra-Municipal Flow Metering Stations - Currently wastewater flows from New London which flow into the Waterford Collection System are not metered. New London utilizes individual household water meter readings to determine the wastewater flow into the Waterford Collection System. The equivalent sewer use fee is then "credited" by New London. Since the New London sewers are subject to significant I/I flow, it is likely that the current billing method does not accurately reflect actual wastewater flows and the corresponding fees. An evaluation of flow metering alternatives recommended installation permanent flow metering stations at four locations in order to ensure that New London is paying their fair share of costs to convey wastewater through the Waterford Collection system. The estimated cost to install the permanent flow metering stations is approximately \$220,000. It is recommended that these stations be installed within the next 2 years.

- Sewer Extensions - Several sewer extension projects were identified in the *Town of Waterford Utility Commission Capital Improvements Program, FY 2004 - 2008*. The following short-term, high-priority projects were identified in this document:

High Priority Sewer Extensions

<u>Project Location</u>	<u>Estimated Cost</u>
Harrison's Landing	\$2,580,000
Oil Mill Road/Gurley Road	\$625,000
Woodworth Drive	\$255,000
Cinderella Lane	\$660,000
Dayton South	\$1,670,000

Longer term sewer extension projects, or those which were not projected to be constructed within the next 10 years were also identified in the FY 2004 - 2008 document. These included the following:

Long Term Sewer Extensions

<u>Project Location</u>	<u>Estimated Cost</u>
Hickory Lane-Dayton	\$950,000
Paula/Ina	\$1,525,000
Dimmock Road	\$1,960,000
Pepperbox Road	\$580,000

2.2 Long Term Anticipated Needs

As discussed above, all wastewater collection systems require continuous repair and upgrade, along with routine maintenance. Items that will eventually need to be addressed in the Waterford system include:

- Long Term SSEs Efforts/CMOM - It is prudent operating practice to implement an annual program to routinely clean and inspect portions of the sewer system in order to avoid SSOs and prevent catastrophic failures. If/when promulgated, Federal CMOM

rules will require a formal program to monitor the collection system in order to identify maintenance and repair requirements. This will include cleaning and TV inspections of the entire sewer system, along with inspections of every manhole. Currently, the Utility Commission does not have all of the necessary equipment or staff to perform this type of monitoring work.

The recommended initial SSES program includes this type of effort for a portion of the sewer system. Over time, the Utility Commission will need to plan for such monitoring over the entire system on an annual basis. For example, the CMOM rule indicates that the entire sewer system should be cleaned and TV inspected every 5 years. Based on our experience, this seems somewhat excessive and we believe that a 15 year average frequency is more appropriate, especially for a newer system such as Waterford's.

Based on a typical current cost of \$1.35/foot for sewer cleaning and TV inspection, the cost for the entire Waterford system would be on the order of \$1.0 million or approximately \$70,000 per year over a fifteen year period. (*The actual cost for any particular sewer segment is dependent on the pipe size and amount of cleaning required*). The recommended initial 5-year SSES program includes sewer cleaning and TV inspection of approximately 120,000 feet of sewer, leaving approximately 635,000 feet of sewers to be completed over the next ten years with an estimated cost of approximately \$860,000 (not including inflation). Therefore, the Utility Commission could consider budgeting somewhere on the order of between \$75,000 to \$100,000 per year for sewer cleaning and TV inspection, depending on how much grant funding can actually be obtained from the DEP for the initial SSES effort.

The amount of sewer cleaning and TV inspection that initially should be performed by the Utility Commission will depend on how much DEP funding is obtained to perform the recommended initial SSES program. The costs are based on the work being performed by an outside contractor. Given the large amount of annual cost that would be allocated to sewer cleaning and TV inspection, it may be prudent for the Utility Commission to consider purchase of their own TV inspection equipment. However, this

in-turn will require additional staff to perform the necessary work. A reasonably equipped TV inspection system would cost on the order of \$130,000 to \$150,000 depending on the desired features.

- **Sewer Repairs and I/I Remediation** - One of the goals of the SSES program will be to identify sewer improvements and repair needs. This can include a variety of spot repairs up to full replacement of sewers and manholes. Until specific needs are identified, it is difficult to determine when the repairs will be needed, and what the cost will be to perform the repairs. One approach to determine the budget costs for sewer repair over time is to assume that all the sewers will eventually need to be replaced. Sewer replacement costs can vary considerably depending on size, depth, groundwater elevation and location. Assuming an overall current average cost of \$100/foot for sewer construction, the 142 miles of sewers in Waterford have an approximate replacement cost value of \$75 million, not including the pump stations. If it is assumed that the sewers will be gradually replaced over a period of 500 years, this would equate to an annual budget of approximately \$150,000 per year which could be allocated to sewer repair or replacement, or approximately \$2.25 million over 15 years (not including inflation).
- **Pump Stations** - Wright-Pierce has performed evaluations of the Evergreen and Mago Pump Stations as described above, and has also performed a cursory inspection of the remaining 24 pump stations. These stations are well maintained by Utility Commission staff and they are generally in good condition. However, the mechanical, electrical and building system components of these stations are aging and all the components will eventually need to be repaired, upgraded or replaced. The time frame and costs for the upgrade of these stations will vary depending on the age and use.

Based on experience in many other communities with similar size and age pump stations, it is projected that each of these stations will require some type of capital upgrade over the next 10 to 15 years. The typical upgrade of these pump stations would cost on the order of \$100,000 to \$500,000. The 2 ejector stations will eventually need to be replaced at an estimated cost of \$360,000 each. Assuming that 22 stations will need to be

upgraded at an average cost of \$250,000 each and replacement of the 2 ejector stations at \$360,000 each, the Utility Commission should be planning on the need to budget approximately \$6.22 million over the next 10 to 15 years, or approximately \$400,000 to \$600,000 per year (not including inflation).

- Individual Property Grinder Style Pump Stations - The Waterford Utility Commission is also responsible for the operation and maintenance of approximately 260 individual property grinder style pump stations. The Utility Commission also provides a user fee credit to each property for the electrical cost to operate each station. Utility Commission staff routinely addresses equipment problems with these stations and reportedly have the equivalent of one staff member dedicated to this effort. Based on our current understanding of this program, it appears that it results in an overall lower equivalent user fee for properties with pumping systems, compared to those without pumping systems. There are other communities in Connecticut and throughout New England that have similar structured programs, but it is more typical for such systems to be the complete responsibility of the individual property owner.

As with the rest of the collection system, the mechanical, electrical and structural components of these systems are aging, and it is projected that major components will need to be replaced over the next 10 to 15 years. Based on an assumed upgrade cost of \$2,200 for each individual unit, the Utility Commission should be planning on the need to budget approximately \$600,000 over the next 10 to 15 years (not including inflation).

3 CAPITAL IMPROVEMENT PLAN

Because the Waterford system is relatively new, it has not been subject to any significant repair and upgrade needs. However, most of the sewers and pump stations are now over 25 to 30 years old, and there will be an increasing need to address normal deterioration and wear. The Waterford Utility Commission should plan for the long-term repair and upgrade of the collection system in addition to routine operation and maintenance requirements.

The Waterford Utility Commission will need to address short-term capital improvements over the next 5-years, along with longer term capital improvements over the next 15-years.

Based on the analysis presented in the previous sections, possible budget requirements over the next 15 years beginning in fiscal year 2006 through 2020 are summarized on an annualized basis in Table 3-1. Costs shown in Table 3-1 assume a 5 percent inflation rate for escalating current year costs to future costs.

Costs for recurring longer term improvement items (Grinder pump station repairs, sewer repairs, partial upgrades to remaining pump stations, ongoing SSES/CMOM program) have been shifted and apportioned as shown so that the total annual costs remain fairly constant from year to year.

It should be noted that the original *Preliminary Evaluation of Capital Funding Needs for Waterford Collection System - October 2004* assumed that pump stations and sewer repairs could qualify for 20% Grant/ 80% Loan from the DEP CWF. However, the CWF is currently under funded and the DEP has indicated that no grant funding will be available for sewer repairs. The DEP has also indicated that 2% loan money *may* be available in Fiscal Year 2007, which starts July 1, 2006.

Total costs assumed to be eligible for CWF loan funding are based on the net total annual costs less the costs for the ongoing SSES/CMOM program (not eligible for CWF loan funding). Annual debt service is shown based on CWF loan funding at 2 % over 20 years with the pay down of the loan beginning one year following the commitment from the DEP. Based on discussions with Commission staff, a \$100,000 per year contribution from the Town's Enterprise Fund to the debt service has been assumed, subject to final Commission approval. Mill value is shown annually based on a current year value of \$2,443,366 and a 1% per year mill value escalation rate. The accumulated mill rate impact shown for each year is based on dividing the accumulated debt service by the mill value. (Note that the analysis on Table 3-1 is based on the assumption that the state legislature will continue the CWF in its current form, will increase funding of the program, and that Waterford pump stations and sewer repair projects will qualify for loan-only funding on an annual basis.)

4 RECOMMENDATIONS

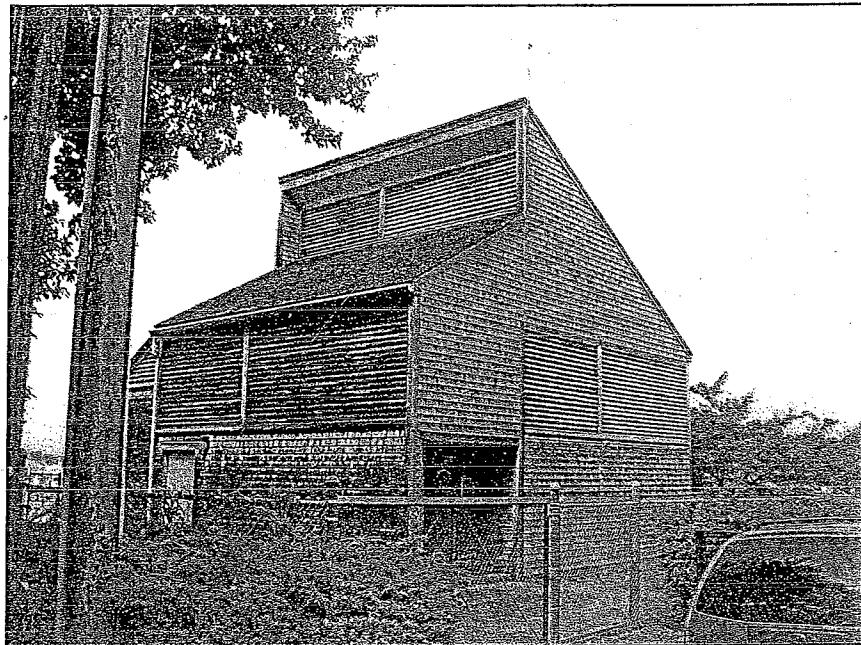
Based on this preliminary analysis, it is recommended that the Waterford Utility Commission consider planning for major capital expenditures over the next 5-years, and also develop an annual budget to address longer-term issues as part of a 15-year capital improvement plan. The source of funds could include a portion of the user fee sinking fund, contributions from the Town's general fund, and possible state grants and loans. The DEP Clean Water Fund (CWF) currently allows for a 20%/80% grant/loan program for cost-effective sewer repairs, and 55% grant funds to perform a Sewer System Evaluation Survey (SSES). However, the CWF is currently under funded, and there is no 20%/80% grant/loan funds available for the next state budget cycle of 2007 to 2009. The DEP does have a 2% interest loan program for general sewer infrastructure and pump station repairs. This 2% loan program *may* be available for Waterford projects beginning in July 1, 2006 (2007 fiscal year).



Wright-Pierce

Civil and Environmental Engineering Services

EVALUATION OF MAGO POINT WASTEWATER PUMP STATION



Prepared for the
Waterford Utility Commission
Town of Waterford, Connecticut

FINAL

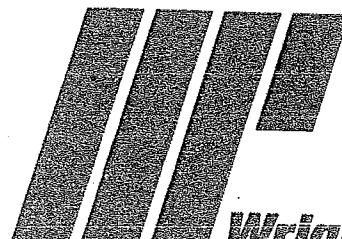


August 2004

**INTER-MUNICIPAL FLOW MONITORING
EVALUATION**
for the
WATERFORD UTILITY COMMISSION
TOWN OF WATERFORD, CONNECTICUT

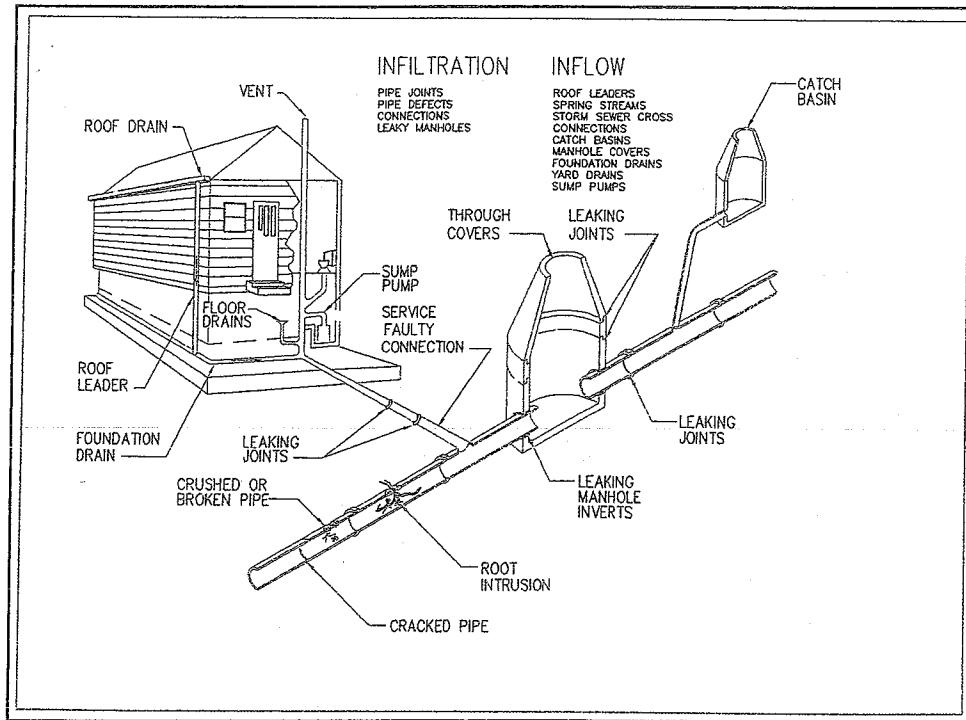


DRAFT
AUGUST 2005



Wright-Pierce
Civil and Environmental Engineering Services

INFILTRATION AND INFLOW ANALYSIS



Prepared for the
Waterford Utility Commission
Town of Waterford, Connecticut



May 2004

***SUPERVISORY CONTROL
AND DATA ACQUISITION
AND
TELEMETRY EVALUATION***



*Prepared for the
Waterford Utility Commission
Town of Waterford, Connecticut*



June 2004

ODOR CONTROL EVALUATION



***Prepared for the
Waterford Utility Commission
Town of Waterford, Connecticut***

DRAFT



AUGUST 2004